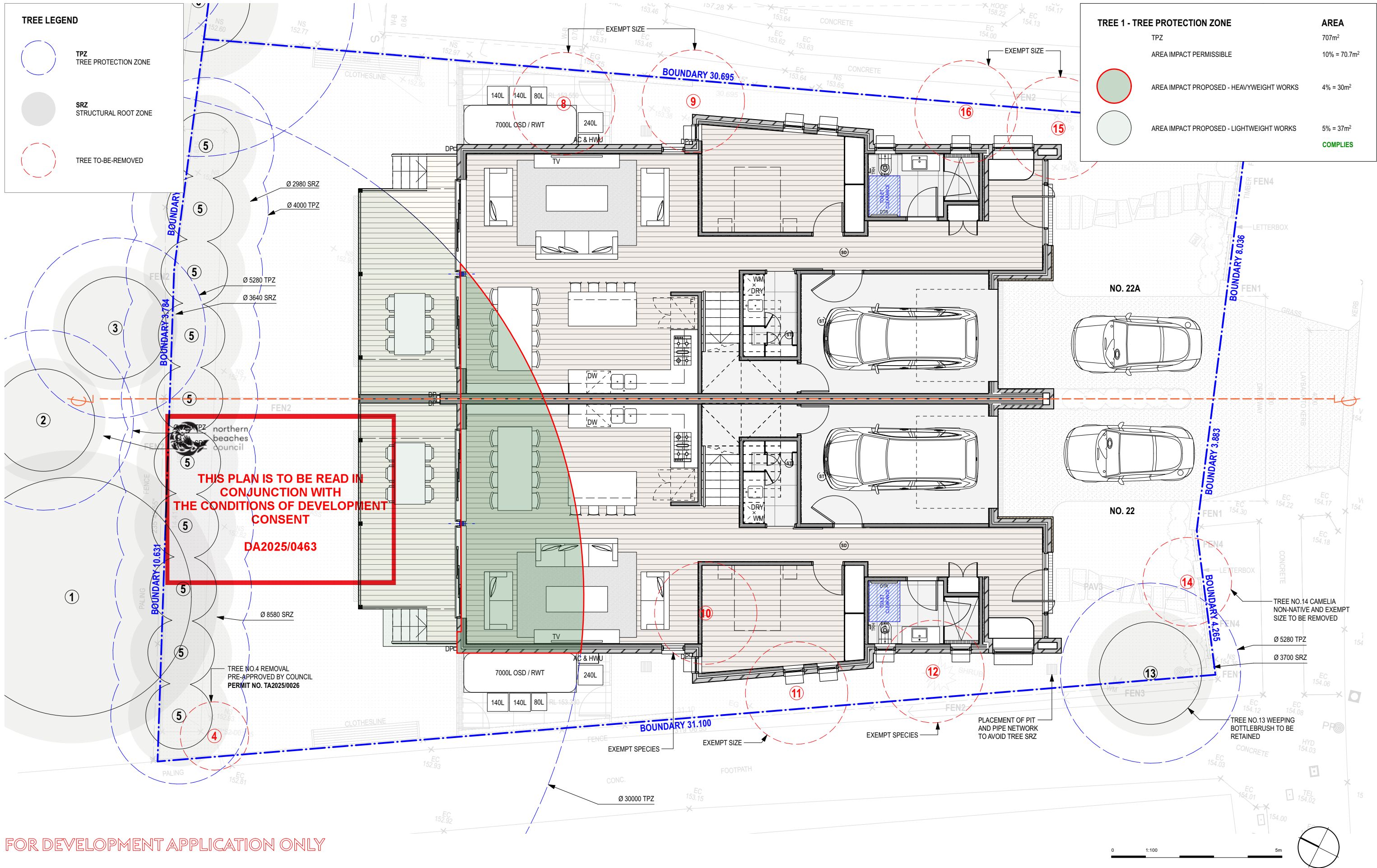


This aerial map displays a residential area in Warrington, NSW. The map shows several streets: Warringah Road, Karingah Road, Karingah Crescent, and Akora Street. Property boundaries are outlined in black, and lot numbers are visible on many of the lots. Lot 108A is highlighted in orange and labeled with #22A and #22. Other lot numbers visible include 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107A, 109, 110, 111, 121, 60, 61, 62, and 28.

