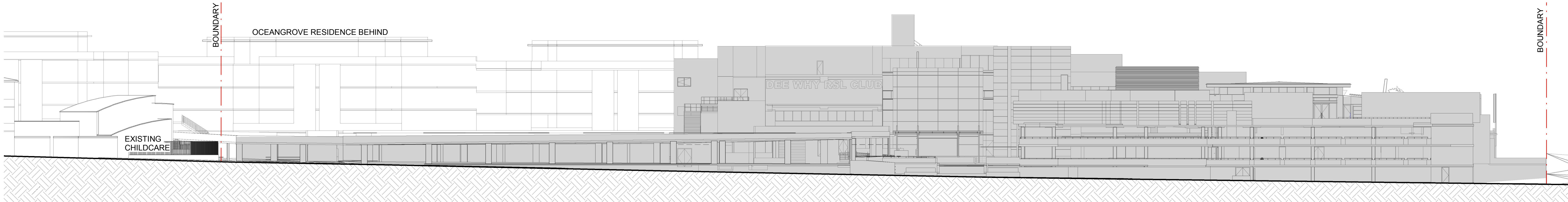
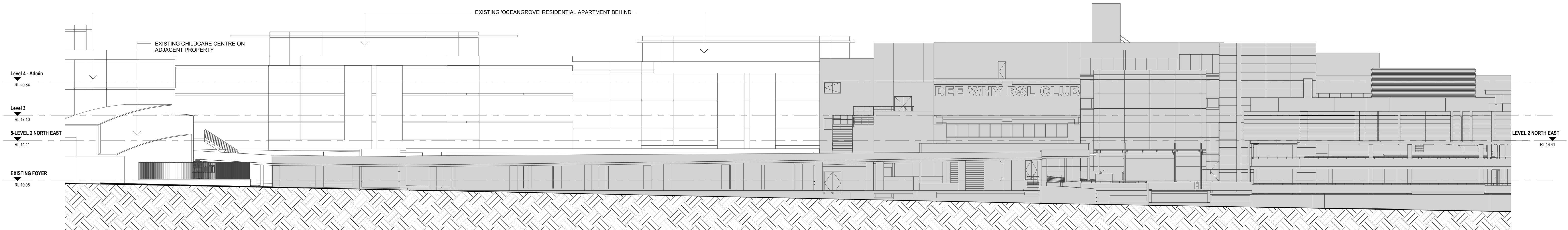


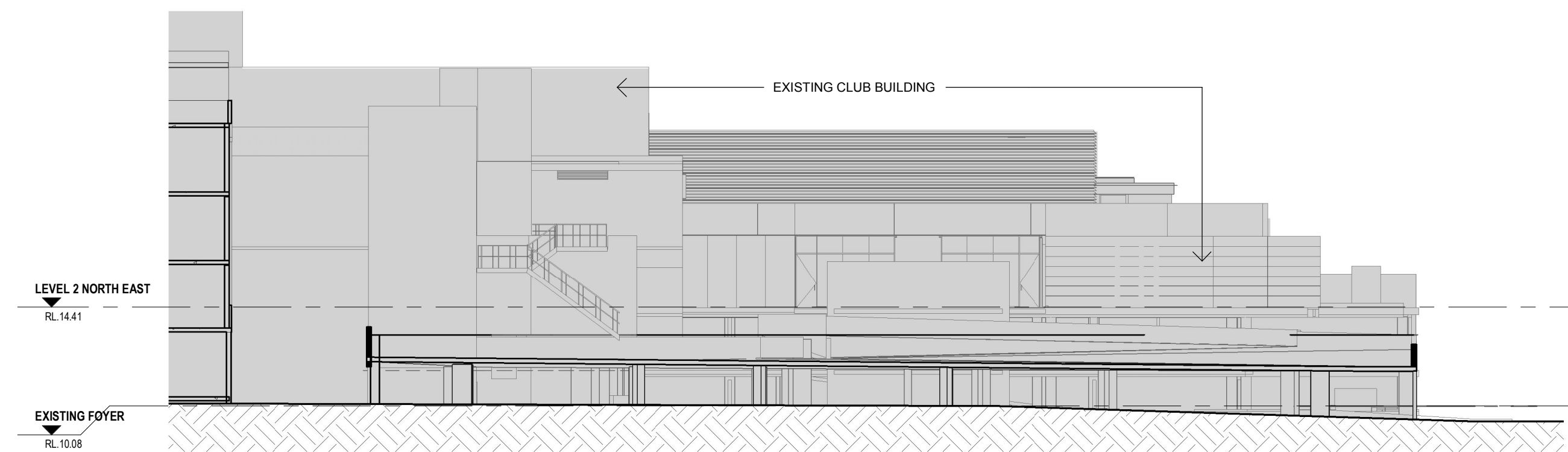
ISSUE	DATE	DESCRIPTION
1	17.03.2017	ISSUED FOR DEVELOPMENT APPLICATION
2	02.03.2018	PRELIMINARY SECTION 96



