

HERITAGE IMPACT STATEMENT



Development Application

No. 22-24 Raglan Street, Manly

December 2022 | J3555

**Weir
Phillips**
Heritage
and Planning

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Cover Image: No. 22 Raglan Street, Manly.

We acknowledge that the land on which we live, learn and work as the traditional country of the Gadigal people of the Eora Nation. We acknowledge these traditional owners of this land and acknowledge their living cultures and the unique roles they have played in maintaining life, language, and culture in this region. We pay respect to their Elders past, present and emerging and all Aboriginal people.

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CONTENTS	PAGE
1.0 INTRODUCTION	1
1.1 PREAMBLE	1
1.2 AUTHORSHIP	1
1.3 LIMITATIONS	1
1.4 METHODOLOGY	2
1.5 PHYSICAL EVIDENCE	2
1.6 DOCUMENTARY EVIDENCE	2
1.6.1 GENERAL REFERENCES	2
1.6.2 HISTORIC PLANS AND PHOTOGRAPHS	3
1.6.3 HERITAGE LISTING SHEETS	3
1.6.4 PLANNING DOCUMENTS	3
1.7 SITE LOCATION	3
2.0 BRIEF OUTLINE OF THE HISTORICAL DEVELOPMENT	4
2.1 ORIGINAL OCCUPATION	4
2.2 EARLY EUROPEAN HISTORY	4
2.3 EARLY LAND GRANTS	4
2.4 THE FOUNDATION OF MODERN DAY MANLY AND HENRY GILBERT SMITH	6
2.5 RAGLAN STREET AND THE MANLY WARRINGAH AMBULANCE STATION	9
3.0 SITE ASSESSMENT	17
3.1 THE SITE	17
3.2 THE BUILDING - EXTERNAL	20
3.4 THE BUILDING - INTERNAL	21
3.5 THE SURROUNDING AREA	28
4.0 ASSESSMENT OF SIGNIFICANCE	31
4.1 SUMMARY OF HERITAGE LISTINGS	31
4.1.1 STATUTORY HERITAGE LISTINGS	31
4.2 HERITAGE ITEMS IN THE VICINITY OF THE SITE	32
4.3 INTEGRITY	36
4.4 STREETScape CONTRIBUTION	36
4.5 COMPARATIVE ANALYSIS	37
DISCUSSION	38
4.5 ASSESSMENT OF SIGNIFICANCE	38
5.0 SCOPE OF WORKS	44
6.0 EFFECT OF WORK ON THE HERITAGE CONSERVATION AREA	45
6.1 METHOD OF ASSESSMENT	45
7.0 EFFECT OF WORKS ON THE HERITAGE CONSERVATION AREA	46
7.0 EFFECT OF WORKS ON HERITAGE ITEMS IN THE VICINITY OF THE SITE	47
8.0 CONCLUSION	50

1.0 INTRODUCTION

1.1 Preamble

This Heritage Impact Statement for No. 22-24 Raglan Street, Manly, New South Wales, has been prepared to accompany a Development Application to demolish all the structures on the site and construct a four storey mixed use development with basement level parking.

The site is located within the Northern Beaches Council area. The principal planning control for the site is the *Manly Local Environmental Plan 2013 (LEP 2013)*. The site is not listed as a heritage item but is located within the Pittwater Road Conservation Area and is within the vicinity of other heritage items as listed by Schedule 5, Part 1 of this plan. Under Part 5.10 of the LEP 2013, a heritage management document is to be submitted with the development application.

(4) Effect of proposed development on heritage significance

The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).

(5) Heritage assessment

The consent authority may, before granting consent to any development:

- (a) on land on which a heritage item is located, or
- (b) on land that is within a heritage conservation area, or
- (c) on land that is within the vicinity of land referred to in paragraph (a) or (b),

require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item

The appropriate heritage management document in this instance is a Heritage Impact Statement (HIS). This statement has been prepared at the request of the owner of the site and accompanies plans prepared by Carlisle Architects.

1.2 Authorship

This statement was prepared by Patricia Sims, B.Arts, B.Appl.Sc. (Env.Des.), B. Arch., Grad.Dip.Herit.Cons. and James Phillips, B.Sc.(Arch), B.Arch, M.Herit.Cons.(Hons), of Weir Phillips Heritage.

1.3 Limitations

A detailed history of the site and a full assessment of significance to NSW Heritage Division standards were not provided for. The information contained in this HIS was taken from readily available sources. An Archaeology assessment was not provided for.

1.4 Methodology

This assessment has been prepared with reference to the *NSW Heritage Manual* update *Assessing Heritage Significance* (2001).

1.5 Physical Evidence

A site visit was carried out in November 2022. Unless otherwise stated, the photographs contained in this assessment were taken by the authors at that time.

1.6 Documentary Evidence

The following resources were accessed for the preparation of this report:
Mitchell Library, State Library of New South Wales.
Wellings Local Studies Collection, Manly Library.

1.6.1 General References

Attenbrow, Val, *Sydney Aboriginal Past: investigating the archaeological and historical records*, NSW, University of New South Wales Press Ltd, 2002.

Champion, George, *Manly in 1855*, September, 2004. Wellings Local Studies Collection, Manly Library.

Champion, Shelagh and George, *Manly, Warringah and Pittwater: First Fleet Records of Events, 1788-1790*, 1990. Wellings Local Studies Collection, Manly Library.

Connellan, Ian, 'A Place by the Sea: A short history of Warringah' in *Pictures of Warringah*, NSW, Warringah Shire Council, 2002.

Curby, Pauline, *Seven Miles from Sydney: A History of Manly*, Manly Municipal Council, 2001.

Kate Blackmore and Associated Consultants, *Heritage Study: Municipality of Manly*. Unpublished report prepared for Manly Council and the Department of Environment and Planning, April, 1986. Mitchell Library.

Muir, D.K., *Henry Gilbert Smith (1802-1886) of Manly and Wollongong*, NSW, Manly, Warringah and Pittwater Historical Society, 1996.

Sydney Wants to See You! Booklet designed and printed by Waite & Bull, Sydney, under instructions from the Commissioner for Railways, Commissioner for Road Transport and Tramways, the Sydney and Manly Ferry Companies, undated (late 1930s).

The Official Jubilee Souvenir to Commemorate the 50th Anniversary of the Incorporation of the Municipality of Manly 1877-1927, NSW, Manly Council.

Wells, W.H., *A Geographical Dictionary or Gazetteer of the Australian Colonies*, 1848. Facsimile edition published by the Council of the Library of New South Wales, 1970.

Wing, Judy, *Historical Context Report Prepared for the Warringah Heritage Study*.
Unpublished study dated April, 1992.

1.6.2 Historic Plans and Photographs

- NSW Lands Department, *Aerial Photograph over Manly* 1943.
- SIX Maps.

1.6.3 Heritage Listing Sheets

- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. State Heritage Register SHR: 00938.
- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. S.170 NSW State Agency Heritage Register.
- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. State Heritage Inventory Data Base No: 3430433.
- St Mary's Church, Presbytery and School, Whistler Street (corner Raglan Street), Manly, NSW 2095. State Heritage Inventory Data Base No: 2020234.
- Baby Health Care Centre Building, 1 Pittwater Road, Manly, NSW, 2095. State Heritage Inventory Database No: 2020539.
- Pittwater Road Conservation Area, Manly, NSW 2095. State Heritage Inventory Database No: 2020536.

1.6.4 Planning Documents

- Manly Local Environmental Plan 2013
- Manly Development Control Plan 2013

1.7 Site Location

No. 22-24 Raglan Street, Manly is located on the northern side of Raglan Street between Pittwater Road to the west and Whistler Street to the east (Figure 1). The site is identified as Lot 100 D.P. 1009880.

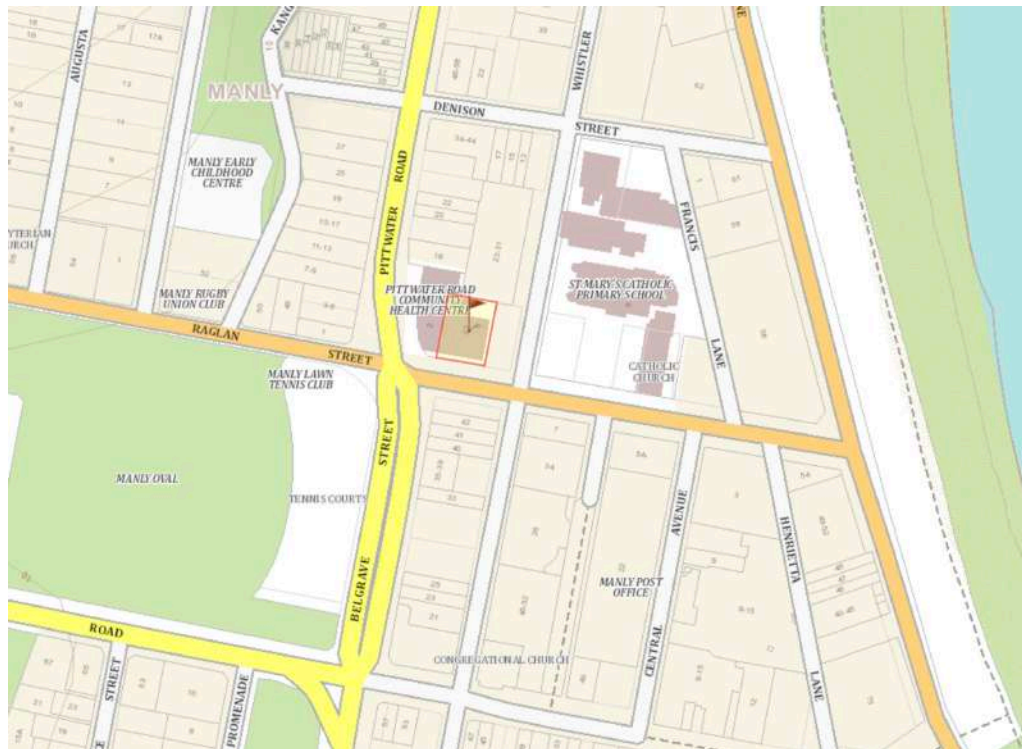


Figure 1: Site Location. Six Maps 2022.

2.0 BRIEF OUTLINE OF THE HISTORICAL DEVELOPMENT

2.1 Original Occupation

While an Aboriginal history is not provided for, it is acknowledged that the original inhabitants of Manly are the Cannagal, Borogegal and Kay-e-my people.¹

2.2 Early European History

European exploration of the North Shore began soon after the settlement of Sydney. On 2nd March, 1788, Governor Arthur Phillip led the first of several explorations into the area that Captain Cook had named Broken Bay. Captain John Hunter in 1792 mapped the coastline between Manly beach and Barrenjoey.

From the 16th January, 1793, successive colonial governors made land grants outside declared boundaries of the township of Sydney in order to open up land and augment the colony's food supplies². The northern shores of Sydney Harbour were located well outside the township boundaries and were largely ignored by the colonists until a series of land grants were made in and around modern day North Sydney in the mid 1790's.

2.3 Early Land Grants

Aside from the activities of the explorers, lime burners and seaman, the coastal area between Manly and Broken Bay was left essentially undisturbed during the first twenty years of European settlement. The inaccessibility and ruggedness of

¹ *Aboriginal Heritage*. Online reference: http://www.kmc.nsw.gov.au/About_Ku-ring-gai/History_heritage/Aboriginal_heritage.

² The right to land grants was only gradually conceded from this date. Land was granted to officers from 16 January, 1793 and to emancipists from 1794.

the landscape was such that the more obvious direction for farming and pastoral activities lay to the west and along the Hawkesbury River Valley.

The first land grants in the modern day Municipality of Manly were made during the brief period following the 'Rum Rebellion' of 1808, when the officers of the New South Wales Corps assumed control of the Colony. The nine grants within the present-day municipal area made before 1842 ranged in size from Charles Chapman's 17 acres (1842) to D'Arcy Wentworth's substantial 380 acre grant (1818). The first small lot subdivision in the Manly area occurred when Surveyor General Mitchell laid out the Village of North Harbour (later called Balgowlah) at the head of North Harbour in 1826.

Among the grants made before 1842 was a 100 acre grant made to John Thompson on 13/4/1842. It is upon a part of this grant that the subject property now stands (Figure 2). By this time, land, although still often termed grants, had to be purchased. What, if anything, Thompson used his grant for is not known.