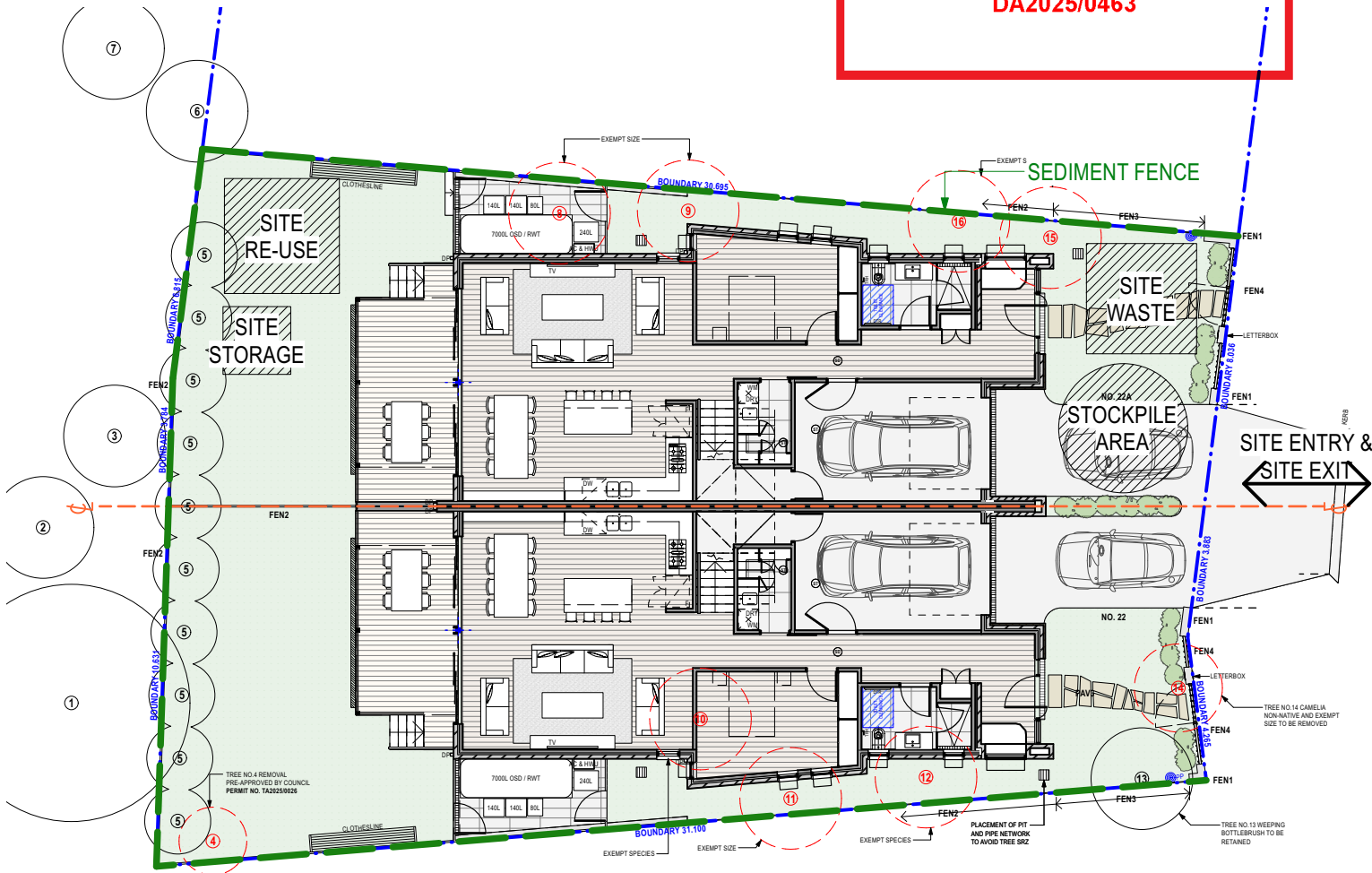




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REFER TO STORMWATER ENGINEER'S PLANS
FOR STORMWATER MANAGEMENT

1 SITE ANALYSIS PLAN
SCALE 1:200

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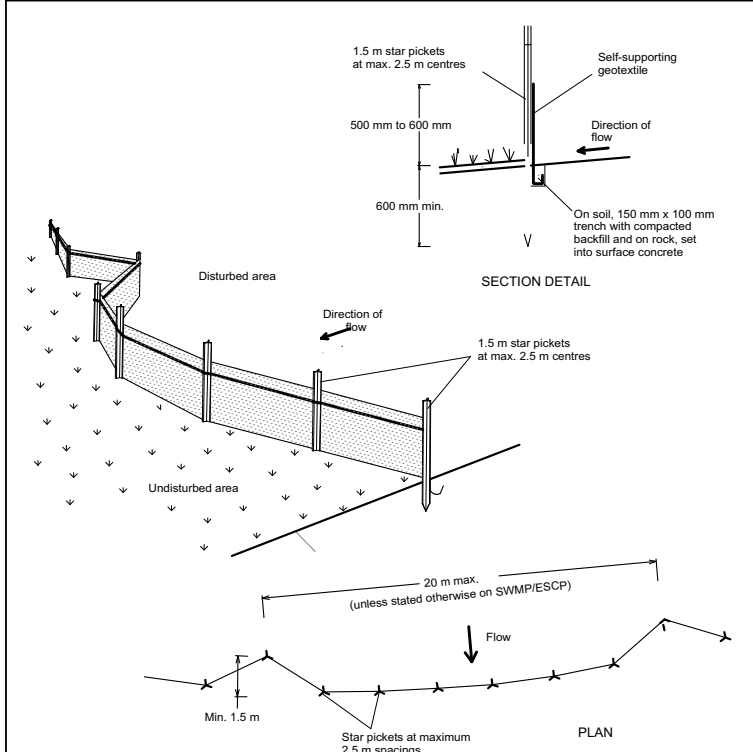
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NEW ATTACHED DWELLING
22 KARINGAL CRESCENT FRENCHS FOREST