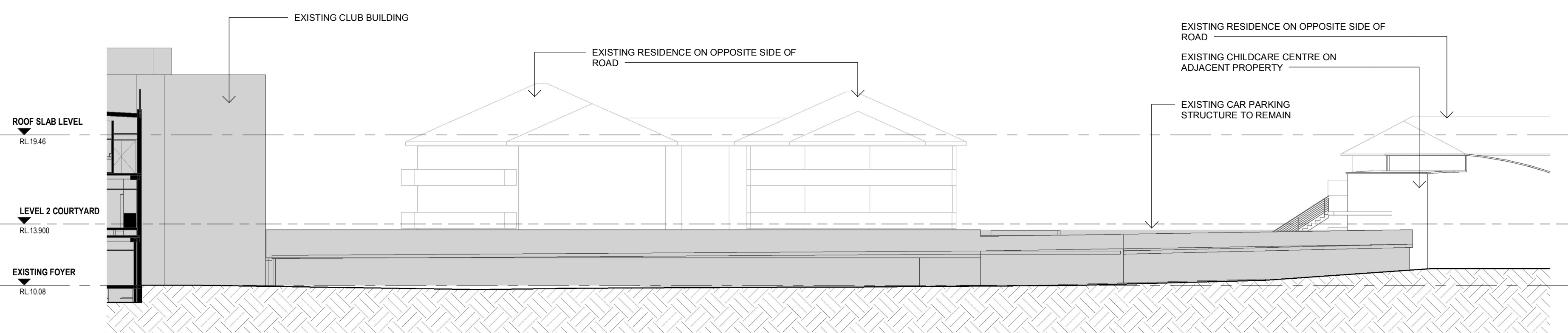
4 EX-EAST ELEVATION (Overall)  
Scale: 1 : 250



1 EX-EAST ELEVATION  
Scale: 1 : 200



2 EX-SOUTH ELEVATION  
Scale: 1 : 200



3 EX-WEST ELEVATION  
Scale: 1 : 200

CLIENT:  
DEE WHY RSL CLUB

PROJECT:  
STAGE 5 - CLUB EXTENSION

ARCHITECT:  
**ALTIS**  
architecture  
p 61 2 9364 9000 f 61 2 9571 7930 lower deck jones bay wharf  
suite 123 / 26 - 32 pirrama road pymont nsw 2009 australia

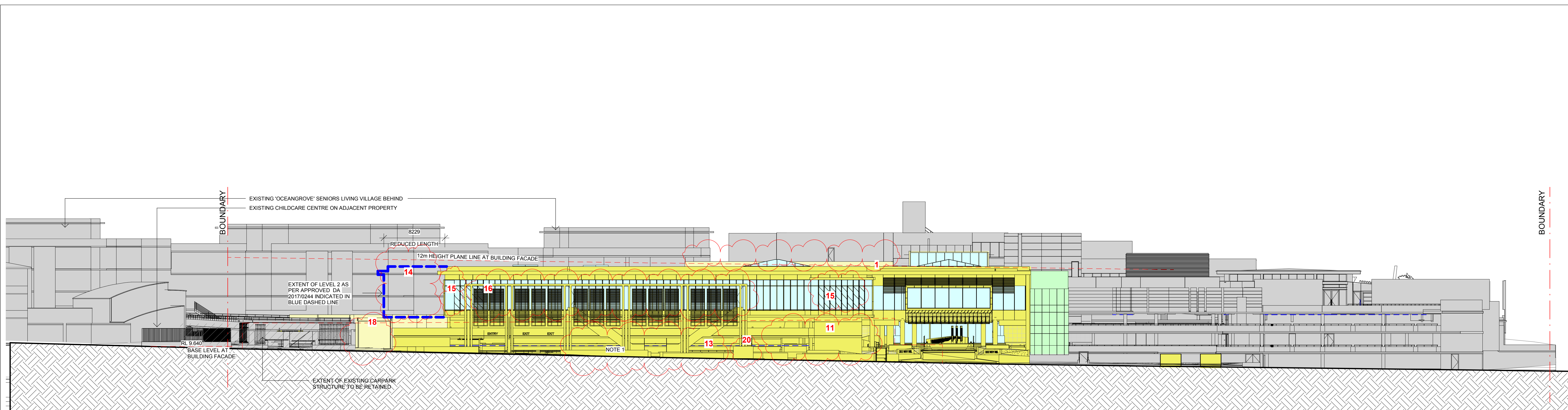
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EXISTING ELEVATIONS