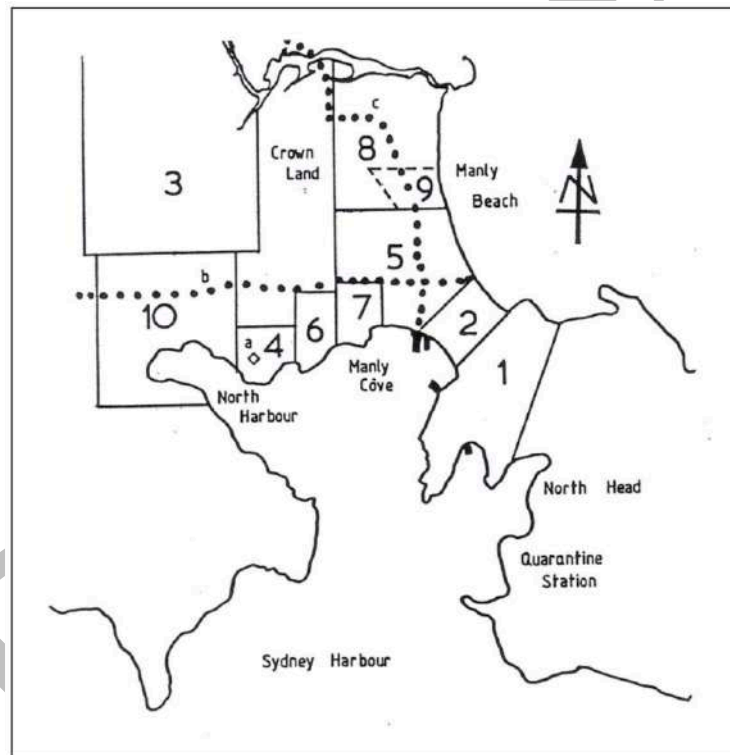


Figure 2: Grants in the manly area 1810-1850. Heritage Study of the Municipality of Manly, 1986.

1. Richard Cheers, 100 Acres, 1/1/1810.
2. Gilbert Baker, 30 acres, 1/1/1810.
3. D'Arcy Wentworth, 380 acres, 25/7/1818.
4. Jn Crane Parker, 20 acres, 12/4/1837.
- 5. John Thompson, 100 acres, 13/4/1842.**
6. Charles Chapman, 17 acres, 18/4/1842.
7. John Wheeler, 19 acres, 18/4/1842.
8. C. Andrews and Co. Scally, 100 acres, 9/5/1842.
9. H.G. Smith, 20 acres purchased 1853. Area marked out as the 'Village of Balgowlah', c.1831.

‘a’ marks Henry Smith’s villa, *Fairlight*, constructed in 1854; ‘b’ marks the Sydney Road; ‘c’ marks the Pittwater Road.

Manly remained sparsely populated well into the mid nineteenth century. Access into the area during the early nineteenth century was by means of an early track, possibly an Aboriginal pathway, first shown on a parish map of 1842. This track led from Manly Cove to the Head of Curl Curl Lagoon.³ Only 29 residents were listed in the censuses of 1841 for the Manly/North Harbour area; W.H. Well’s *Gazetteer of the Australian Colonies*, published in 1848, indicates that there were only 24 houses and 63 inhabitants in the whole of the Parish of Manly Cove at this time.⁴

The slow development of the area during the first half of the nineteenth century cannot solely be attributed to its distance from Sydney, given that settlers had reached as far west as Cootamundra by the 1830s. Several factors limited development. Agricultural potential was perceived to be low and the area was isolated from potential markets. One family, the Wentworth family consolidated a substantial estate and did little to develop it. The decision to establish the Quarantine Station on North Head in 1832 removed another large parcel of land from development.

2.4 The Foundation of Modern Day Manly and Henry Gilbert Smith

The establishment of a permanent ferry service at The Spit in 1850 improved access into the Manly area. Over the following ten years, the businessman and entrepreneur Henry Gilbert Smith pieced together a sizeable estate through purchase and lease. Thompson’s grant, and thus the subject property, was the first of Smith’s purchases (March 1853). By 1856, Smith had consolidated an estate of over 200 acres, to he added a further 130 acres of leased land to the south of The Corso.⁵ Smith did not reside permanently on his Manly estate but visited periodically to stay in a stone cottage already on Thompson’s grant when he acquired it in 1853. During the following decade, he would construct two more residences for his personal use, the first a modest cottage designed by Edmund Blacket in 1855 and the second, the much larger and grander villa, *Fairlight*, in 1860.

Smith acquired his Manly estate at a time when Sydney was booming; between 1850 and 1890, the population of Sydney increased sevenfold. Impressed with the natural environs of Manly, Smith marketed lots on his estate under the name *Ellensville-Montpellier* (1855). Refer Figure 3.

‘...to give such a character to these Marine Retreats that they may become the favourite resort of the Colonist...’⁶

In September 1855, he constructed a pier at Manly Cove and began the first regular ferry services between Sydney and Manly.

³ ‘Pittwater Road Conservation Area’, Listing sheet from State heritage Inventory (NSW)>

⁴ W.H.Wells, A geographical Dictionary or Gazetteer of the Australian Colonies, 1848. Muir estimates, using census returns, that there were only 24 people in modern day Manly.

⁵ D.K. Muir, Henry Gilbert Smith, 1996, p.35.

⁶ Cited in Kate Blackmore and Associated Consultants, Heritage Study of Municipality of Manly. Unpublished study prepared for Manly Municipal Council and Department of Environment and Planning, April 1986. Volume 2, p.95.

Smith's vision was just that, a vision. *Ellensville* and *Montpelier* were private villages. The Municipality of Manly would not be formed until 1877, leaving Smith solely responsible for the development of essential services, such as roads, footpaths, garbage and night soil collection. Smith required capital from land sales to provide such services. The first allotment of land, however, was not sold until 1859, over four years after the land had been offered for sale. The area was thus considered not only too far from Sydney, but also under serviced.

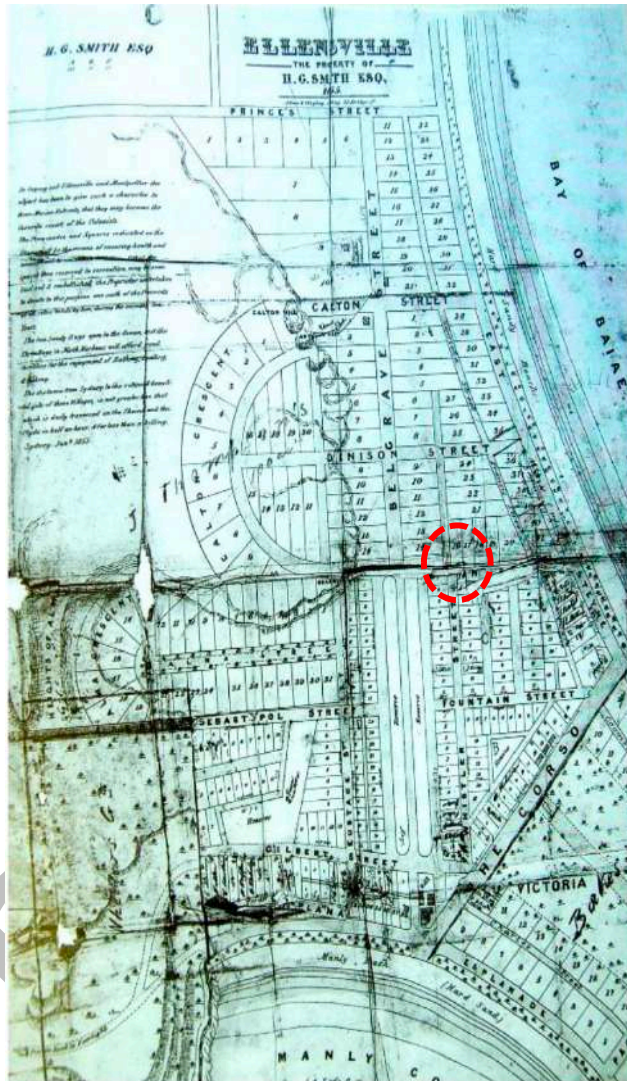


Figure 3: H.G. Smith's first sub-division plan in Manly in January 1855, Source Pittwater Road, Conservation Are, Manly Final Draft History. Tropman and Tropman Architects. Ian Jack Heritage Consulting Pty Ltd January 2017.p12. Site noted in red.

Despite the lack of permanent settlers, Manly did become a popular place for day trippers. Visitors were attracted to Manly because of its undisturbed natural surrounds, for shooting and fishing parties.

By the time that the first block of land was sold at Manly, Smith had abandoned his earlier plans for *Ellensville*. The plans had been revised and offered for sale under the name *Brighton* or *New Brighton*, refer Figure 4. One visitor questioned the tenuous connection of Smith's estate to England's Brighton sea side resort:

‘Manly Beach is known as the Brighton of Australia. Why it would be so called is a mystery for though the trip of steamer across Sydney Harbour to Manly is really beautiful, there is no more resemblance between Manly and our Brighton than Between Sydney Harbour and the Grand Canal....The arrangements upon its beach, however, are worthy of a pantomime. There is a row of structures resembling sheep-pens, each of which encloses some presumably valuable, though diminutive, botanical treasures...’⁷

This more modest second scheme comprised 422 allotments arranged in eight sections and remains discernable in the street pattern of Manly today. The block pattern is rectilinear with wide streets arranged on a grid pattern. When offered to the public in 1858, sales on the Brighton Estate were slow. Only 15 blocks sold in the period to December 1863, for a total of £1,794.⁸ A generous man, Smith donated land to churches or sold some allotments for nominal pepper corn rent of 10 shillings. Tourism continued to remain the most important activity in this phase of Manly’s development. Hotels, boarding houses, and private residences leased to holiday makers thrived as Manly developed a reputation as a sanatorium.

During the period 1850 to 1870 the population of Manly fluctuated considerably between the busy summer periods and quiet winters. There were only 32 entries under ‘Manly or New Brighton’ in *John Sands’ Sydney and Suburban Directory* of 1861. Over the following twenty years, the permanent population began to increase; the number of listings in *Sands’ Directories* increased from 70 in 1870, to 80 in 1875 and 166 in 1880.⁹ During this period, the Municipality of Manly was declared (1877), bringing with it the hope that better amenities would follow. Henry Smith died in Brighton, England in 1886, leaving his Australian property in Trust for his three young sons. The Trustees continued to sell the remaining land in Manly.

Elsewhere in the Municipality during the latter part of the nineteenth century, Crown lands and other private subdivisions had also been offered for sale. Allotments on the *East Brighton Estate* were advertised in 1877 and 1879. The *Bassett-Darley Estate*, between Manly Cove and Cabbage Tree Bay, was advertised in 1882, followed, in 1883, by the *Buckingham Park Estate* on the northern half of the Manly Golf course. Other sales included the Cavendish Park Estate (1885), *Chapman’s Estate* at Fairlight (1885), *Carrington Heights* at Balgowlah (1886) and the *Dobroyd Estate* (1887-1888).¹⁰ While a great deal of clearing was carried out, this subdivision activity did not necessarily result in extensive building activity. Manly did, however, acquire a ‘small but considerable gentry’ during the latter part of the nineteenth century, who built a variety of villa retreats.¹¹ At the other end of the social scale, contemporary accounts suggest that a considerable proportion of the population of Manly lived in sub-standard housing.¹²

Subdivision activity peaked in Manly in the late 1880s before sales slumped with the onset of the economic depression of the early 1890s. While sales once again

⁷ H.Furniss, *Australian Sketches Made on tour, 1899* cited in Kate Blackmore and Associated Consultants, op.cit., April, 1986, Volume 2, p.97.

⁸ D.K.Muir, op.cit., 1996, p.41.

⁹ Pauline Curby, op.cit., 2001, p.66.

¹⁰ Kate Blackmore and Associated Consultants, op.cit. April, 1986, Volume 2, pp111-12.

¹¹ Ibid, p.124.

¹² Ibid, pp.60 onwards.

improved towards the turn of the century, they would never achieve the heights of the 1880s.

Development during the late nineteenth century was never even across the district; for some time, there were comparatively few buildings beyond the village centre and the surrounding high ground. By this time, the Municipality boasted a population of around 3,000 people. There was a total of 585 buildings, the most notable of which were the imposing sandstone forms of the Cardinal's Palace and St. Patrick's College. Despite the development that had occurred during the 1880s, Manly still enjoyed a comparatively low population density when compared to other Sydney Municipalities. Manly had only 0.68 people per acre; Paddington, by contrast, had 26.38 people per acre and Darlinghurst, 44.11 people per acre.

2.5 Raglan Street and The Manly Warringah Ambulance Station

Despite the establishment of the first regular ferry services to Manly Cove in mid-1855 and despite the construction of the first wharf by Smith himself, close to his hotel, Ellensville did not attract purchasers at this time. Smith abandoned the 1855 Ellensville Plan and created a new village called Brighton instead which were offered for sale in 1860. Brighton included more or less the same area as Ellensville. Raglan Street was not shown on the Ellensville plan but did appear on the Brighton plan.