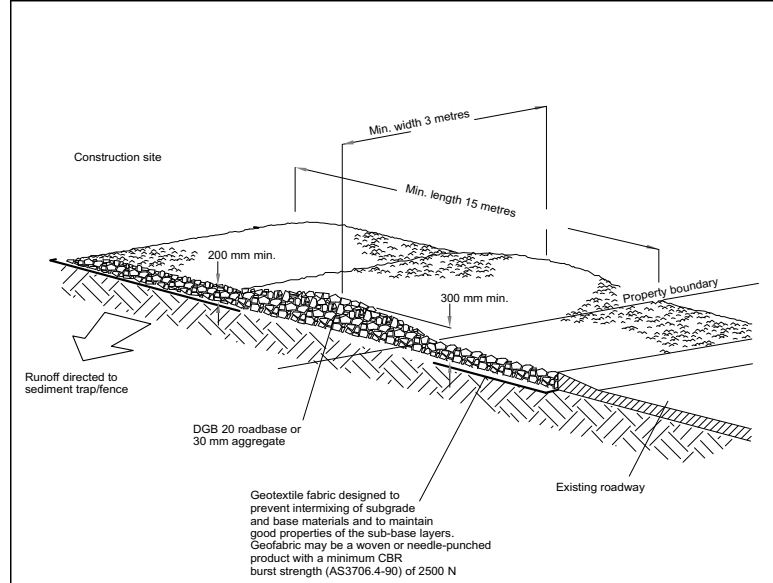


Construction Notes

1. Construct sediment fences as close as possible to being parallel to the contours of the site, but with small returns as shown in the drawing to limit the catchment area of any one section. The catchment area should be small enough to limit water flow if concentrated at one point to 50 litres per second in the design storm event, usually the 10-year event.
2. Cut a 150-mm deep trench along the upslope line of the fence for the bottom of the fabric to be entrenched.
3. Drive 1.5 metre long star pickets into ground at 2.5 metre intervals (max) at the downslope edge of the trench. Ensure any star pickets are fitted with safety caps.
4. Fix self-supporting geotextile to the upslope side of the posts ensuring it goes to the base of the trench. Fix the geotextile with wire ties or as recommended by the manufacturer. Only use geotextile specifically produced for sediment fencing. The use of shade cloth for this purpose is not satisfactory.
5. Join sections of fabric at a support post with a 150-mm overlap.
6. Backfill the trench over the base of the fabric and compact it thoroughly over the geotextile.

SEDIMENT FENCE

SD 6-8

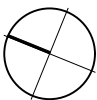


Construction Notes

1. Strip the topsoil, level the site and compact the subgrade.
2. Cover the area with needle-punched geotextile.
3. Construct a 200 mm thick pad over the geotextile using road base or 30 mm aggregate.
4. Ensure the structure is at least 15 metres long or to building alignment and at least 3 metres wide.
5. Where a sediment fence joins onto the stabilised access, construct a hump in the stabilised access to divert water to the sediment fence