NOTES:  
Nominated architect: Rofe Latimer - 6535  
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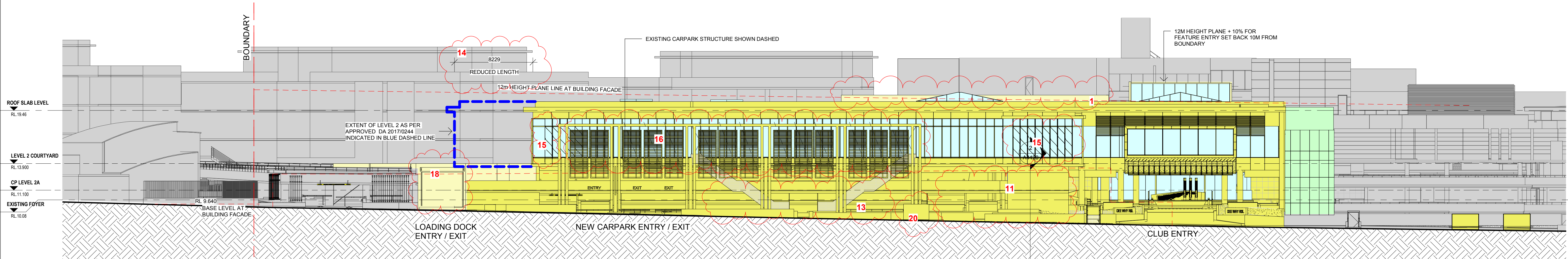
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DRAWN BY: MC  
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PROJECT NO: 2341.06  
CAD FILE:

NORTH:  
DATE: MARCH 2018  
DRAWING NO: DA-2000  
ISSUE: 2

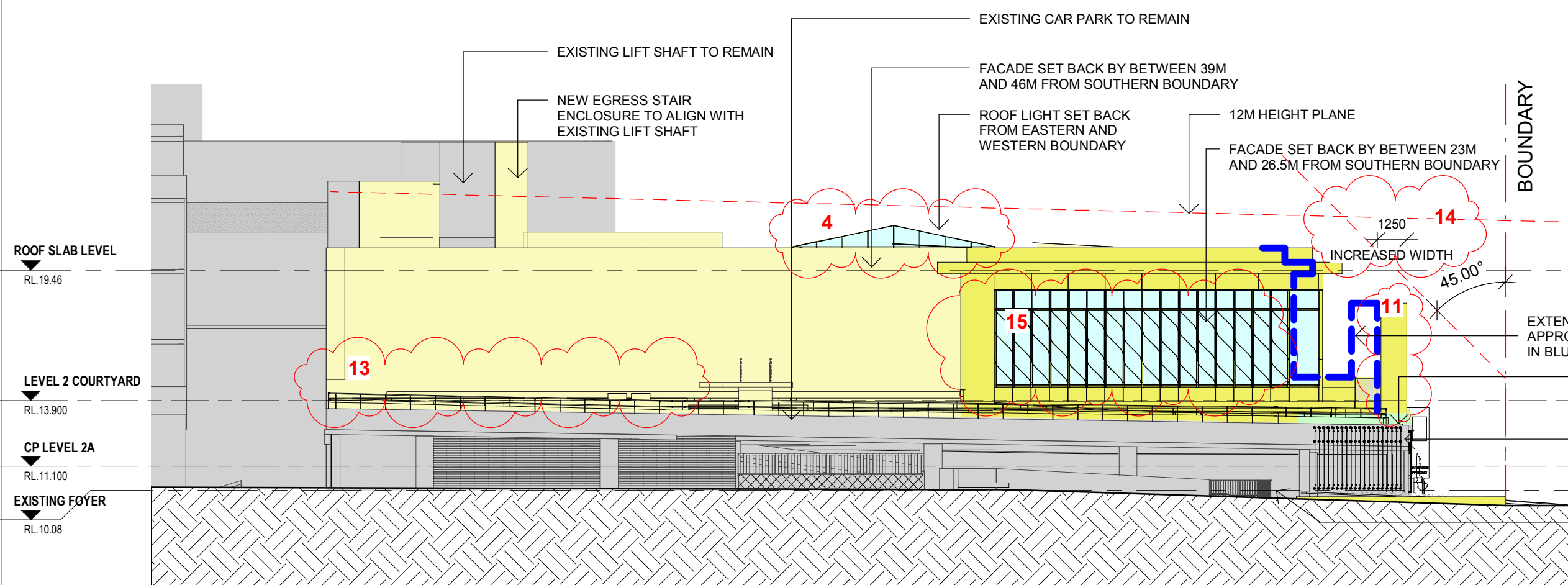




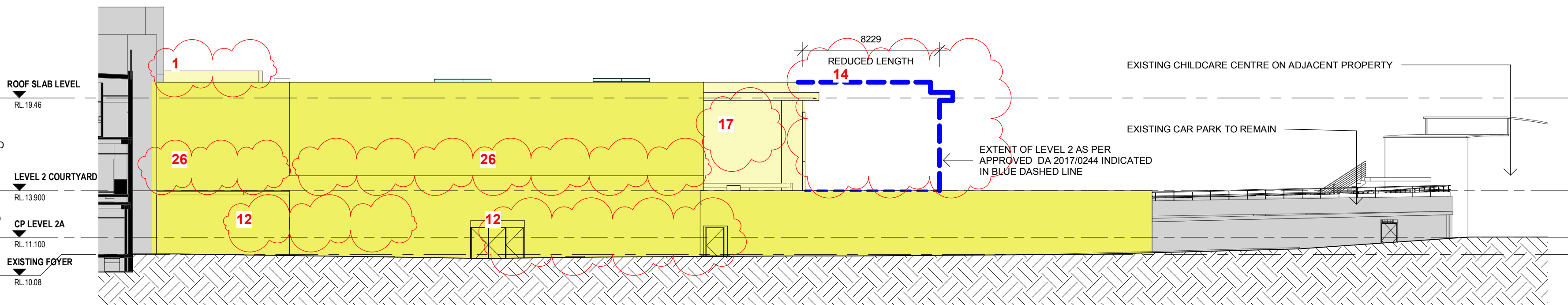
1 REFERENCE EAST ELEVATION  
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2 5B-S96-EAST ELEVATION  
Scale: 1 : 200