As a result of the promotional activities of Smith and others in the late 1850s, there was substantial public interest in the Brighton sale. The 1860 map, Figure 4, is annotated with the names of many purchasers, and most of the allotments in Belgrave Street (now Pittwater Road) within the study area were gradually sold, some in groups to a single investor, which led to future sub-division (*Figure 6*).

As a result of Smith's activities, the state government agreed in June 1860 to develop a public road from Manly to Pittwater, including part of Smith's Belgrave Street and its extension, Pittwater Road. Although it took many years for a satisfactory road to be created, the gazettal in 1860 was a critical step in the history of the study area.

Population grew, but was at first largely located near Manly Cove, along The Corso and along the ocean beach. The area was still largely uncleared bushland at that time, with an increasing number of allotments sold but not yet developed. This is shown in an 1875 drawing (*Figure 4*).

Henry Gilbert Smith left Manly, and Australia, quite soon after sub-dividing much of Brighton. His second wife died in 1866 at Fairlight, their dignified Manly home (which was demolished in 1939). The three sons of the marriage were now aged between three and eight, Smith quickly decided that he should return to Britain and educate his sons there. Once in England Smith married for the third time and never returned to Australia. Very appropriately, he died in the English Brighton in 1886. The three sons did not return to Australia after their father's death.

After 1869, Smith's agents began to offer his Manly property for sale, initially his mansion at Fairlight and some tourist premises around Manly Cove. This was followed by sales and resales of land which he had already put on the market in Brighton. The entire block, he owned, bounded by Raglan, Belgrave, Denison and Whistler Streets which includes the subject site, was recast into 19 'villa sites' in 1877 but was acquired by only three purchasers.

A solitary villa site on the corner of Denison and Whistler Streets was sold to E. Tremain, while a member of the Windeyer family purchased the other five northern allotments. The entire southern segment, containing thirteen villa sites, was acquired by G. Everett, a Pyrmont businessman.

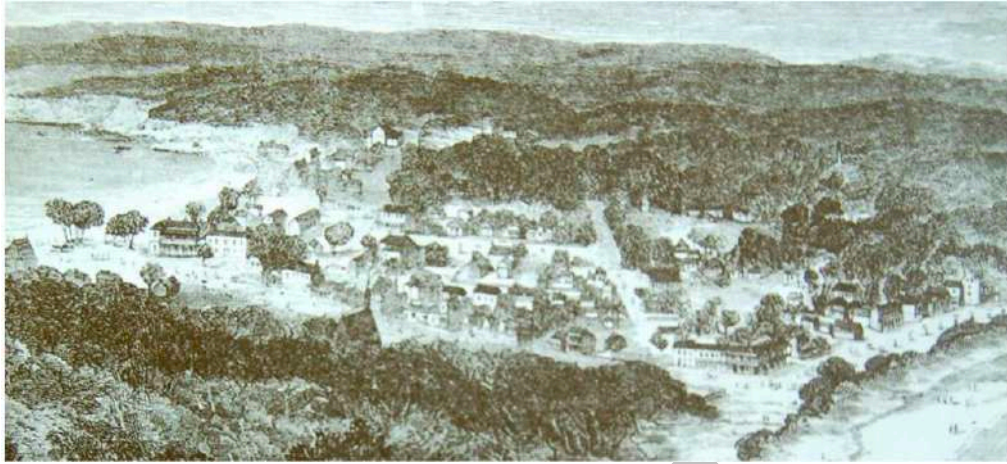


Figure 4: Manly in 1875, looking north west. Manly Cove is on the left, Manly beach on the right, connected by the Corso. Source Pittwater Road, Conservation Are, Manly Final Draft History. Tropman and Tropman Architects. Ian Jack Heritage Consulting Pty Ltd January 2017.p15

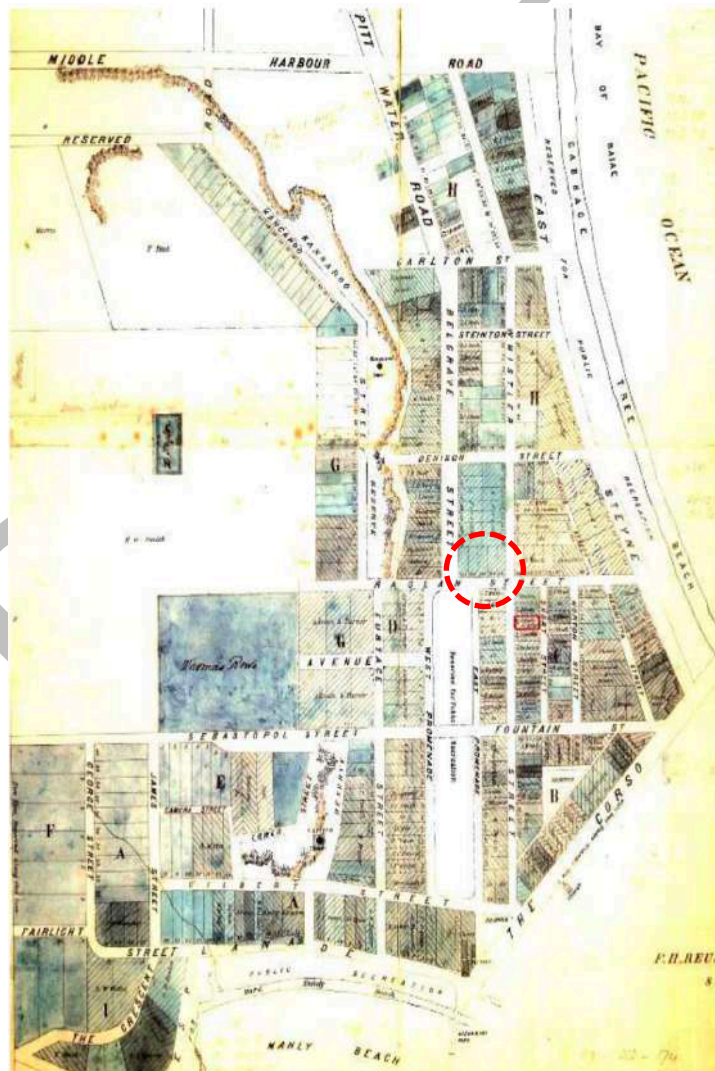


Figure 5: H.G. Smith's sub-division plan of Brighton, 1860. Source Pittwater Road, Conservation Are, Manly Final Draft History. Tropman and Tropman Architects. Ian Jack Heritage Consulting Pty Ltd January 2017.p14.

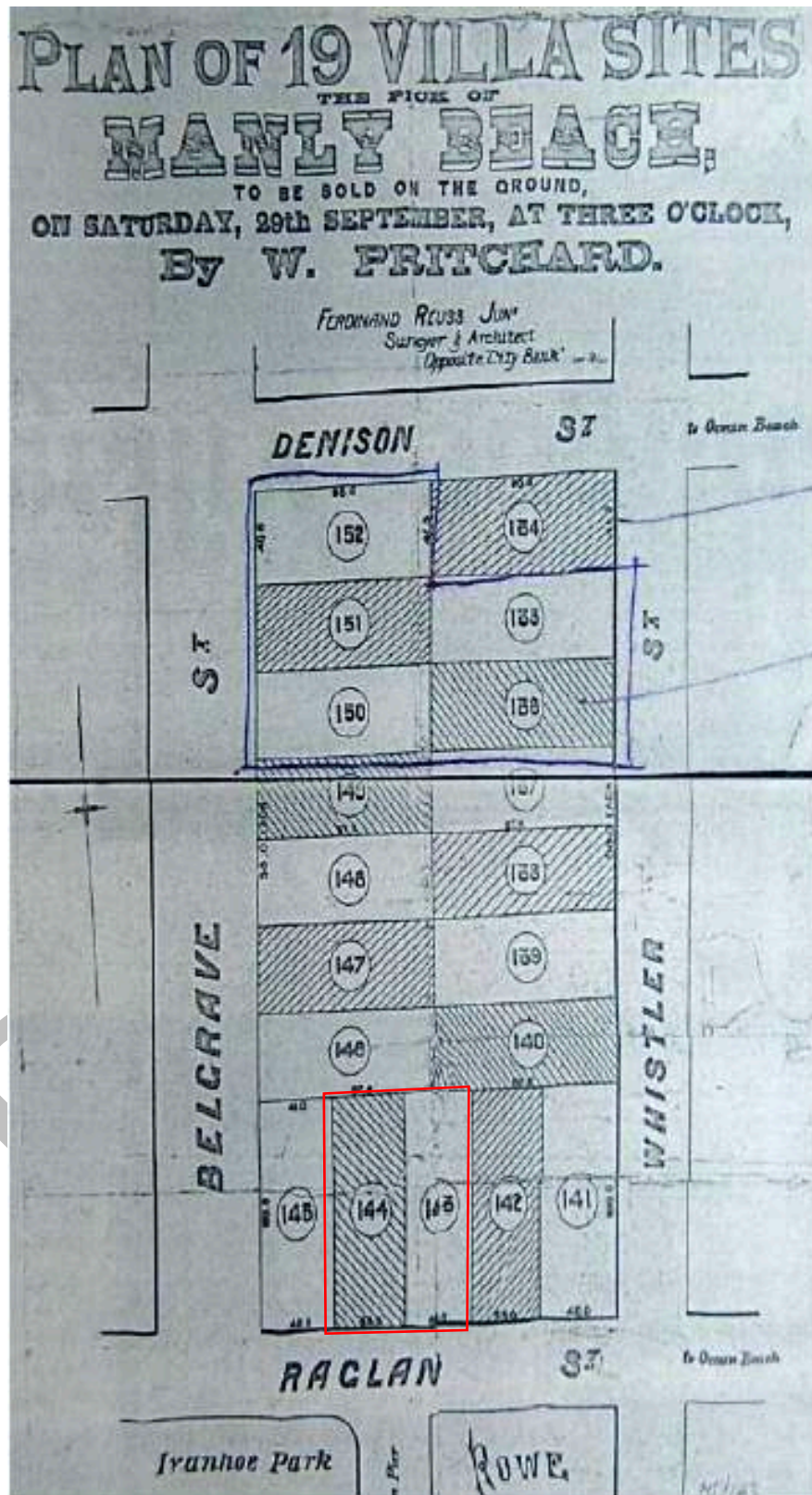


Figure 6: Subdivision in 1877 of the block of vacant land bounded by Denison, Whistlet, Raglan and Belgrave Street. This part of Belgrave Street is now Pittwater Road. Source Pittwater Road, Conservation Area, Manly Final Draft History. Tropman and Tropman Architects. Ian Jack Heritage Consulting Pty Ltd January 2017.p17. Site noted in red.

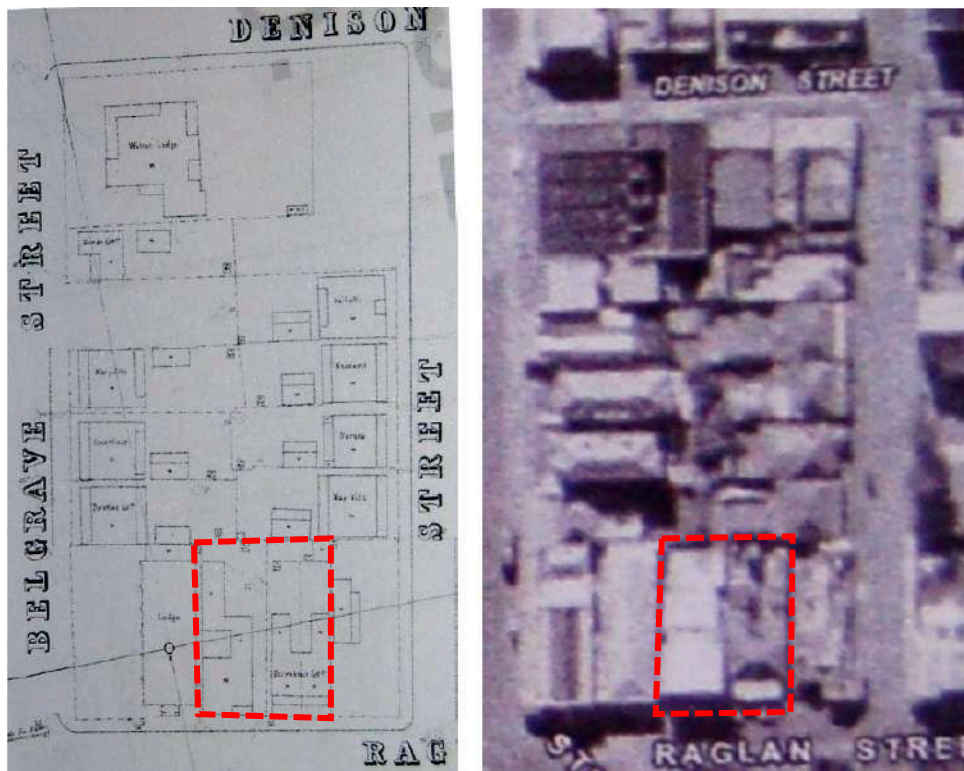


Figure 7: Development of the 1877 sub-division by 1891 (left) and by 1943 (right)
Source. (a) Water Board plan, 1891, Manly, sheet 29; (b) LPI, aerial photograph, 1943. The site is outlined in red.