STABILISED SITE ACCESS

SD 6-14



EROSION & SEDIMENT CONTROL PLAN

2416

1:200 @ A3

A003 A

JANUARY 2025

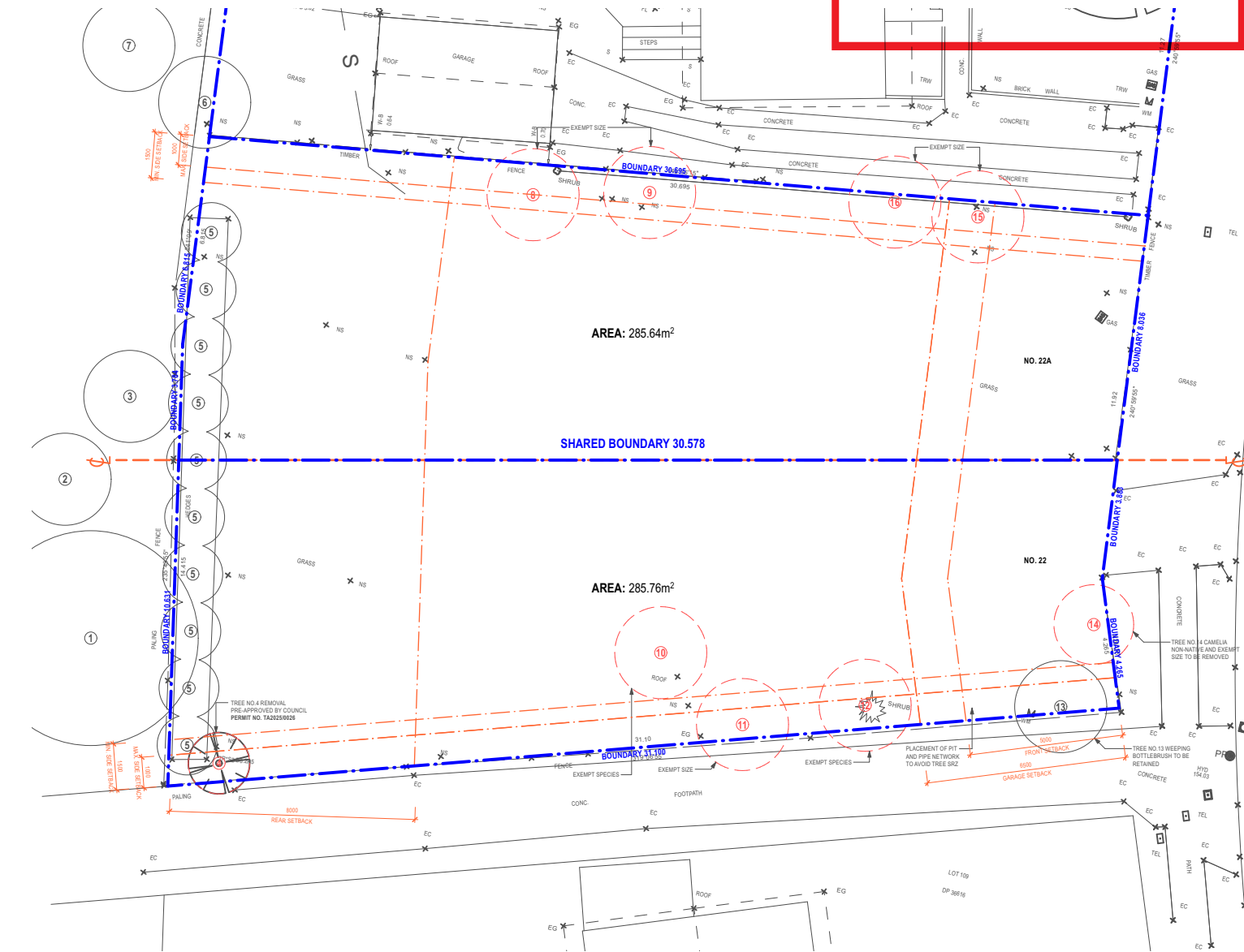
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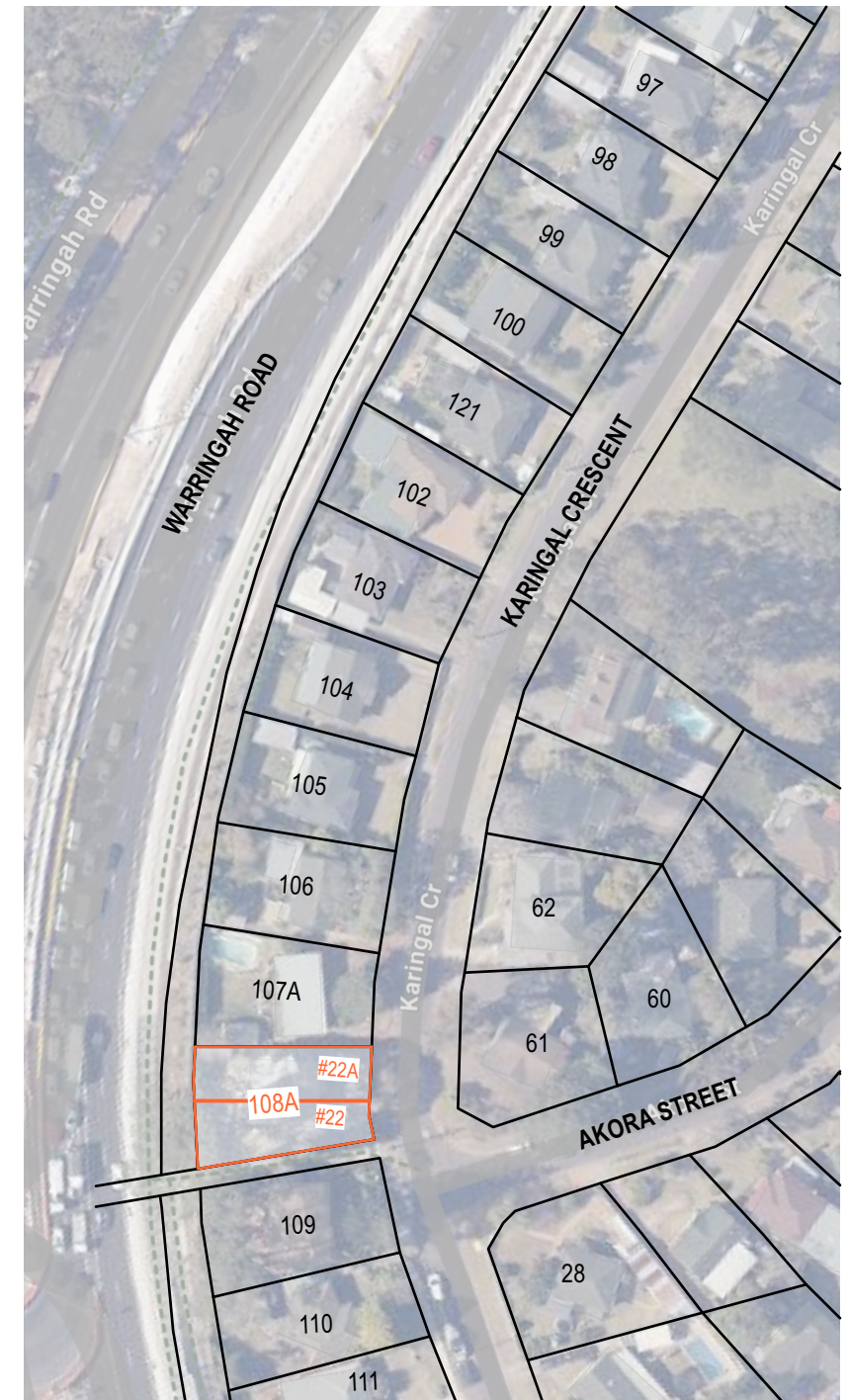
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1 SUBDIVISION PLAN
SCALE 1:200