3 PROPOSED SOUTH ELEVATION  
Scale: 1 : 200



4 PROPOSED WEST ELEVATION  
Scale: 1 : 200

ISSUE	DATE	DESCRIPTION
1	17.03.2017	ISSUED FOR DEVELOPMENT APPLICATION
2	05.03.2018	ISSUED FOR SECTION 96

- CHANGES FOR SECTION 96 APPROVAL
- roof
1. RELOCATED OPEN PLANTROOM AT THE ROOF LEVEL
  2. MODIFIED ROOF OPENING ABOVE MAIN ENTRANCE
  3. MODIFIED CENTRAL SKYLIGHT (REDUCED HEIGHT, MODIFIED PITCH)
  4. MODIFIED COURTYARD: TWO OPEN COURTYARDS AND ONE SKYLIGHT COVERED AREA INSTEAD OF ONE LARGE GLASS ROOF. REDUCED HEIGHT AND PITCH
  5. MODIFIED SMOKING TERRACE: ROOF OPENING, SIZE AND PLANTED AREA. AREA REDUCED BY 21m<sup>2</sup>
  6. REDUCED NUMBER OF ROOF SKYLIGHTS ABOVE ASIAN RESTAURANT: 2 INSTEAD OF 6 TO SUIT THE NEW INTERNAL LAYOUT
  7. MODIFIED EXIT STAIR: POSITION, CONFIGURATION, METAL ROOF NO LONGER REQUIRED
  8. CONCRETE ROOF INSTEAD OF METAL DECK ROOF FOR THE MAJORITY OF THE BUILDING EXTENSION
  9. NO SLAB ABOVE THE EXPOSED CAR PARK LEVEL 2 IN THE SOUTH AREA
  10. NEW ACOUSTIC METAL ROOF ABOVE REVISED LOADING DOCK
- facade
11. MODIFIED FACADE APPEARANCE DUE TO CHANGES IN THE CAR PARK LEVELS, LOADING DOCK AND EGRESS STAIRS
  12. REMOVED LOUVRES ON THE BACK FACADE (WESTERN FACADE) DUE TO MECHANICAL RISER NO LONGER REQUIRED / REVISED LOCATION FOR EGRESS DOORS
  13. 2m WIDE INTERNAL EGRESS STAIR MODIFIED INTO TWO 1m WIDE EXTERNAL STAIRS WITH DECORATIVE BALUSTRADES
  14. EXTENT OF LEVEL 2 (MAIN TRADING LEVEL) REDUCED BY 8320mm FROM THE SOUTHERN BOUNDARY AND INCREASED BY 1100mm TO THE EASTERN BOUNDARY
  15. MODIFIED FACADE CURTAIN WALL MULLION BLADES
  16. CONCRETE BLADE WALL REDUCED BY 1.47m IN HEIGHT AND 6.2m IN LENGTH
  17. VERTICAL FINS REMOVED FROM BACK WALL (WESTERN FACADE)
- ground/landscape/floor plate
18. REVISED LOADING DOCK ENTRANCE AND LOCATION/ ENLARGED DEMOLISHED AREA IN THE EXISTING RETAINED SOUTHERN CAR PARK
  19. NEW / MODIFIED EGRESS PATHS FROM EXISTING CARPARK LEVEL -1. NEW CARPARK LEVEL 1.
  20. RELOCATED ELECTRICAL SUBSTATION / LANDSCAPED AREA MOVED TO SUIT
  21. MEDIAN STRIP ADDED AT ENTRY TO PROVIDE PLACE OF REFUGE AND ENSURE WIDTH OF ENTRY/ EXIT IS LESS THAN 8M AS PER DA CONDITION 23
  22. RELOCATED STORMWATER OVERFLOW PATH
  23. EXTENT OF BASEMENT CAR PARK LEVELS REDUCED. BASEMENT REDUCED BY 14394mm FROM THE WEST BOUNDARY. BASEMENT ENLARGED BY 280mm TOWARDS THE SOUTH BOUNDARY. CARPARK CHANGED FROM FLAT PLATE TO SPLIT LEVEL ON ALL LEVELS. DEPTH OF CAR PARK INCREASED FROM RL - 3.600 TO RL -5.700. OVERALL PARKING SPACES REDUCED FROM 687 TO 680
  24. BASEMENT CAR PARK LEVEL -5 ADDED.
  25. CAR PARK ENTRY / EXIT MOVED TOWARD NORTH BY 30444mm
  26. PLANTER MODIFIED ON THE BACK (WESTERN) FACADE DUE TO REDUCTION OF THE MAIN TRADING FLOOR SLAB
  27. LOCATION OF AIR INTAKE MODIFIED
  28. EXTENT OF RESTAURANT REDUCED BY 136m<sup>2</sup>
  29. CLUB ENTRANCE ACCESSIBLE RAMPS MODIFIED
  30. ONE ADDITIONAL EXISTING CAR PARK SPACE TO BE REMOVED FROM EXISTING NORTHERN CAR PARK
  31. SOLID MASONRY WALL ADDED BETWEEN EXISTING ZONE D CAR PARK AND LOADING DOCK ACCESS.

- NEW CLUB AND CAR PARK
- ALTERATIONS TO EXISTING CLUB AND CAR PARK
- NO NEW WORKS TO EXISTING CLUB

NOTE 1: LANDSCAPING. REFER TO LANDSCAPE ARCHITECTS DOCUMENTS AND PERSPECTIVE VIEWS WHERE APPROPRIATE

CLIENT:  
DEE WHY RSL CLUB

PROJECT:  
STAGE 5 - CLUB EXTENSION  
PHASE 2 WORKS - SOUTHERN  
CARPARK & ENCLOSURE

ARCHITECT:  
**ALTIS**  
architecture  
p 61 2 9364 9000 f 61 2 9571 7930 lower deck jones bay wharf  
suite 123 / 26 - 32 pirrama road pyrmont nsw 2009 australia