Figure 8: 1943 aerial. Six Maps

As a result, some early sub-divisions did not settle into their present aspect for many years. Tremain did not build at all initially on his single villa lot, but Walton Lodge was built on the corner of Pittwater Road and Denison Street and there was another cottage built on the Windeyer purchases. Everett's large investment resulted by

1891 in four villas on Whistler Street, and three on Belgrave Street (Pittwater Road) along with institutional buildings on Raglan Street. The Sydney Sands Directory records, In 1879 The Manchester Unity Order of Oddfellows Hall on the corner of Pittwater Road and Raglan Street refer Figure 9, however the Manly Waterboard Plan of 1891 does not record it. In 1885, The Sydney Sands Directory registers two residents on Raglan Street between Belgrave Street and Whistler Street. By 1905 the Order of Oddfellows shared the Hall with the Wesleyan Church and the Manly School, it appears that three of the five blocks fronting Raglan Street were built upon at this time.

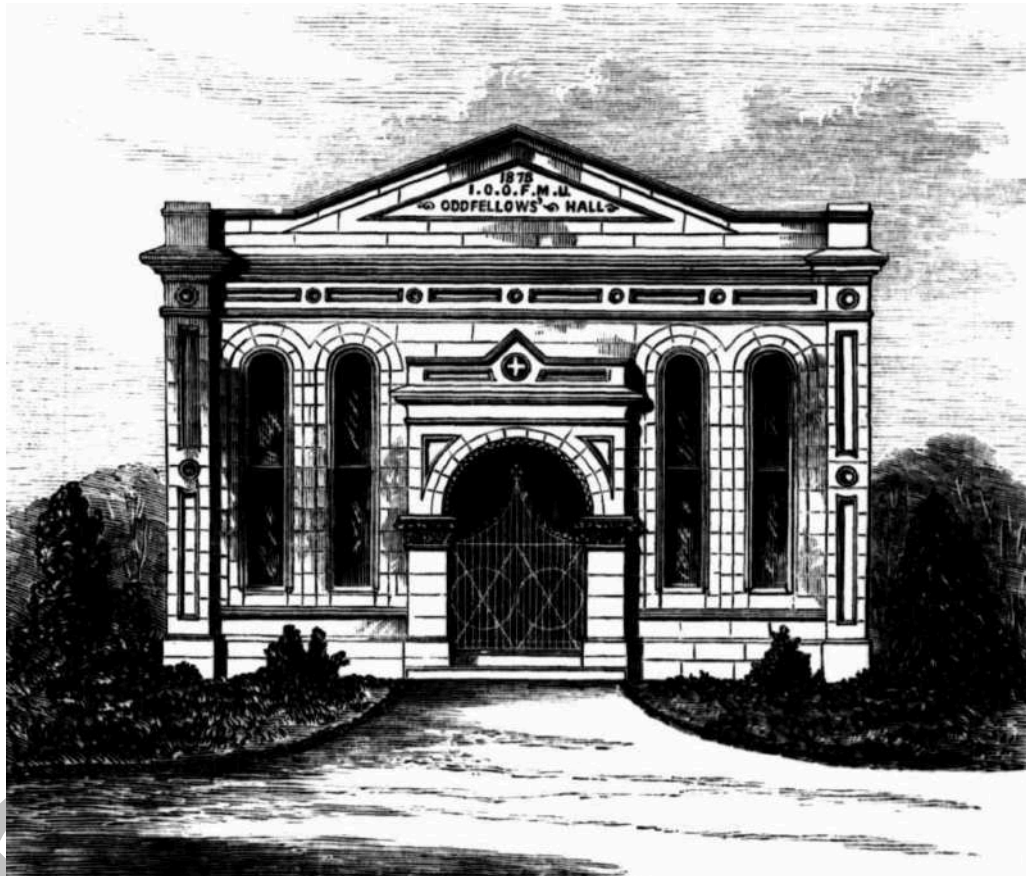


Figure 9: Image of Manly Oddfellows Hall built in 1878 on the Corner of Raglan and Belgrave Street. Trove, SMH 5th October 1878.

The Methodist church occupied the Oddfellows Hall from 1910 to 1924. After this date its use becomes unclear but it was possibly used for dance and singing classes. Whitelaw Flats, located next to The Oddfellows Hall first appears in the directory from 1926. The subject site first appears in the directory in 1895 as a grocery store and appears to have been used for that purpose until 1933, the last edition of the directory. In 1935 construction of the new Manly Ambulance Station began on the site and was opened in January 1936 by State premier Bertram Stevens. The Station replaced an old one on Balgowlah Road and was designed by Trenchard Smith and Maisey. Trenchard Smith and Maisey were prominent Architects of the time designing the Manly Town Hall, The Warringah Town Hall, The Hordern Pavilion and Manly Electrical Substation. Figures 11-ss- illustrate these buildings.

The Manly Ambulance Station became obsolete in 1986 and was sold in 1989. It reopened as the Manly Backpackers and still operates as this today.



Figure 10: Manly Town Hall 1937. Trenchard Smith and Maisey Architects. Wikipedia



Figure 11: Warringah Town Hall 1923. Trenchard Smith and Maisey Architects. Wikipedia



Figure 12: The Hordern Pavilion 1924. Trenchard Smith and Maisey Architects.



Figure 13: Manly Electrical Substation 1920, later extended in 1935.

The Manly Warringah District Ambulance Station had a simple and symmetrical elevation to Raglan Street and was representative of the Inter- War Stripped Classical Style. Built of face brickwork with brick detailing around the multi paned double hung windows. The Ambulance garage door was a timber framed bifold door with multi glass panes in the top section and framed in decorative brickwork. The Symmetrically placed above the garage door was a recessed balcony set into the front elevation with a balustrade of vertical painted steel rods, set between two classical columns supporting three brick arches. Figure 14 to 17 illustrate the original Ambulance Station.

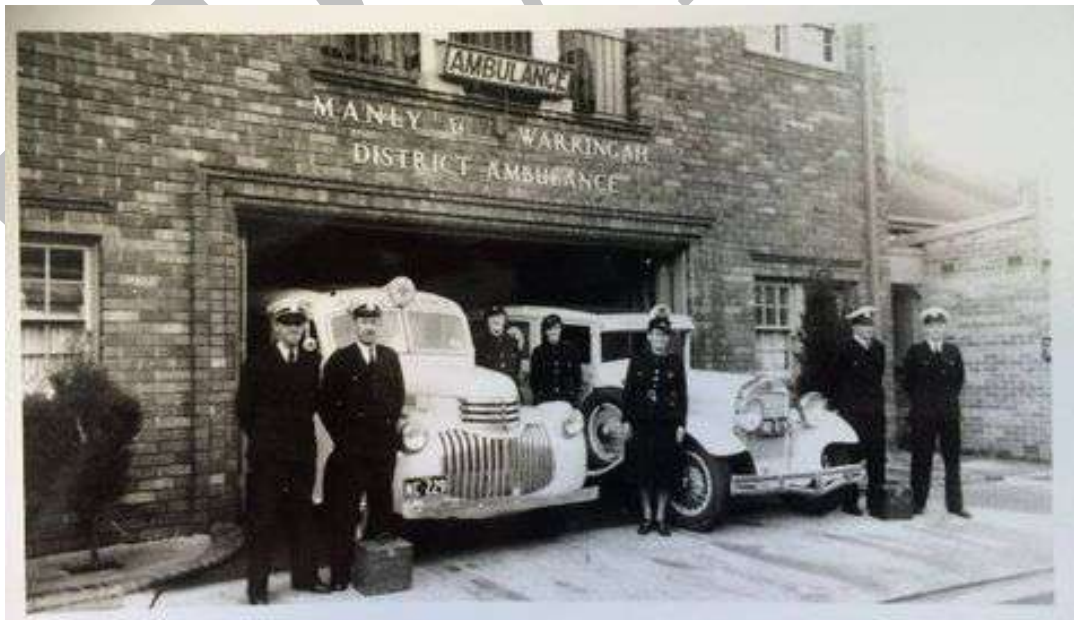


Figure 14: The site No. 22-24 Raglan Street, Manly. Manly Warringah District Ambulance Station. Undated.



Figure 15: The site No. 22-24 Raglan Street, Manly. Manly Warringah District Ambulance Station. Undated.



Figure 16: The site No.22-24 Raglan Street, Manly. Manly Warringah District Ambulance. Source Manly Local Studies Image Library. File: 001/001487. 1957.



Figure 17: The site No. 22-24 Raglan Street, Manly. Manly Warringah District Ambulance Station. 1988.

3.0 SITE ASSESSMENT

3.1 The Site

For the following, refer to Figure 18, an aerial photograph over the site and to the survey that accompanies this application.

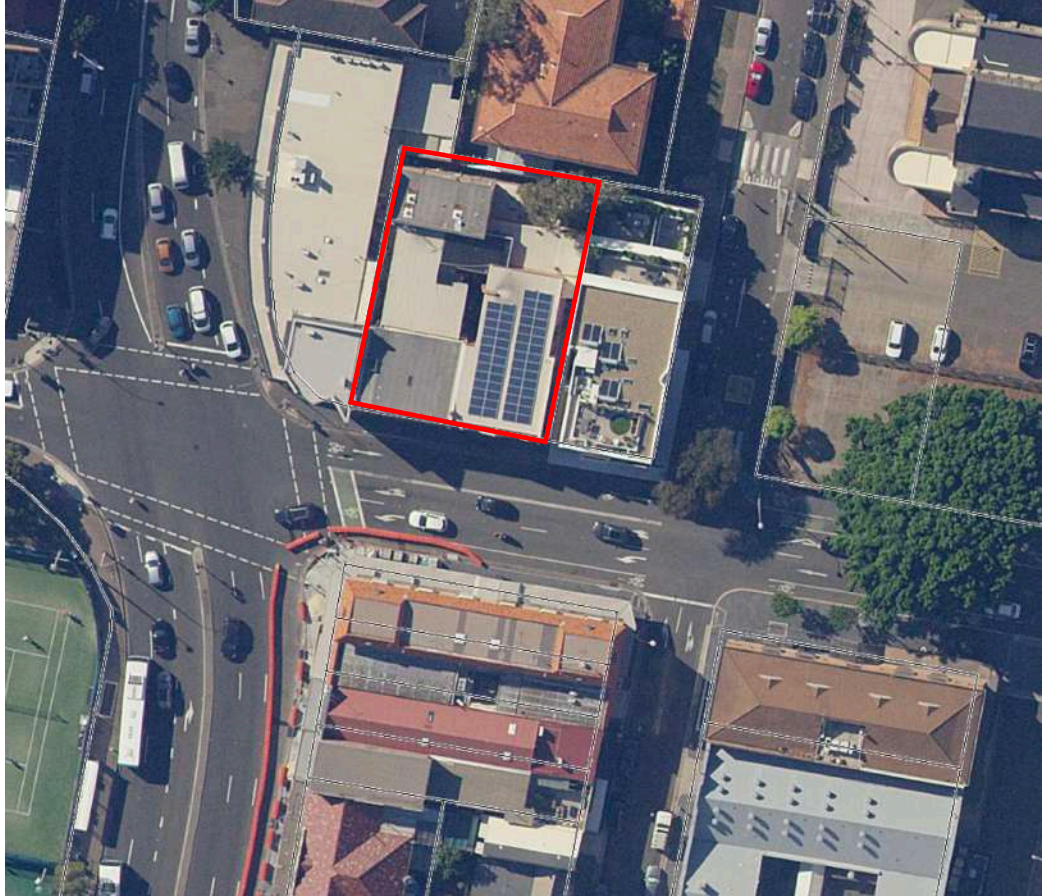


Figure 18: Aerial photograph of the Site. The is outlined in red. SIX Maps.

The site is located on the northern side of Raglan Street, on the block between Pittwater Road to the west and Whistler Street to the east. The site is predominantly flat and is rectangular in shape.

The site consists of two separate adjoining buildings that have been amalgamated into the one single use structure as a backpacker's hotel. The western building is the highly modified former Manly and Warringah District Ambulance Station, the eastern lot was constructed in 2000. Only the southern, Raglan Street elevation of the site is visible from the public domain, the other elevations are screen by adjacent structures. The building described below takes up most of the site area apart from a narrow front and rear setback.

Figures 19 to 22 illustrate the general character of the site.