2 SITE PLAN
NOT TO SCALE

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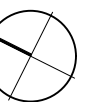
NEW ATTACHED DWELLING
22 KARINGAL CRESCENT FRENCHS FOREST

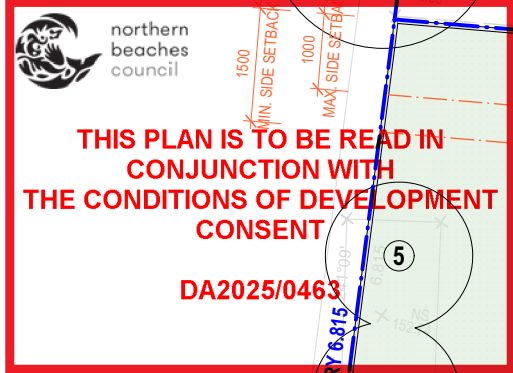
SUBDIVISION PLAN

2416
1:200 @ A3

A004 A
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FIRST FLOOR PLAN

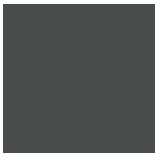
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EXTERNAL FINISHES




EP1
LINEA WEATHERBOARD
PAINTED LIGHT GREY



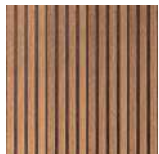
EP2
RENDERED INSULCLAD
DARK GREY




EP3
WALL
PAINTED MID GREY




MR
METAL ROOF
COLORBOND MID GREY
SA: 0.57 MEDIUM




WC1
FEATURE WALL CLADDING
TIMBER BATTENS




SSC
FEATURE WALL CLADDING
SANDSTONE



TC
FRONT DOOR
SOLID TIMBER



GL1
ALUMINIUM FRAMED
WINDOWS AND DOORS
POWDERCOAT MONUMENT
(UNO)



AWN
METAL AWNING





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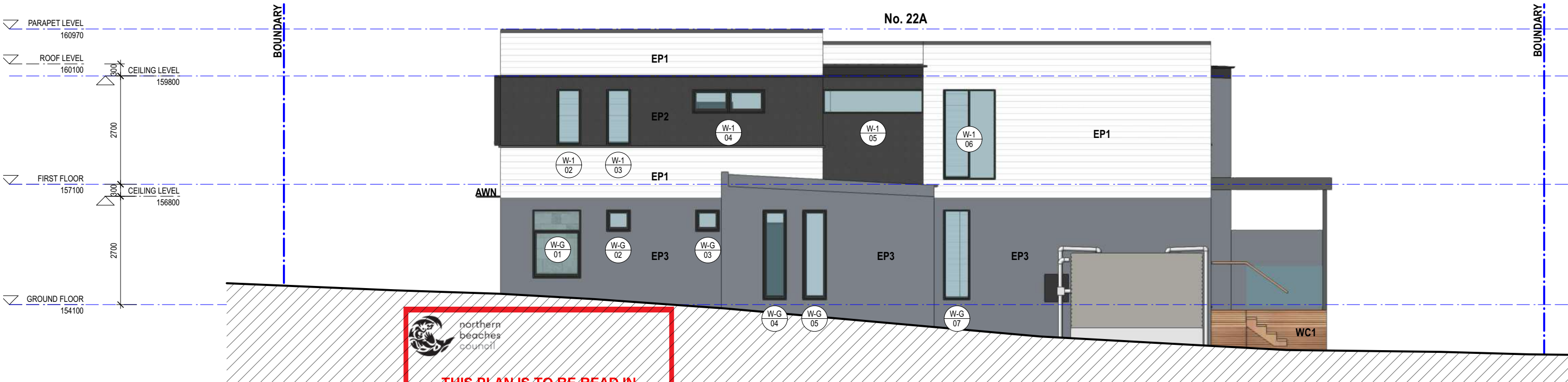
NORTH AND SOUTH ELEVATIONS