DRAWING TITLE:  
PROPOSED ELEVATIONS

NOTES:  
Nominated architect: Rofe Lattimer - 5535  
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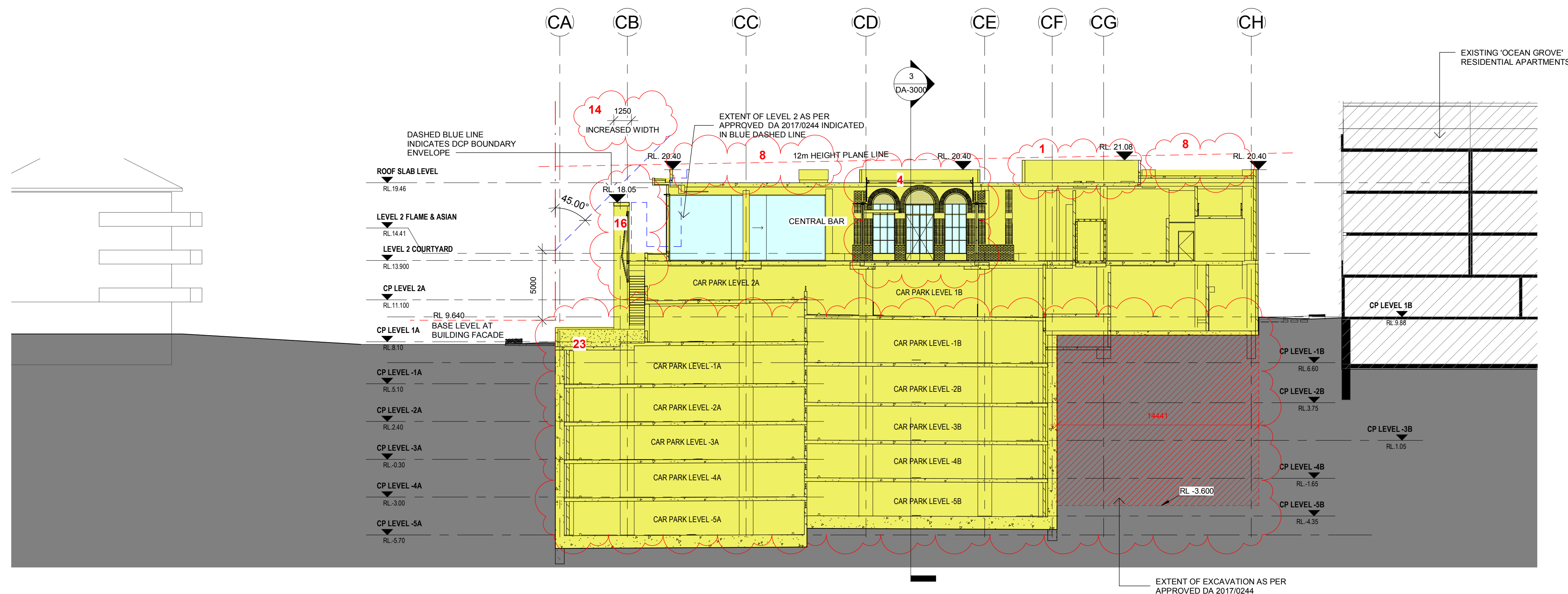
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As indicated @ B1  
DRAWN BY: MC  
CHECKED BY: DOC  
PROJECT NO: 2341.06  
CADD FILE:

DATE:  
MARCH 2018  
DRAWING NO: DA-2100  
ISSUE: 2

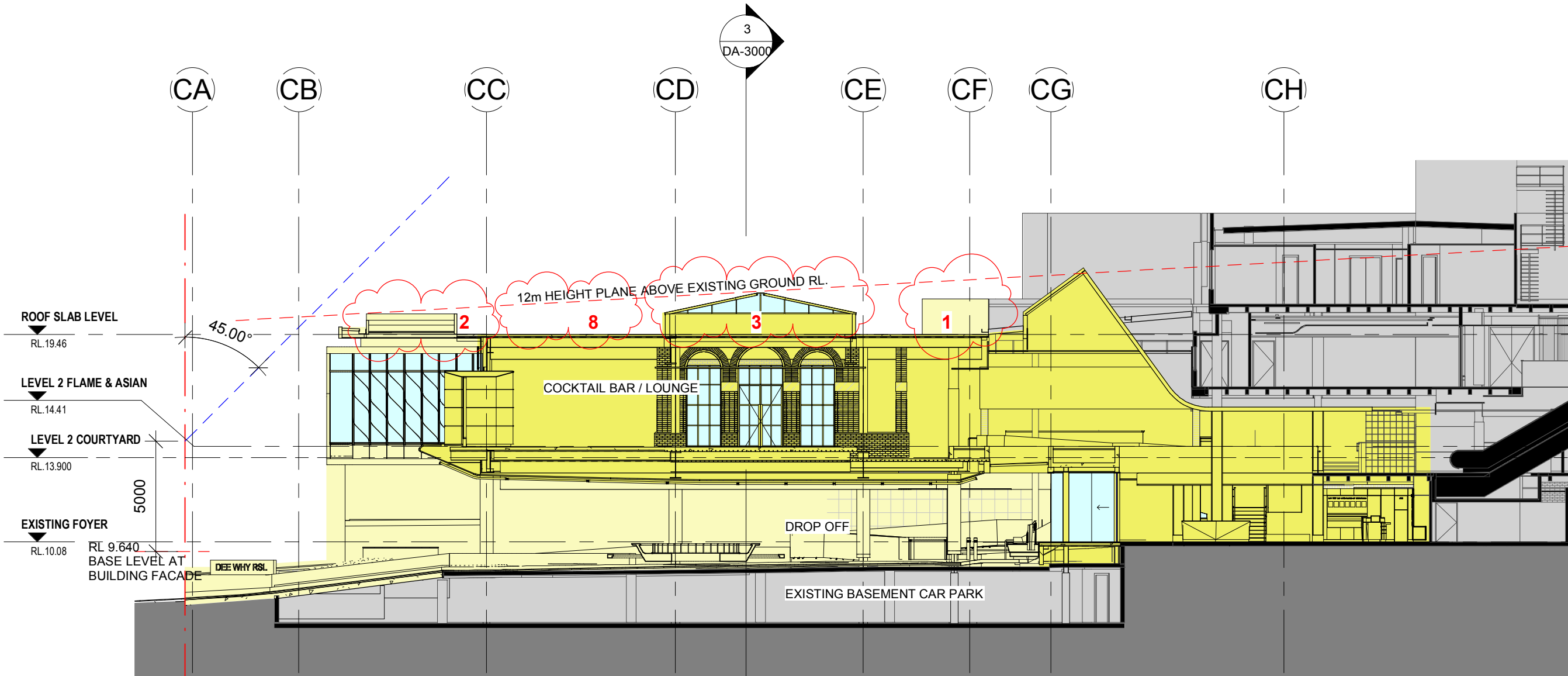
SECTION 96

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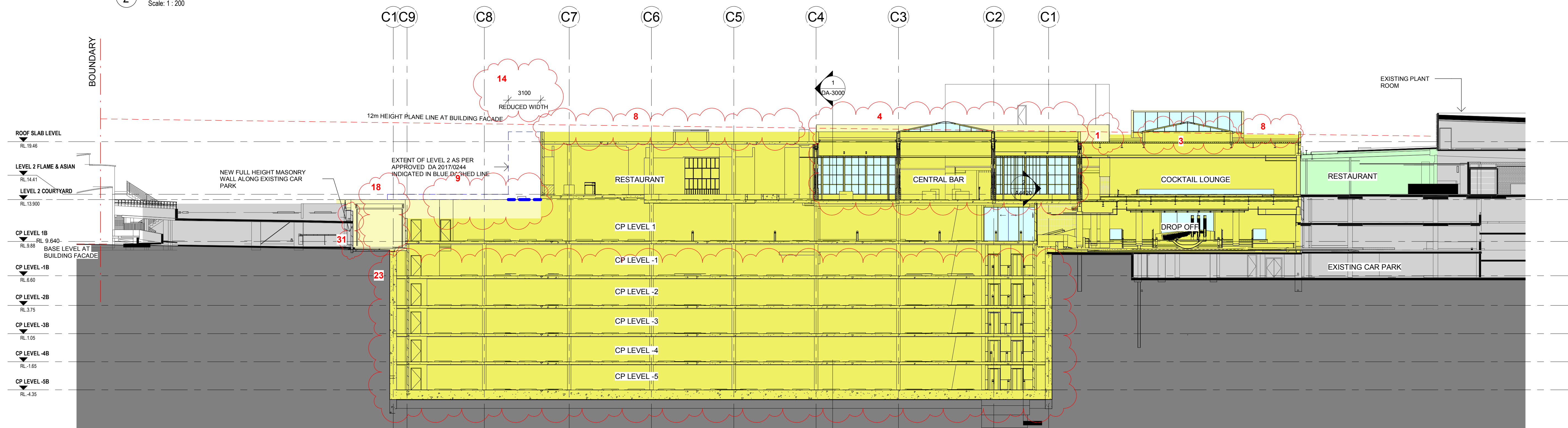