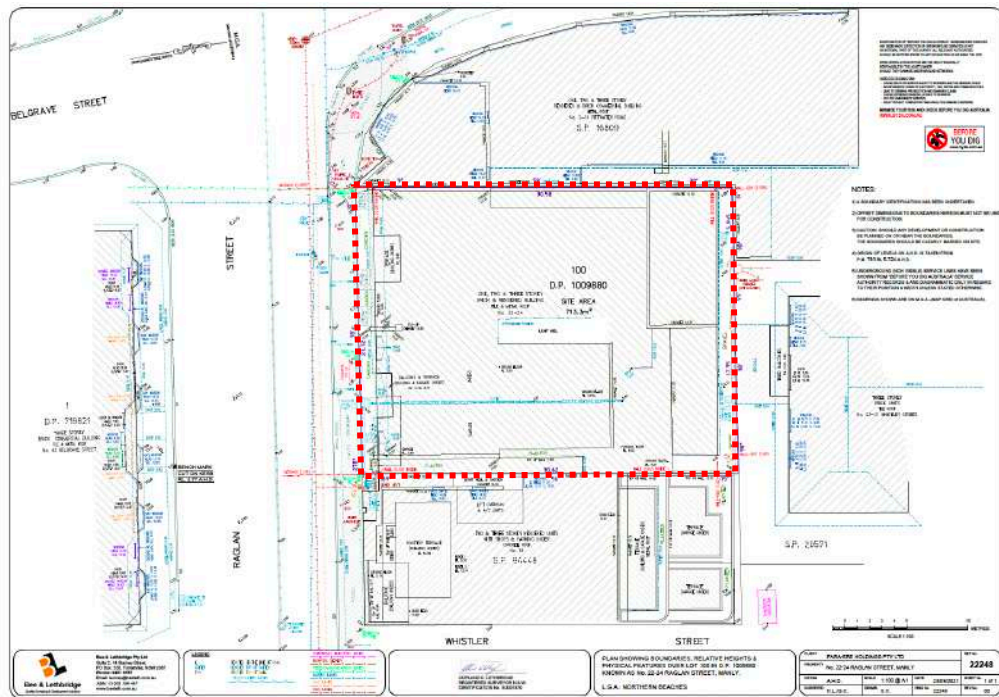


Figure 19: Survey of the site. Bee and Lethbridge 2021. The site is outlined in red.



Figure 20: The front elevation, No.22-24 Raglan Street. WPH photograph.



Figure 21: Photo taken from Whistler Street looking south west toward the rear of subject site. This photo illustrates that the rear of the site is not visible from the public domain. The Arrow points to the site.



Figure 22: Photo of the rear elevation of then site and rear setback to northern boundary.

3.2 The Building - External

The site is setback from the front boundary and predominant urban street wall along Raglan Street by a narrow concrete setback punctuated with a small garden beds. There is a narrow setback of unbuilt upon area along the full extent of the rear boundary. The west and east elevations are hard to the boundary at ground level. There is a setback at level one and above to the east elevation which is punctuated with aluminum framed windows. The western elevation is hard on the boundary over all levels. There is a lightwell between the two buildings at ground and level one.

The entry to the backpackers is via the former Ambulance Station Garage door which has been infilled with glass panels and doors. The former Ambulance Station elevation is rendered and painted with two aluminium framed windows on level one. Central to the windows and the old garage opening below is a recessed balcony with two classical columns supporting a tripled arched lintel. The balustrade is painted steel rods. The roof is built between two expresses party walls and is a combination of terracotta tiles facing Raglan Street and a series of steel skillion roof planes to the rear.

The eastern building was constructed in 2000 to look similar to the former Ambulance building. The Raglan Street elevation is rendered and painted and has a large garage door to provide access to on-grade parking under the building. At level one there are aluminum framed sliding windows either side of a recessed balcony similar in style and proportions to one on the former Ambulance building. The recessed balcony is duplicated on the level above and has a gable roof with similar columns and triple arch. The roof is built between two expressed party walls with a terracotta tiled rake roof facing Raglan Street and a series of steel skillions to the rear. The front roof plane is punctuated with dormer windows set either side of the gable roof form over the balcony.



Figure 23: No.22-24 Raglan Street along the northern rear boundary.
Weir Phillips Heritage.

3.4 The Building - Internal

The interior is best understood by the internal floor plans as illustrated in figures 24-27.

Internally the former Ambulance Station building has been completely modified for use as a backpacker accommodation, services and facilities. On the ground level is a reception area. Behind the reception area a central corridor provides access to storerooms, accommodation and the on-grade parking area under the eastern building on the site. The stair up to the first floor branches to the east and west to service accommodation, communal bathrooms, laundries, kitchen, lounge, dining and entertaining areas.

Figures 24-43 illustrate the interior of the building.

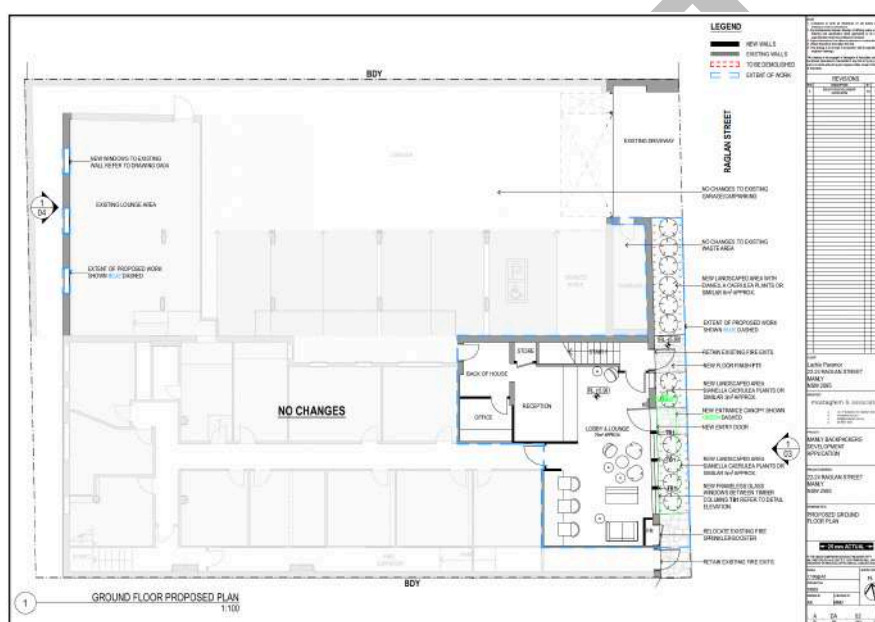


Figure 24: No.22-24 Raglan Street existing ground floor plan. Mostaghim and Associates.



Figure 25: No.22-24 Raglan Street existing first floor plan. Mostaghim and Associates.



Figure 26: No.22-24 Raglan Street existing second floor plan. Mostaghim and Associates.



Figure 27: No.22-24 Raglan Street existing third floor plan. Mostaghim and Associates



Figure 28: No.22-24 Raglan Street, Backpacker entry lobby off Raglan Street. Weir Phillips Heritage.



Figure 29: No.22-24 Raglan Street, Backpacker lobby.
Weir Phillips Heritage.



Figure 30: No.22-24 Raglan Street, Backpacker entry lobby and hallway.
Weir Phillips Heritage.

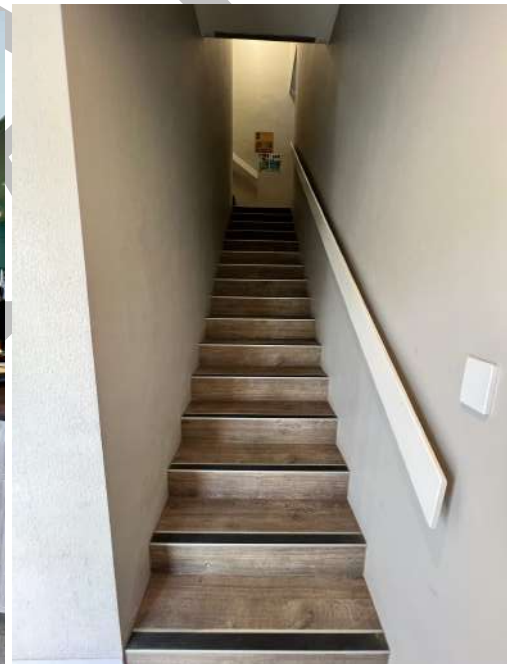


Figure 31: No.22-24 Raglan Street, Stair up to first floor.
Weir Phillips Heritage.



Figure 32: No.22-24 Raglan Street, Typical bedroom.
Weir Phillips Heritage.

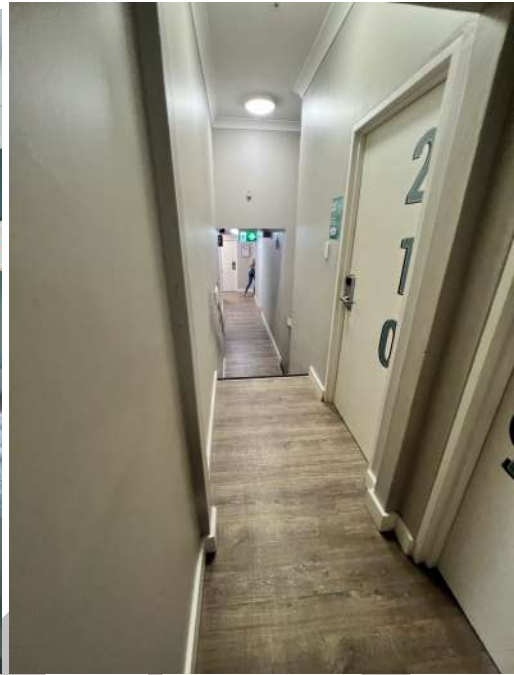


Figure 33: No.22-24 Raglan Street, first floor hallway.
Weir Phillips Heritage.



Figure 34: No.22-24 Raglan Street, Dining and lounge area.
Weir Phillips Heritage.



Figure 35: No.22-24 Raglan Street, open outdoor area at first floor.
Weir Phillips Heritage.



Figure 36: No.22-24 Raglan Street, Backpacker dining area and kitchen.
Weir Phillips Heritage.



Figure 37: No.22-24 Raglan Street, Backpacker kitchen looking out into the outdoor area on the first floor.
Weir Phillips Heritage.



Figure 38: No.22-24 Raglan Street, laundry facilities. Weir Phillips Heritage.

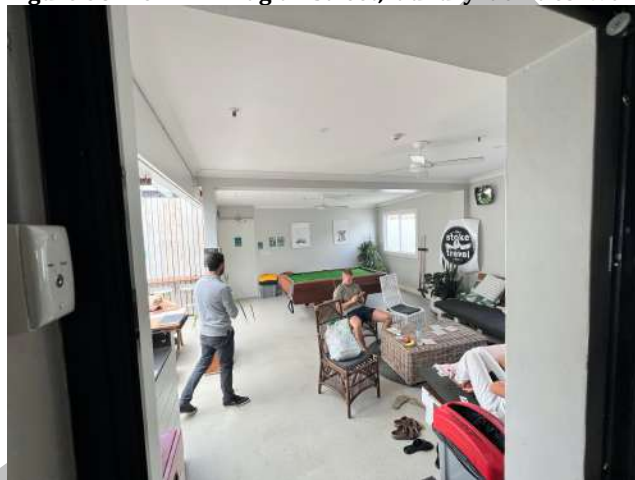


Figure 39: No.22-24 Raglan Street, First floor Lounge and entertaining area. Weir Phillips Heritage.

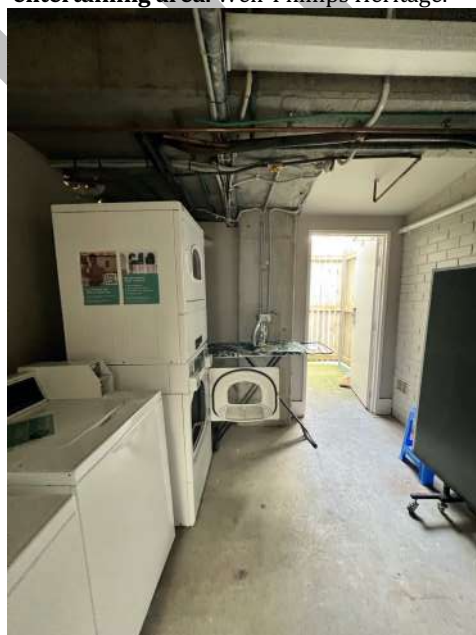


Figure 40: No.22-24 Raglan Street, Ground floor Laundry. Weir Phillips Heritage.



Figure 41: No.22-24 Raglan Street, Typical Bathroom. Weir Phillips Heritage.



Figure 42: No.22-24 Raglan Street, setback between the east and west buildings on the site. Weir Phillips Heritage.



Figure 43: No.22-24 Raglan Street, carpark, under the eastern building. Weir Phillips Heritage.