2416
1:100 @ A3

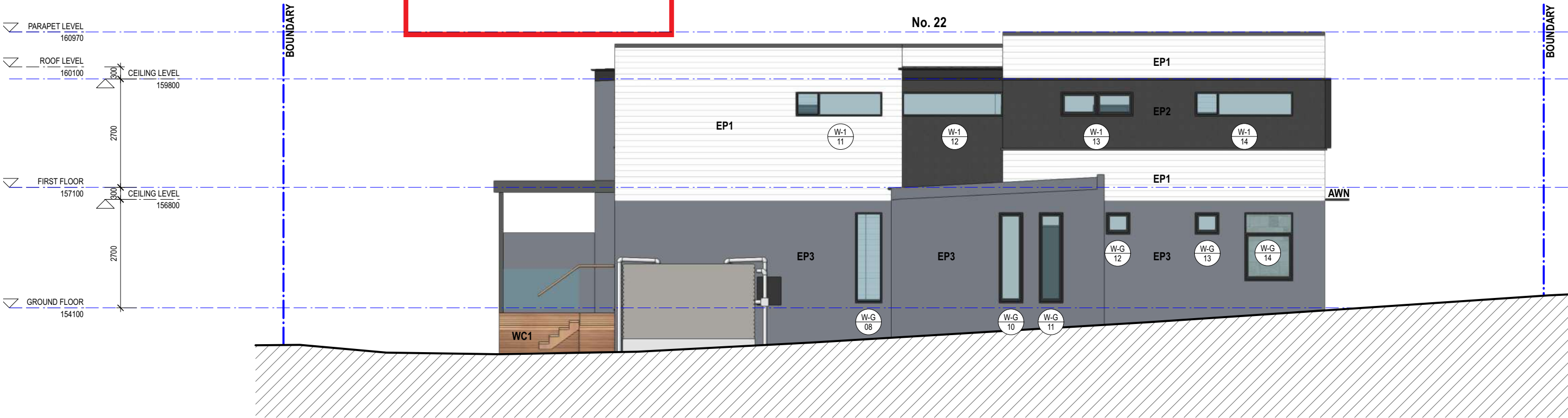
A201
JANUARY 2025

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1 EAST ELEVATION
SCALE 1:100