1 SECTION 06  
Scale: 1 : 200



2 SECTION  
Scale: 1 : 200



3 SECTION  
Scale: 1 : 200

ISSUE	DATE	DESCRIPTION
1	17.03.2017	ISSUED FOR DEVELOPMENT APPLICATION
2	05.03.2018	ISSUED FOR SECTION 96

- CHANGES FOR SECTION 96 APPROVAL
1. RELOCATED OPEN PLANTROOM AT THE ROOF LEVEL
  2. MODIFIED ROOF OPENING ABOVE MAIN ENTRANCE
  3. MODIFIED CENTRAL SKYLIGHT (REDUCED HEIGHT, MODIFIED PITCH)
  4. MODIFIED COURTYARD: TWO OPEN COURTYARDS AND ONE SKYLIGHT COVERED AREA INSTEAD OF ONE LARGE GLASS ROOF. REDUCED HEIGHT AND PITCH
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  6. REDUCED NUMBER OF ROOF SKYLIGHTS ABOVE ASIAN RESTAURANT: 2 INSTEAD OF 6 TO SUIT THE NEW INTERNAL LAYOUT
  7. MODIFIED EXIT STAIR: POSITION, CONFIGURATION, METAL ROOF NO LONGER REQUIRED
  8. CONCRETE ROOF INSTEAD OF METAL DECK ROOF FOR THE MAJORITY OF THE BUILDING EXTENSION
  9. NO SLAB ABOVE THE EXPOSED CAR PARK LEVEL 2 IN THE SOUTH AREA
  10. NEW ACOUSTIC METAL ROOF ABOVE REVISED LOADING DOCK
  11. MODIFIED FACADE APPEARANCE DUE TO CHANGES IN THE CAR PARK LEVELS, LOADING DOCK AND EGRESS STAIRS
  12. REMOVED LOUVRES ON THE BACK FACADE (WESTERN FACADE) DUE TO MECHANICAL RISER NO LONGER REQUIRED / REVISED LOCATION FOR EGRESS DOORS
  13. 2m WIDE INTERNAL EGRESS STAIR MODIFIED INTO TWO 1m WIDE EXTERNAL STAIRS WITH DECORATIVE BALUSTRADES
  14. EXTENT OF LEVEL 2 (MAIN TRADING LEVEL) REDUCED BY 8326mm FROM THE SOUTHERN BOUNDARY AND INCREASED BY 1100mm TO THE EASTERN BOUNDARY
  15. MODIFIED FACADE CURTAIN WALL MULLION BLADES
  16. CONCRETE BLADE WALL REDUCED BY 1.47m IN HEIGHT AND 6.2m IN LENGTH
  17. VERTICAL FINIS REMOVED FROM BACK WALL (WESTERN FACADE)
  18. REVISED LOADING DOCK ENTRANCE AND LOCATION/ ENLARGED DEMOLISHED AREA IN THE EXISTING RETAINED SOUTHERN CAR PARK
  19. NEW / MODIFIED EGRESS PATHS FROM EXISTING CARPARK LEVEL -1. NEW CARPARK LEVEL 1.
  20. RELOCATED ELECTRICAL SUBSTATION / LANDSCAPED AREA MOVED TO SUIT
  21. MEDIAN STRIP ADDED AT ENTRY TO PROVIDE PLACE OF REFUGE AND ENSURE WIDTH OF ENTRY/ EXIT IS LESS THAN 8M AS PER DA CONDITION 23
  22. RELOCATED STORMWATER OVERFLOW PATH
  23. EXTENT OF BASEMENT CAR PARK LEVELS REDUCED. BASEMENT REDUCED BY 14394mm FROM THE WEST BOUNDARY. BASEMENT ENLARGED BY 280mm TOWARDS THE SOUTH BOUNDARY. CARPARK CHANGED FROM FLAT PLATE TO SPLIT LEVEL ON ALL LEVELS. DEPTH OF CAR PARK INCREASED FROM RL -3.600 TO RL -5.700. OVERALL PARKING SPACES REDUCED FROM 687 TO 680
  24. BASEMENT CAR PARK LEVEL -5 ADDED.
  25. CAR PARK ENTRY / EXIT MOVED TOWARD NORTH BY 3044mm
  26. PLANTER MODIFIED ON THE BACK (WESTERN) FACADE DUE TO REDUCTION OF THE MAIN TRADING FLOOR SLAB
  27. LOCATION OF AIR INTAKE MODIFIED
  28. EXTENT OF RESTAURANT REDUCED BY 136m<sup>2</sup>
  29. CLUB ENTRANCE ACCESSIBLE RAMPS MODIFIED
  30. ONE ADDITIONAL EXISTING CAR PARK SPACE TO BE REMOVED FROM EXISTING NORTHERN CAR PARK
  31. FULL HEIGHT SOLID MASONRY WALL ADDED BETWEEN EXISTING ZONE D CAR PARK AND LOADING DOCK ACCESS.

- NEW CLUB AND CAR PARK
- ALTERATIONS TO EXISTING CLUB AND CAR PARK
- NO NEW WORKS TO EXISTING CLUB

CLIENT:  
DEE WHY RSL CLUB

PROJECT:  
STAGE 5 - CLUB EXTENSION  
PHASE 2 WORKS - SOUTHERN  
CARPARK & ENCLOSURE

ARCHITECT:  
**ALTIS**  
architecture  
p 61 2 9364 9000 f 61 2 9571 7930 lower deck jones bay wharf  
suite 123 / 26 - 32 pirrama road pyrmont nsw 2009 australia

DRAWING TITLE:  
PROPOSED SECTIONS

NOTES:  
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NORTH:

SCALE: As indicated @ B1	DATE: MARCH 2018
DRAWN BY: MC	CHECKED BY: DOC
PROJECT NO: 2341.06	DRAWING NO: DA-3000
CAD FILE:	ISSUE: 2

## SECTION 96

8/3/2018 5:09:28 PM