3.5 The Surrounding Area

For the following, refer to Figure 44, an aerial photograph of the site and its surrounds.

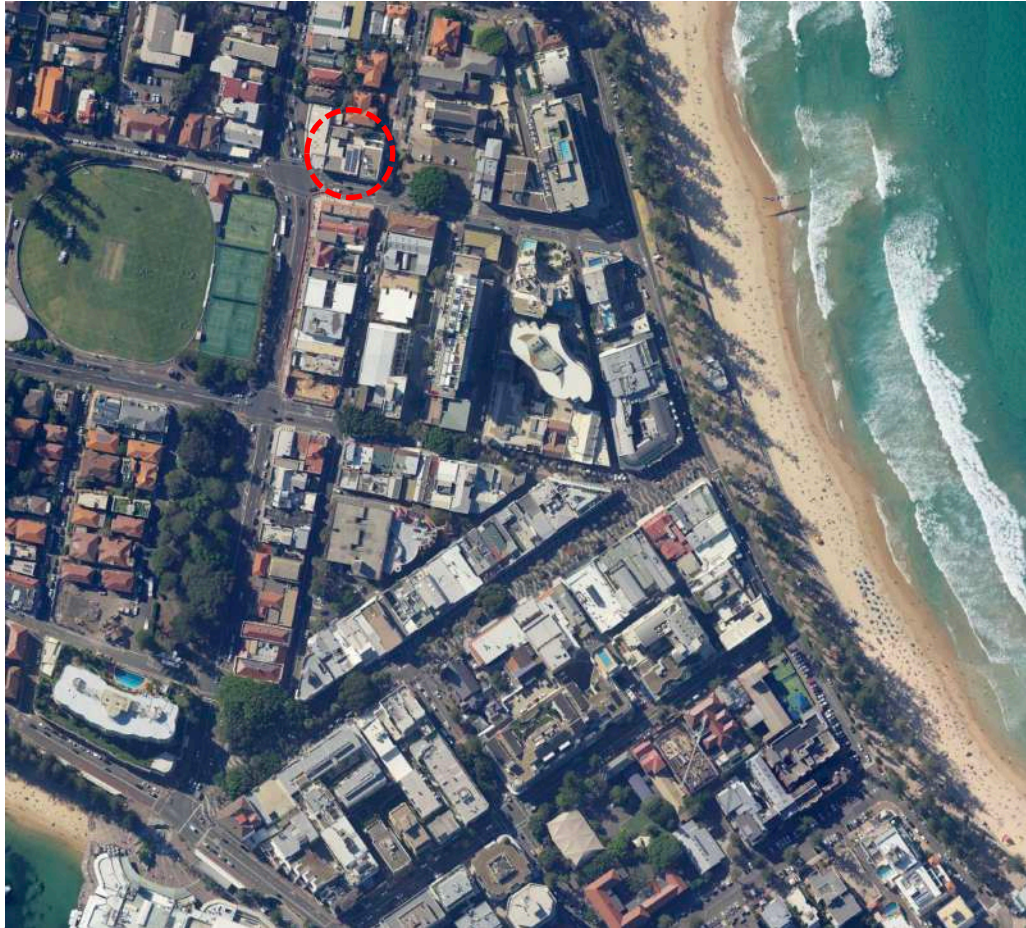


Figure 44: No.22-24 Raglan Street, Manly, noted with a red circle. Six Maps 2022.

The site is located on the northern edge of the Manly commercial centre in the strip of land located between the ocean to the east and the harbour to the south west and is approximately 500m west of Manly Beach. In the section of the subject site, Raglan Street is a triple carriageway and cycle path with no stopping to either side.

The area is a dense and architecturally diverse mix of low to mid rise commercial and residential. To the east of the site and on the corner of Whistler Street is a contemporary three storey residential building with ground floor retail overlooking the St Mary's Catholic Primary School, including carpark, grounds and buildings. Immediately west of the site is a two and three storey contemporary building. Opposite the site on Raglan Street is a three storey commercial building. Diagonally opposite the site on Pittwater Road is Manly Oval, including tennis courts and small service buildings.

The height and density of buildings increases to the south, toward the main commercial precinct.

Figures 45-35 illustrates the surrounding area.



Figure 45: No.22-24 Raglan Street, front elevation. Weir Phillips Heritage.



Figure 46: Looking north east toward the Church and carpark from the corner of Raglan and Whistler Street. Weir Phillips Heritage.



Figure 47: Looking north east along Raglan Street toward the beach. Weir Phillips Heritage.



Figure 48: Looking south west along Raglan Street.
Weir Phillips Heritage.



Figure 49: Looking west along Whistler Street and the rear of the site.
The arrow marks the site. Weir Phillips Heritage.

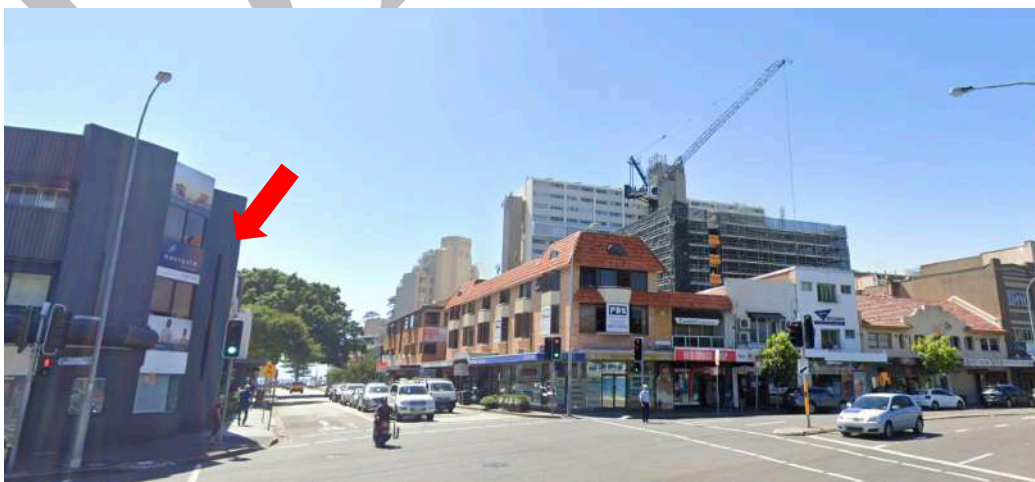


Figure 50: Looking east along Raglan Street toward Manly Beach.
The arrow marks the site. Weir Phillips Heritage.



Figure 51: Looking north east at the corner of Raglan Street and Pittwater Road.
The arrow marks the site. Weir Phillips Heritage.

4.0 ASSESSMENT OF SIGNIFICANCE

4.1 Summary of Heritage Listings

4.1.1 Statutory Heritage Listings

No.22-24 Raglan Street Manly:

- Is not listed as a heritage item by Schedule 5, Part 1 of the *Manly LEP 2013*.
- Is listed within the Pittwater Road Heritage Conservation Area by Schedule 5, Part 2 of the *Manly LEP 2013*.

The site is:

- Not listed on the State Heritage Register under the auspices of the *NSW Heritage Act 1977*.

Pittwater Road Conservation Area

Pittwater Road Conservation Area, Manly, NSW 2095. State Heritage Inventory Database No: 2020536.

The State Heritage Inventory provides the following Statement of Significance for this item and has been relied upon for this Report:

This street pattern is distinctive and underpins the urban character of the area. The streets remain unaltered in their alignment, although the names of Malvern, Pine and North Steyne are now names for what were Whistler, Middle Harbour and East Steyne respectively.

The Manly DCP provides the following statement of significance for the Pittwater Road Conservation Area.

- a) *The Pittwater Road Conservation Area street pattern is distinctive and underpins the urban character of the area. This street pattern comprises the alignment, detailing and silhouette of the street facades and the overall scale of building in Pittwater Road is important. The streets remain unaltered in their alignment, although the names of Malvern, Pine and North Steyne are now names for what were Whistler, Middle Harbour and East Steyne respectively. In particular, the streetscape along Pittwater Road from Dennison Road to Collingwood Street is a fine example of a remaining vista of the early settlement period in the Municipality specifically its scale and architectural interest and mixed use and for its association with the tram route and*

the major northern transport route.

- b) New development should recognise the linear nature of the buildings in Pittwater Road for their contribution to the visual character of this street. New development should not visually overwhelm the four groups of individual heritage items in that part of the street which is zoned for business purposes in the LEP.

4.2 Heritage Items in the Vicinity of the Site

For the following, refer to Figure 52 a detail of the Heritage Plan from the LEP 2013 which shows the location of heritage items in the immediate vicinity of the site. 'In the vicinity' has been determined by physical proximity to the site, existing and potential view corridors and the nature of the proposed works. Heritage items are coloured brown and numbered. Conservation Areas are hatched in red and numbered. The subject site is noted by a black arrow

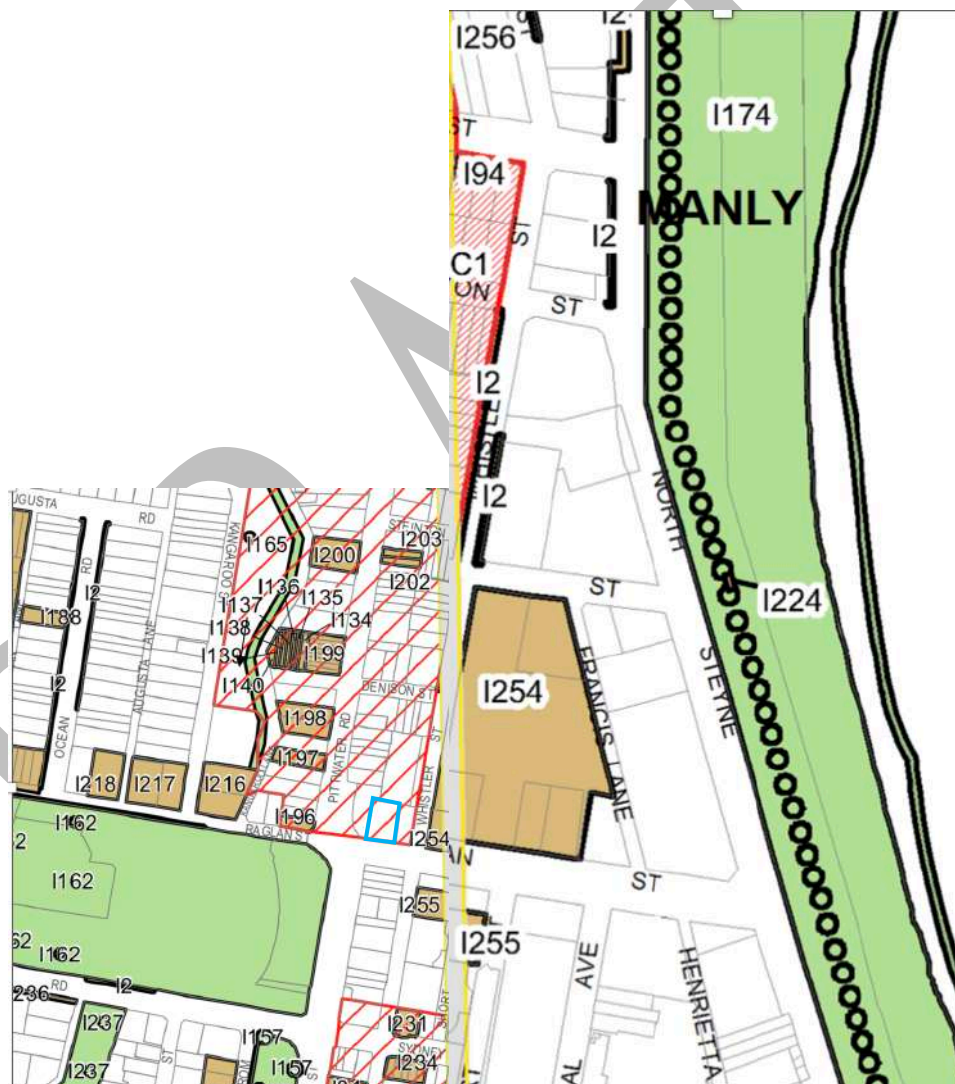


Figure 52: Detail of the Manly LEP 2013 Heritage PMap. The site is outlined in blue.

The following heritage items, listed by Schedule 5 Part 1 of the Manly LEP 2013, are located within the immediate vicinity of the site:

- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. State Heritage Register SHR: 00938. Heritage item I255 on figure 52.

- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. S.170 NSW State Agency Heritage Register. Heritage item I255 on figure 52.
- Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. State Heritage Inventory Data Base No: 3430433. Heritage item I255 on figure 52.
- St Mary's Church, Presbytery and School, Whistler Street (corner Raglan Street), Manly, NSW 2095. State Heritage Inventory Data Base No: 2020234. Heritage item I254 on figure 52.
- Baby Health Care Centre Building, 1 Pittwater Road, Manly, NSW, 2095. State Heritage Inventory Database No: 2020539. Heritage Item I196 on figure 52.
- Pittwater Road Conservation Area, Manly, NSW 2095. State Heritage Inventory Database No: 2020536 on figure 52.

4.2.1 Electricity Substation No. 15009. 34-36 Whistler Street, Manly, NSW, 2095. State Heritage Register SHR: 00938. Time no. 1255 on Figure 52.

The State Heritage Inventory provides the following Statement of Significance for Electricity Substation No. 15009, this statement has been relied upon for this Report:

The Manly Zone substation is a fine and robust, well detailed face brick purpose designed and built structure. It is an excellent and externally intact representative example of the Interwar Art Deco style. It is considered to be a rare example of this style and of State Significance.

The principal view corridors towards the Electrical Substation are obtained from directly outside of the site from Whistler street and on approach either side of it. The site is on a quiet street with one way traffic with buildings built to its boundaries either side of it. There are no view corridors between the substation and the subject site. Refer to Figure 53.



Figure 53: Electricity Substation 15009 on Whistler Street. SHR 00938. WPH photo.

- 4.2.2 St Mary's Church, Presbytery and School, Whistler Street (corner Raglan Street), Manly, NSW 2095. State Heritage Inventory Data Base No: 2020234.



Figure 54: St. Mary's Church, Presbytery and School from Whistler Street.

The State Heritage Inventory provides the following Statement of Significance for this item and has been relied upon for this Report:

The listing covers a significant group of five religious and school buildings dating from circa 1890 to the 1950s, complementary in style. The group is of significance for Manly area for its historical, aesthetic and social reasons, and

for the rarity and representativeness. The site has a long and ongoing spiritual association with the local Roman Catholic community since c.1890 when the church building was created. The site is a major landmark visible from a distance from both Raglan Street and Whistler Street. The complex makes a major contribution to the streetscape and to the townscape character, augmented as the group of buildings are adjoining each other at the site. The Church building, the Presbytery and School are each representative of their respective type of building around the date of creation, and the complex as a whole is a rare collection of five fine buildings with spiritual associations in the local area.

The principal view corridors towards St Mary's Church, Presbytery and School is obtained from Whistler Street. Because the Church and associated buildings are near the corner of Raglan Street and adjacent an open carpark there are also principal views from Raglan Street. There are no view corridors between the item and the subject site. The item is identified as I1254 in Figure 52. For image Refer to Figure 54.

4.2.3 Baby Health Care Centre Building, 1 Pittwater Road, Manly, NSW, 2095. State Heritage Inventory Database No: 2020539.



Figure 55: Baby Health Care Centre. Google Maps 2019.

The State Heritage Inventory provides the following Statement of Significance for this item and has been relied upon for this Report:

This item is of local cultural heritage significance for its ability to demonstrate the pattern of Manley's development and that of Government utilities. The building is an example of a Baby Health Clinic constructed in the mid Twentieth Century in response to the development of the care for children's health. It is a fine and intact example of Post War International style of architecture.

The item is located on a prominent intersection of Raglan Street and Belgrave Street and directly opposite Ivanhoe Park, as such there are numerous view corridors to this site. The item is identified as I196 on Figure 52. There are no view corridors between the item and the subject site. Refer to Figure 55.

4.3 Integrity

The site has a low level of integrity as the building has been substantially modified internally and externally from its original form and use as an Ambulance Station to accommodate adaptive reuse as a backpacker hostel. Refer to figures 8 and 14-17 which illustrates the early buildings and footprints on the site. The following alterations to the building are noted:

- The face brick on the Raglan Street elevation has been rendered and painted.
- The roof tiles are replacement.
- The timber framed windows on the first floor have been replaced with aluminium profiles.
- The windows and doors on the ground floor have been removed and replaced with contemporary doors and openings including a fire door exit.
- The Ambulance vehicle garage door has been infilled with fixed glass and timber posts.
- The building has been extended toward the north boundary with contemporary additions and openings.
- The interior room layout to both levels has been completely modified to adaptively reuse the building as a backpacker hostel.
- The internal finishes and fixtures are contemporary.

Due to the high level of external modification of the building, the site makes a neutral contribution to the Heritage Conservation Area.

4.4 Streetscape Contribution

Only the front, southern elevation of the site to Raglan Street is visible from the public domain. The principal view corridors to this elevation are from directly in front of it or from the corner of Pittwater Road and Raglan Street.

The small setback of the building behind the corner building and proximity of adjacent buildings either side of it built boundary to boundary, reduce the view corridors and exposure of the elevation on approach to the site in both directions.

The original simplicity and symmetrical proportions of the face brick front elevation, including the central recessed balcony decorated with an arched colonnade and vehicle entry once framed with brick detail has been lost under layers of render and paint. The intrusive replacement aluminium framed windows, shopfront glazing, timber posts and contemporary doors have further eroded the original fabric and any contribution the elevation once made to the Heritage Conservation Area.

Although the 2000 addition was intended to compliment the former ambulance building by incorporating a similar roof profile and balcony detail, the building only succeeds in reducing the significance of the former ambulance through the copy of detail. The difference in floor levels between the buildings coupled with the use of small aluminium windows creates an uneven and bland streetscape elevation.



Figure 56: Subject site within the Raglan Streetscape.

4.5 Comparative Analysis

The following table provides examples of Ambulance Station built at a similar time to the subject site within Sydney.

<p>Paddington Ambulance Station The Paddington Ambulance Station is later than the key period of significance for Victoria Barracks and Paddington, but is within a significant historic precinct and adjoins other highly significant buildings. It is a good example of an Inter-War Georgian Revival building which makes a positive contribution to the streetscape. The ambulance station is still operational.</p>	 <p>Figure 1: The Paddington Ambulance Station, No. 2 Oatley Road, Paddington. NSW State Heritage Inventory</p>
<p>Former Marrickville Ambulance Station The former Marrickville Ambulance Station was opened by the NSW Governor Sir Dudley de Chair on Saturday 15 March 1930. The Marrickville branch of the NSW Ambulance and Transport Association was constructed to alleviate pressure on the Newtown - Marrickville Ambulance Station which attended more than 4000 cases per year. The former Ambulance Station is a two story face brick structure with a hipped terracotta roof constructed in</p>	

<p>the Inter War style. The station is currently vacant and features amounts of graffiti on the roller door bays.</p>	
<p>Auburn Ambulance Station The former Auburn Ambulance Station is historically and aesthetically significant. The building is historically significant as it was the premises of the local St. Johns Ambulance brigade for over twenty years. It is aesthetically significant as an intact example of a local Inter War Free Classical / Functionalist building to house an important local utility. The siting of the building and the integrity of its aesthetic qualities make it a notable, though run down, landmark in its local area.</p>	 <p>Figure 2: The Auburn Ambulance Station No. 54 Queen Street, Auburn. NSW Heritage Inventory</p>
<p>Lane Cove Ambulance Station Functionalist Ambulance Station still operational.</p>	 <p>Figure 3: The Lane Cove Ambulance Station</p>

Discussion

The Stoke House Backpacker Hostel, the former Manly and Warringah Ambulance Station Summer Hill Ambulance Station has been highly modified. Although it resembles its original form all of its key details which contribute to its heritage significance have been removed including the exposed face brick elevation to Raglan Street, fine brick detailing, timber framed windows and doors and terracotta tile roof.

Its heritage significance has been so eroded by alterations and additions that it no longer forms part of the group of Ambulance Stations built at a similar time in NSW.

4.5 Assessment of Significance

The subject site is assessed for heritage significance under the following criteria of Heritage NSW with regards to its contribution to the Conservation Area. The

Guidelines for Inclusion/Exclusion are as provided by *Assessing Heritage Significance, NSW Heritage Manual Update*.

Criterion (a)

An item is important in the course, or pattern, of New South Wales' cultural or natural history (or the cultural or natural history of the local area)

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none">shows evidence of a significant human activity	<ul style="list-style-type: none">has incidental or unsubstantiated connections with historically important activities or processes	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none">is associated with a significant activity or historical phase	<ul style="list-style-type: none">provides evidence of activities or processes that are of dubious historical importance	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none">maintains or shows continuity of a historical process or activity	<ul style="list-style-type: none">has been altered so that it can no longer provide evidence of a particular association	The site does not fulfil these guidelines for inclusion as an item.

No. 22-24 Raglan Street, Manly is an example of a highly modified Ambulance Station. Constructed in 1936. The former station has been extensively refurbished for adaptive reuse as backpacker hostel with extensive and intrusive alterations and additions undertaken to the front elevation so that it is no longer recognisable as the Manly Warringah Ambulance Station or significant to the heritage Conservation Area.

The site does not make a positive contribution to the Conservation Area on the basis of its level of significance under this criterion because of the level of alteration and the lack of evidence of the original detailing.

Criterion (b)

An item has strong or special association with the life or works of a person, or group of persons, of importance in New South Wales' cultural or natural history (or the cultural or natural history of the local area).

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none">shows evidence of a significant human occupation	<ul style="list-style-type: none">has incidental or unsubstantiated connections with historically important people or events	The site does not fulfil these guidelines for inclusion as an item.

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> is associated with a significant event, person, or group of persons 	<ul style="list-style-type: none"> provides evidence of people or events that are of dubious historical importance 	The site does not fulfil the guidelines for inclusion as an item.
<ul style="list-style-type: none"> maintains or shows continuity of a historical process or activity 	<ul style="list-style-type: none"> has been altered so that it can no longer provide evidence of a particular association 	The site does not fulfil the guidelines for inclusion as an item.

The Former Manly Warringah Ambulance Station was designed by Architect Trenchard Smith and Maisey, the firm who also designed the Manly Town Hall. The former Ambulance Station is a modest and ordinary example of their work that has now been highly modified. There are better and more intact of examples of their work including the Manly Town Hall.

The former Ambulance Station was once associated with the NSW Ambulance Service originally founded as the Civil Ambulance and Transport Brigade was established in 1895. The service, originally a dedicated community service, expanded throughout NSW to operate from 266 locations by 2015.¹³ The former Manly Warringah Ambulance Station ties with The Ambulance Station ceased on 1986 when the station was decommissioned and extensively modified for use as a Hostel.

The site does not contribute to the Conservation Area on the basis of its association with significant people or groups.

Criterion (c)

An item is important in demonstrating aesthetic characteristics and/or a high degree of technical achievement in New South Wales (or the local area)

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> shows or is associated with, creative or technical innovation or achievement 	<ul style="list-style-type: none"> is not a major work by an important designer or artist 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is the inspiration for creative or technical innovation or achievement 	<ul style="list-style-type: none"> has lost its design or technical integrity 	The site does not fulfil these guidelines for inclusion as an item.

¹³ " History - NSW Ambulance ". 2017. Ambulance.Nsw.Gov.Au. Accessed July 11 2017.
<http://www.ambulance.nsw.gov.au/about-us/History.html>.

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> is aesthetically distinctive or has landmark qualities 	<ul style="list-style-type: none"> its positive visual or sensory appeal or landmark and scenic qualities have been more than temporarily degraded 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> exemplifies a particular taste, style or technology 	<ul style="list-style-type: none"> has only a loose association with a creative or technical achievement 	The site does not fulfil these guidelines for inclusion as an item.

No. 22-24 Raglan Street is an example of a highly modified Ambulance Station constructed in 1936. Originally designed in the Art Deco functionalist style most of its identifying architectural detail and materiality has been lost or concealed under intrusive contemporary alterations. Any significant internal fabric has been removed.

The intrusive alterations have a negative impact on the presentation to the street and Heritage Conservation area. Neither the exterior or interior demonstrate a high degree of architectural merit or have landmark qualities. The site does not contribute to the Conservation Area on the basis of its lack of design integrity.

Criterion (d)

An item has strong or special association with a particular community or cultural group in New South Wales (or the local area) for social, cultural or spiritual reasons.

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> is important for its association with an identifiable group 	<ul style="list-style-type: none"> is only important to the community for amenity reasons 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is important to a community's sense of place 	<ul style="list-style-type: none"> is retained only in preference to a proposed alternative 	The site does not fulfil these guidelines for inclusion as an item.

The subject site is not known to have any strong or special associations with any group for social, cultural or spiritual reasons.

No. 22-24 raglan Street does not contribute to the Heritage Conservation Area under this basis.

Criterion (e)

An item has potential to yield information that will contribute to an understanding of New South Wales' cultural or natural history (or the cultural or natural history of the local area)

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> has the potential to yield new or further substantial scientific and/or archaeological information 	<ul style="list-style-type: none"> has little archaeological or research potential 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is an important benchmark or reference site or type 	<ul style="list-style-type: none"> only contains information that is readily available from other resources of archaeological sites 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> provides evidence of past human cultures that is unavailable elsewhere 	<ul style="list-style-type: none"> the knowledge gained would be irrelevant to research on science, human history of culture 	The site does not fulfil these guidelines for inclusion as an item.