2 WEST ELEVATION
SCALE 1:100

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EAST AND WEST ELEVATION

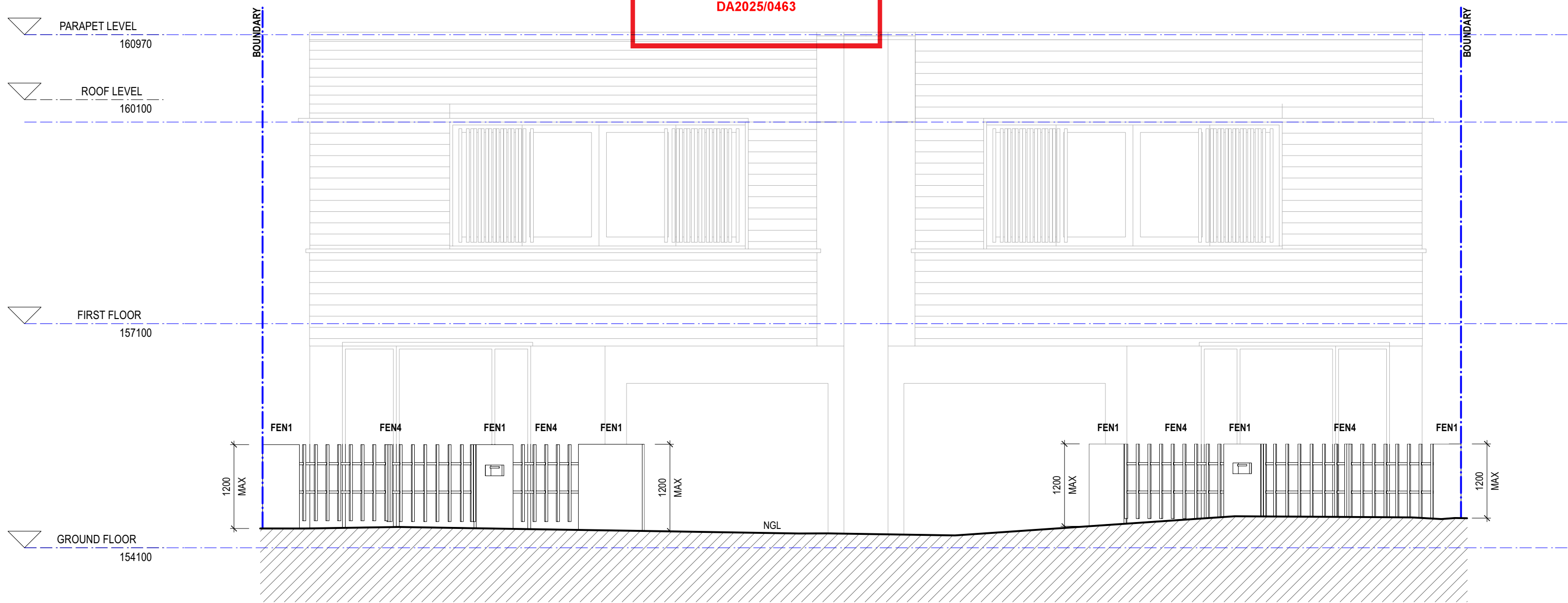
2416 A202 A
1:100 @ A3 JANUARY 2025

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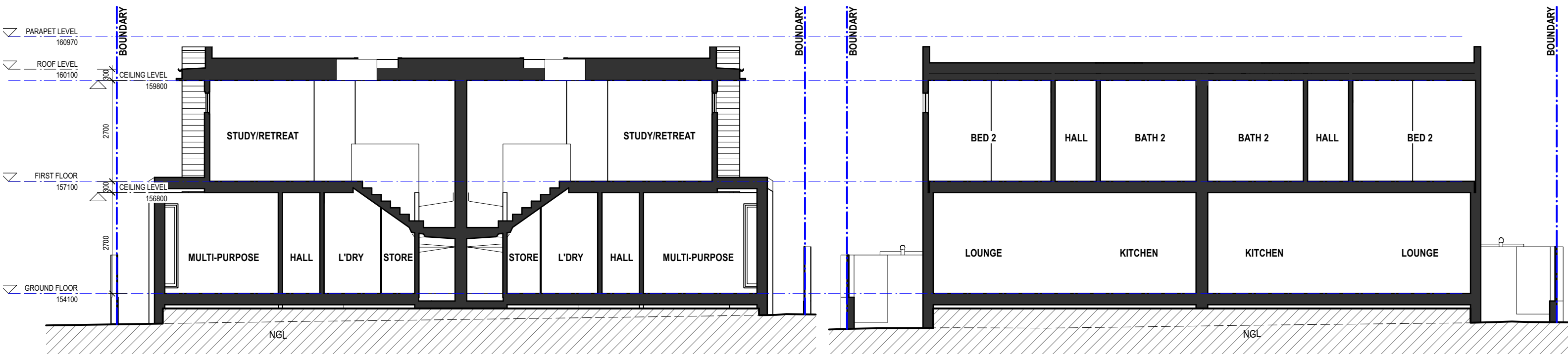
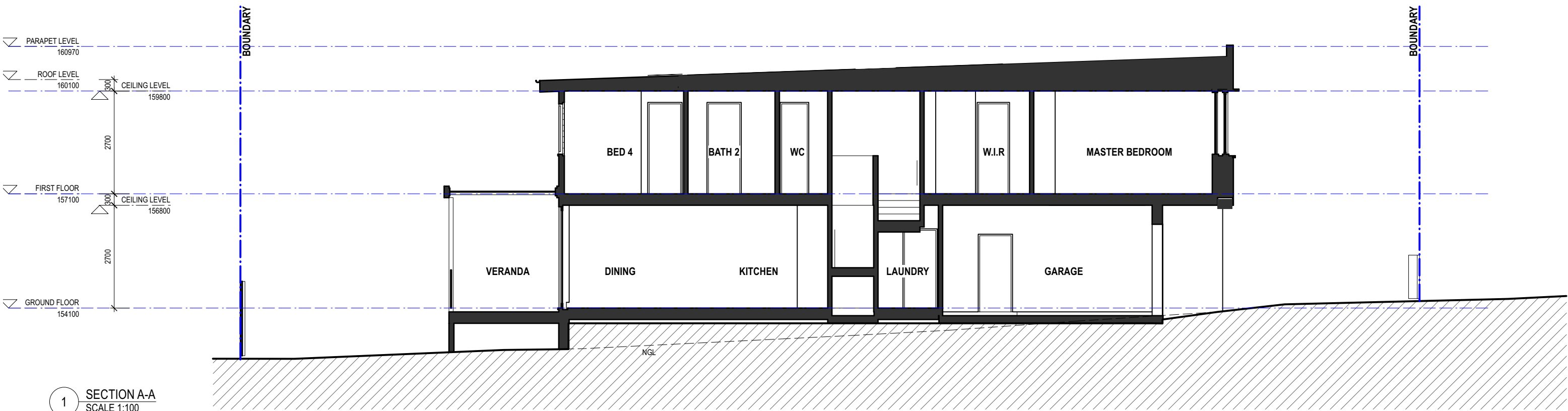
FRONT FENCE ELEVATION


2416
1:50 @ A3

A203 A
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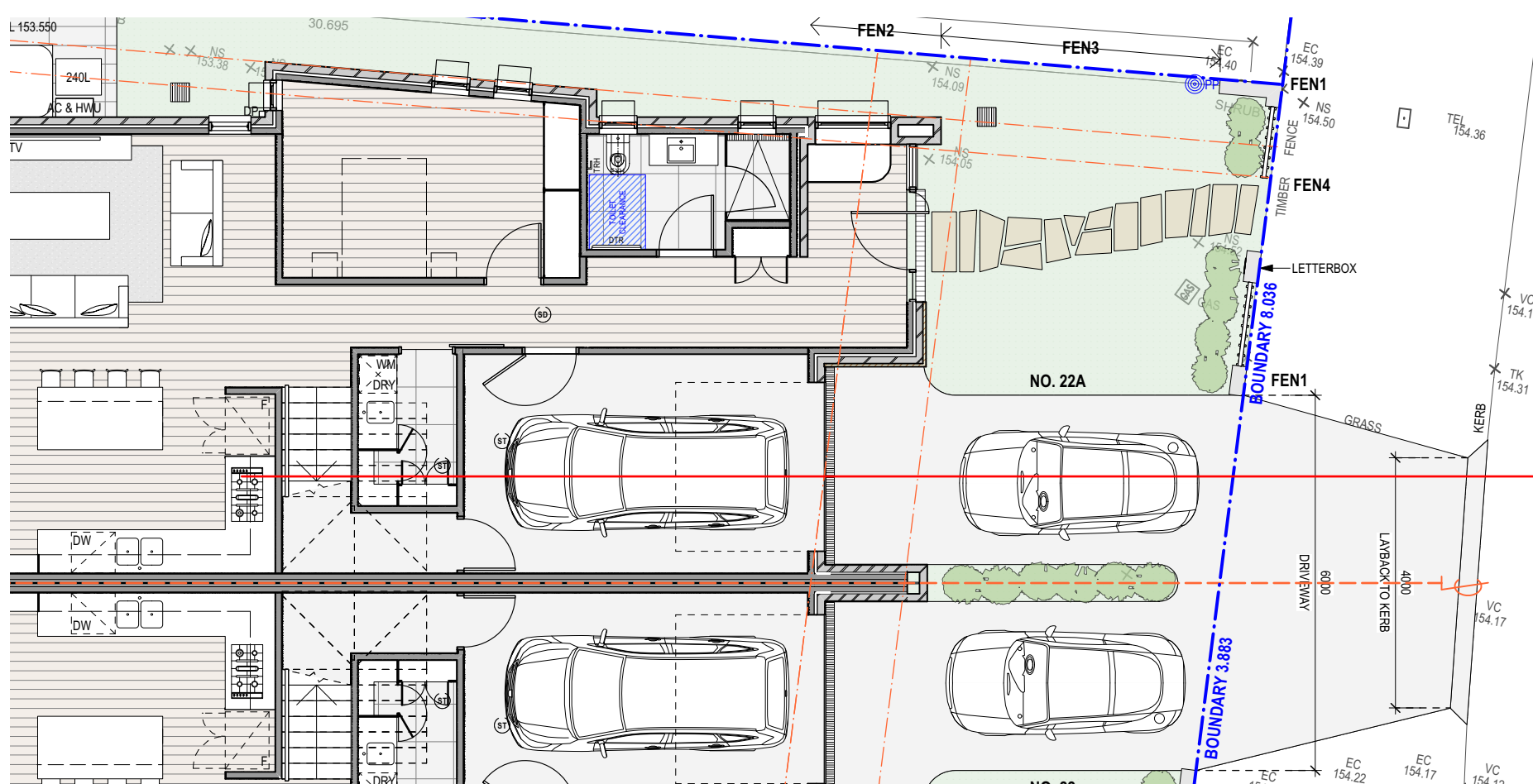
SECTION A-A & B-B & C-C

2416
1:100 @ A3

A221 A
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NOTE:
- DRIVEWAY TO BE CONSTRUCTED ON GRADE AND RAISED TO INCLUDE SUB-BASE CONSTRUCTION
- APPROXIMATELY 100mm OF SUB-BASE WILL BE REQUIRED UNDER DRIVEWAY
- WHERE HATCHED, ARBORIST HAS INDICATED AS AREAS REQUIRING SPECIAL ATTENTION WITH NO EXCAVATION OR HEAVY MACHINERY ALLOWED
- SIDE OF DRIVEWAY TO BE MOUNDED AND LANDSCAPED DOWN TO EXISTING GROUND LEVEL

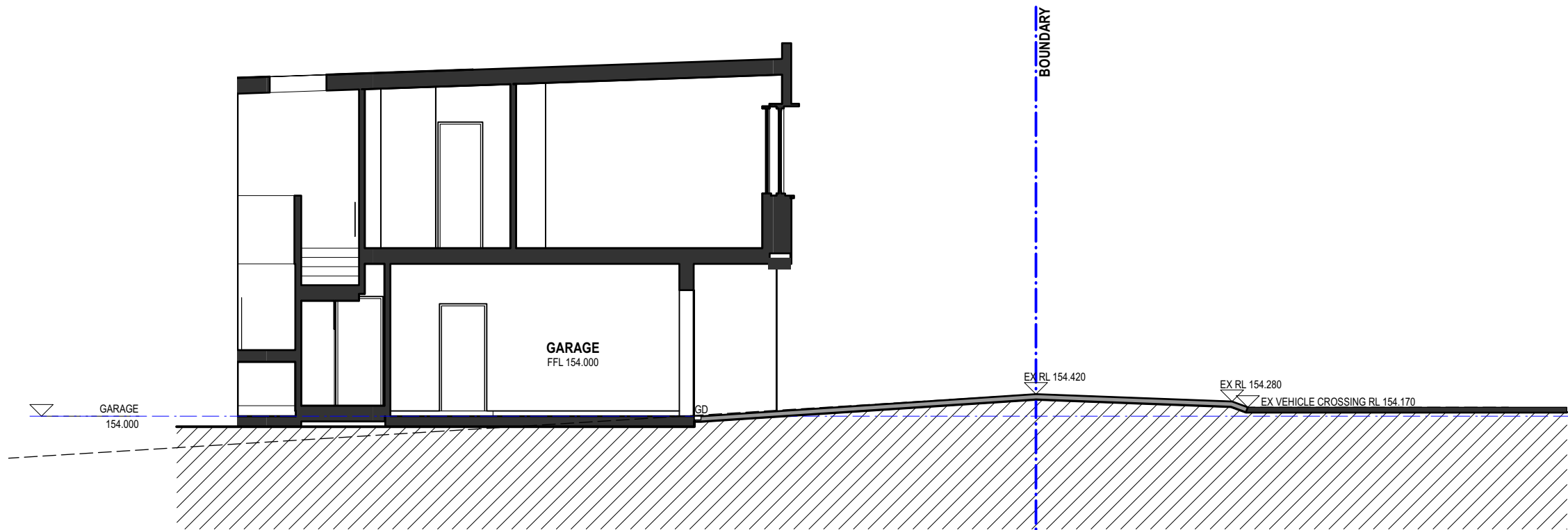


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1 PLAN
SCALE 1:100



2 SECTION
SCALE 1:100

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DRIVEWAY PROFILE

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