No. 22-24 Raglan Street has been modified with intrusive alteration and additions resulting in the erosion and concealment of the original Ambulance Station features and details to render it almost unrecognisable as the former Ambulance Station. It has lost its original function and significance as an Ambulance Station. There are more intact Ambulance Stations of this type in the Sydney area.

The site does not contribute to the Conservation Area on the basis that it will not contribute to the understanding of the area's cultural history.

The Ambulance Station does not possess the potential to yield information that will contribute to an understanding of New South Wales' cultural or natural history.

Criterion (f)

An item possesses uncommon, rare or endangered aspects of New South Wales' cultural or natural history (of the cultural or natural history of the local area).

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> provides evidence of a defunct custom, way of life or process 	<ul style="list-style-type: none"> is not rare 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> demonstrate a process, custom or other human activity that is in danger of being lost 	<ul style="list-style-type: none"> is numerous but under threat 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> shown unusually accurate 		The site does not fulfil these guidelines for inclusion as an item.

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
evidence of a significant human activity		
<ul style="list-style-type: none"> is the only example of its type 		The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> demonstrate designs or techniques of exceptional interest 		The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> shown rare evidence of a significant human activity important to a community 		The site does not fulfil these guidelines for inclusion as an item.

No. 22-24 Raglan Street is a 1936 highly modified former Ambulance Station that is no longer recognisable as being of this period and has been altered to the extent that it is no longer representative as an example of the Art Deco Functionalist style in which it was built. This report has identified Ambulance Stations within Sydney of this period. Not all make a positive contribution; however, those that do present a significantly more intact front elevation and original detailing than the subject dwelling does.

The site does not contribute to the Conservation Area on the basis that it will contribute to the understanding of the area's cultural history.

Criterion (g)

An item is important in demonstrating the principal characteristics of a class of New South Wales (or a class of the local areas):

- Cultural or natural places; or
- Cultural or natural environments

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
<ul style="list-style-type: none"> is a fine example of its type 	<ul style="list-style-type: none"> is a poor example of its type 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> has the potential characteristics of an important class or group of items 	<ul style="list-style-type: none"> does not include or has lost the range of characteristics of a type 	The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> has attributes typical of a particular way of life, philosophy, custom, significant 	<ul style="list-style-type: none"> does not represent well the characteristics that make up a 	The site does not fulfil these guidelines for inclusion as an item.

Guidelines for Inclusion	Guidelines for Exclusion	Compliance
process, design, technique of activity	significant variation of type	
<ul style="list-style-type: none"> is a significant variation to a class of items 		The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is part of a group which collectively illustrates a representative type 		The site fulfils these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is outstanding because of its setting, condition or size 		The site does not fulfil these guidelines for inclusion as an item.
<ul style="list-style-type: none"> is outstanding because of its integrity or the esteem in which it is held 		The site does not fulfil these guidelines for inclusion as an item.

No 22-24 Raglan Street is not identifiable as an Ambulance Station that was built in 1936 and is not a good example of its type because key elements have been removed or concealed which has confused any reading of its original detail or significance.

The site does not contribute to the Conservation Area under this criterion.

Conclusion of Assessment

The subject site has been assessed for heritage significance under the Heritage NSW criteria with regards to its contribution to the Conservation Area. The Guidelines for Inclusion/Exclusion are as provided by *Assessing Heritage Significance, NSW Heritage Manual Update*.

The site failed to reach the threshold for heritage listing or for any positive contribution it makes to the Pittwater Road Conservation Area under any of the criteria.

5.0 SCOPE OF WORKS

The following should be read in conjunction with the plans prepared by Carlisle Architects that accompany this application. The proposed works include:

- Remove all the structures on the site
- Construct a new four storey mixed use building with basement parking.



Figure 57 : Proposed scheme as viewed from Raglan Street. Carlisle Architects.2022.

6.0 EFFECT OF WORK ON THE HERITAGE CONSERVATION AREA

6.1 Method of Assessment

This Section has been prepared with reference to 4.4.1 demolition and 3.2 Heritage Considerations within the Manly DCP 2013.

4.4.1 Demolition

Relevant DCP objectives in this plan in relation to these paragraphs include:

Objective 1) To protect the environment during demolition, site works, and construction phases of development.

Where development involves demolition, the applicant is to demonstrate that the degree of demolition considers any existing building on the land that should be retained and appropriately adapted in order to:

A) Meet ecologically sustainable development principles by conserving resources and energy and reducing waste from any demolition process; and

Not Applicable to the Heritage Impact Statement

b) Conserve the cultural heritage of the existing building and that of the locality. An appropriate assessment of potential heritage significance must accompany any DA in relation to demolition. If the property has merit as a potential heritage item, the heritage controls and considerations in this plan apply, and

This Heritage Impact Statement has established that the former Manly and Warringah Ambulance Station has retained little or no cultural heritage. The heritage assessment within this report has illustrated that the former Ambulance Station does not reach the threshold for any of the criteria established by Heritage NSW for any positive contribution to the Pittwater Road Heritage Conservation Area.

c) Comply with the requirements of the Northern Beaches Waste Management Policy.

Not Applicable to the Heritage Impact Statement

7.0 EFFECT OF WORKS ON THE HERITAGE CONSERVATION AREA

The following responses consider:

3.2.1 Consideration of Heritage Significance Manly DCP 2013.

LEP Clause 5.10(4) requires that Council consider the effect of proposed development on heritage significance of a heritage item or heritage conservation area. LEP Clause 5.10(5)(c) further requires that the development of land in the vicinity of Heritage Items or Conservation Areas may require further assessment into the effect on the heritage significance of the item/area.

The proposed works will have an acceptable impact on the Pittwater Road Conservation Area (HCA) for the following reasons:

- No subdivision is proposed. The existing lot subdivision pattern will be retained.
- The site is an example of a highly modified former Ambulance Station that is no longer recognisable as a former Ambulance Station or its original architectural style. Layers of intrusive alterations and additions have eroded the fabric and architectural detail of the building so that it is no longer recognizable as the former Manly Warringah Ambulance Station.
- Due to the extent of modifications to the existing building it no longer makes a positive contribution to the HCA.
- The building has lost its original use, function and purpose as an Ambulance Station which ceased operation as an Ambulance Station in 1986.
- The proposed works are in keeping and similar to other contemporary mixed-use buildings recently approved by the Northern Beaches Council.
- The proposed design interprets the recessed balcony and arched colonnade on the Raglan Street elevation of the former Ambulance Station, which helps retain a reading and impression of one of the most significant architectural features of the Ambulance Station.
- The articulation of the Raglan Street elevation achieved by the recessed balconies, arched colonnade and brick detailing pushes back the glazing line so the that building has a similar solid to void ratio and more matte materiality as the former Ambulance building and significant buildings in the HCA.
- The significance of the former Ambulance Station can be meaningfully and successfully illustrated through a considered Heritage Interpretation Plan of display panels internally and externally on the building.

- The proposed design has been carefully considered and provides a highly articulated elevation to Raglan Street of subtle variations in materiality, texture and colour that will enhance the significance of the HCA.

7.0 EFFECT OF WORKS ON HERITAGE ITEMS IN THE VICINTIY OF THE SITE

This section has been prepared with an understanding of the relevant sections 3.2.1.1 Development in the vicinity of heritage items, or conservation areas of the DCP 2013.

3.2.1.1 Development in the vicinity of heritage items, or conservation areas

- a) *In addition to LEP listings of environmental heritage this DCP requires consideration of the effect on heritage significance for any other development in the vicinity of a heritage item or conservation area.*
- b) *Proposed development in the vicinity of a heritage item or conservation area must ensure that; It does not detract or significantly alter the heritage significance of any heritage items, conservation area or place;*

The proposed works will have a minimal and acceptable impact on the heritage items in the vicinity for the following reasons:

The St Mary's Church. Presbytery and School group is physically and visually removed from the site by Whistler Street, the width of the on-grade carpark and the contemporary three storey residential building on the corner of Raglan and Whistler Street. As such there are no shared view corridors between the subject site and this heritage site from Raglan or Whistler Street.



Figure 58: View corridor between the St Mary's Church Group and the site at No. 22 Raglan Street, Manly. Heritage item noted with red arrow.

The principal view corridors toward the Electricity Substation heritage item on Whistler Street are from the opposite direction to the subject site. The substation is physically and visually removed from the subject site by intervening contemporary

buildings and Raglan Street. Council have accepted contemporary development in the context of this heritage item with the development of an eight storey residential building directly next to the substation. Refer Figure 59. This adjacent development will have a far greater impact on the heritage item than the proposed development at No. 22-24 Raglan Street Manly.



Figure 59: New eight storey residential building directly adjacent to the Electrical Substation.



Figure 60: View corridor between the Electrical Substation and the No. 22-24 Raglan Street, Manly. Site noted with red arrow.

1) *The heritage values or character of the locality are retained or enhances; and;*

The proposed works will retain the heritage values and character of the locality:

- The proposed design interprets the recessed balcony and arched colonnade on the Raglan Street elevation of the former Ambulance Station, which helps retain a reading and impression of one of the most significant architectural features of the Ambulance Station.
- The articulation of the Raglan Street elevation achieved by the recessed balconies, arched colonnade and brick detailing pushes back the glazing line so the that building has a similar solid to void ratio and more matte materiality as the former Ambulance building and significant buildings in the HCA.
- The proposed design has been carefully considered and provides a highly articulated elevation to Raglan Street of subtle variations in materiality, texture and colour that will enhance the significance of the HCA.

2) *Any contemporary response may not necessarily seek to replicate heritage details or character of heritage buildings in the vicinity but must preserve heritage significance and integrity with complementary and respectful building form, proportions scale style materials colours and finishes and building /street alignments.*

- The proposed works are a subtle contemporary response that is modest in scale, using simple brick detailing and architectural forms which are sympathetic to and in keeping with the former Ambulance Station and the Heritage Conservation Area including.
- The proposed design interprets the recessed balcony and arched colonnade on the Raglan Street elevation of the former Ambulance Station, which helps retain a reading and impression of one of the most significant architectural features of the Ambulance Station.
- The articulation of the Raglan Street elevation achieved by the recessed balconies, arched colonnade and brick detailing pushes back the glazing line so the that building has a similar solid to void ratio and more matte materiality as the former Ambulance building and significant buildings in the HCA.
- The proposed design has been carefully considered and provides a highly articulated elevation to Raglan Street of subtle variations in materiality, texture and colour that will enhance the significance of the HCA.

c) *The impact on a setting of the heritage item or conservation area is to be minimised by:*

a) *Providing an adequate area around the building to allow interpretation of the heritage item;*

The proposed works are modest in scale and are visually separated from the heritage items in the vicinity. The proposed works will not therefore have an adverse impact on these heritage items or interfere with their understanding.

b) Retaining original or significant landscaping.

There is no original or significant landscaping on the site.

c) Protection and allowing the interpretation of any archaeological features; and;

This heritage Impact statement does not cover archaeology.

d) Retaining and respecting significant views to and from the heritage item

The proposed work will not impact on significant views to or from heritage items in the vicinity.

8.0 CONCLUSION

This Heritage Impact Statement has outlined the history and established the significance of the The Stoke House Backpackers at No. 22-24 Raglan Street, Manly. The building was originally built as the Manly and Warringah Ambulance Station in 1936. It ceased operation as an Ambulance Station in 1986 and was extensively altered internally and externally for adaptive reuse as a Backpackers Hotel in or around 2000 in conjunction with the development of the eastern allotment. The building underwent further alteration and refurbishment in 2019.

The proposed removal of the existing buildings on the site will have an acceptable impact on the Pittwater Road Heritage Conservation Area (HCA) and the heritage item in the vicinity. The former Ambulance Station ceased functioning as an Ambulance Station over 26 years ago and has lost any heritage significance through extensive and intrusive alterations and additions that has removed the building association with its former use and architectural character, such that it no longer makes a positive contribution to the HCA or heritage items in the vicinity.

The proposed building will have an acceptable and positive impact on the HCA and heritage items in the vicinity. The proposed design has carefully considered and interpreted the original architectural character of the former Ambulance Station that once made it significant to this area of Manly. The subtlety of the proposed materials, texture and colour is enhanced by the highly articulated Raglan Street elevation and arched colonnade. The colonnade is reminiscent of the existing recessed balcony and helps to reduce large areas of glazing to the HCA and creates a solid to void ratio that is in keeping with significant buildings in the HCA and the former Ambulance Station.

The proposed works comply with the controls and objectives for heritage items as outlined in Section 3.2 of the *Manly DCP 2013*.