



13 PRINCE EDWARD ROAD SEAFORTH

**STATEMENT OF ENVIRONMENTAL EFFECTS FOR
ALTERATIONS AND ADDITIONS TO AN EXISTING DWELLING INCLUDING
THE CONSTRUCTION OF A SINGLE CARPORT**



Report prepared for
Mathew Hodges
December 2024

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1. Introduction

- 1.1 This is a statement of environmental effects for alterations and additions to an existing dwelling including a new carport and landscaping, at 13 Prince Edward Road, Seaforth.
- The report describes how the application addresses and satisfies the objectives and standards of the Manly Local Environmental Plan 2013, the Manly Development Control Plan 2013 and the heads of consideration listed in Section 4.15 of the Environmental Planning and Assessment Act 1979 (as amended).
- 1.2 This statement of environmental effects has been prepared with reference to the following:
- ◆ Site visit
 - ◆ Survey Plan prepared by ENG Land Services
 - ◆ Drawings prepared by Lone Pine Landscapes
 - ◆ Arborist Report prepared by Urban Arbor
 - ◆ Waste Management Plan
- 1.3 The proposed development is consistent with the objectives of all Council controls, considerate of neighbouring residents and will result in improved amenity for the residents of the site. It is an appropriate development worthy of Council consent.

2. The site and its locality

- 2.1 The subject site is located on the eastern side of Prince Edward Road, approximately 60 metres south of its intersection with Armstrong Street in Seaforth. It is legally described as Lot 154 DP 11162 and is known as 13 Prince Edward Road, Seaforth.
- 2.2 It is a rectangular shaped lot with front and rear boundary of 12.19 metres and side boundaries of 42.67 metres.
- 2.3 The site has an area of 518.5m² and is currently occupied by a two storey clad dwelling with a metal roof and hardstand at the front of the site. The lot slopes from west to east (front down to the rear), by approximately 4 metres.
- 2.4 The property is surrounded by detached residential dwellings in all directions and is in close proximity to Wakehurst Parkway and Garigal National Park to the west. Seaforth Shopping Village lies to the south-east and transport services are available on Wakehurst Parkway and Lister Avenue.

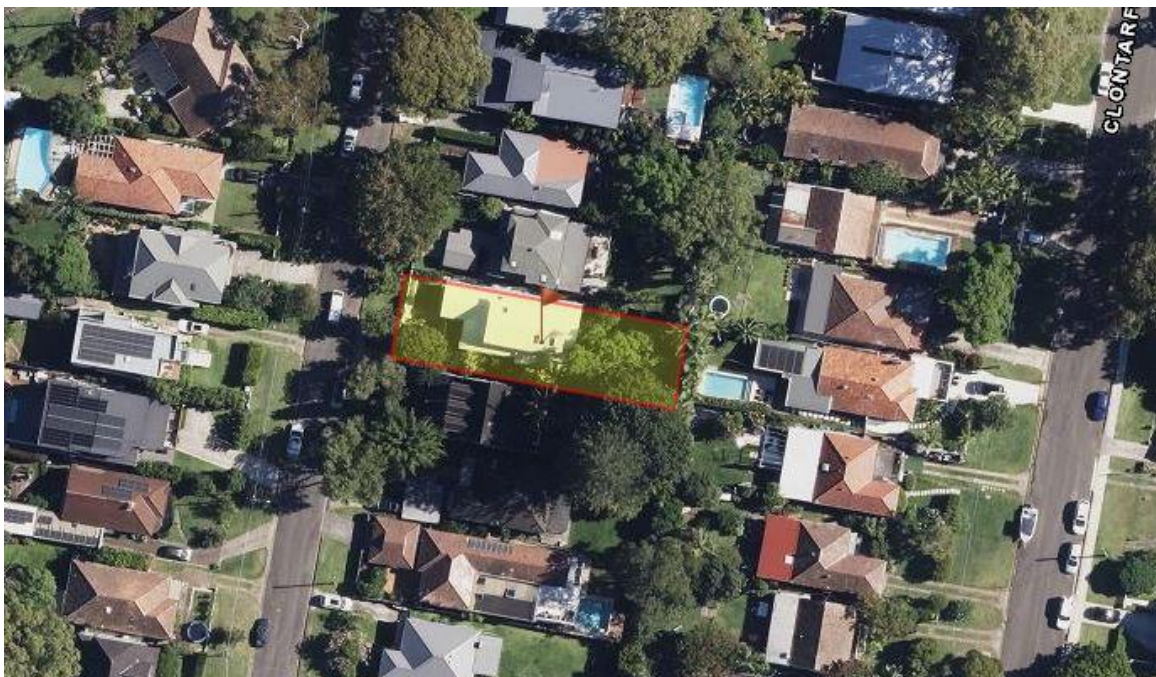


Figure 1. The site and its immediate surrounds

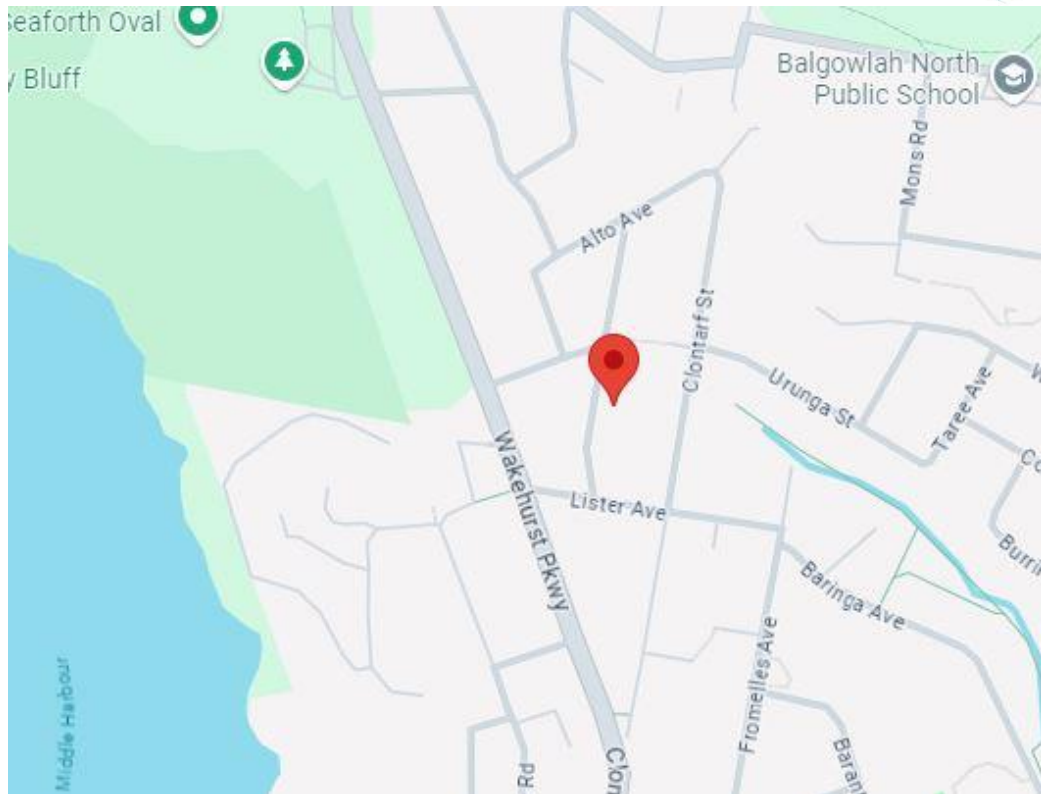


Figure 2. The site within the locality

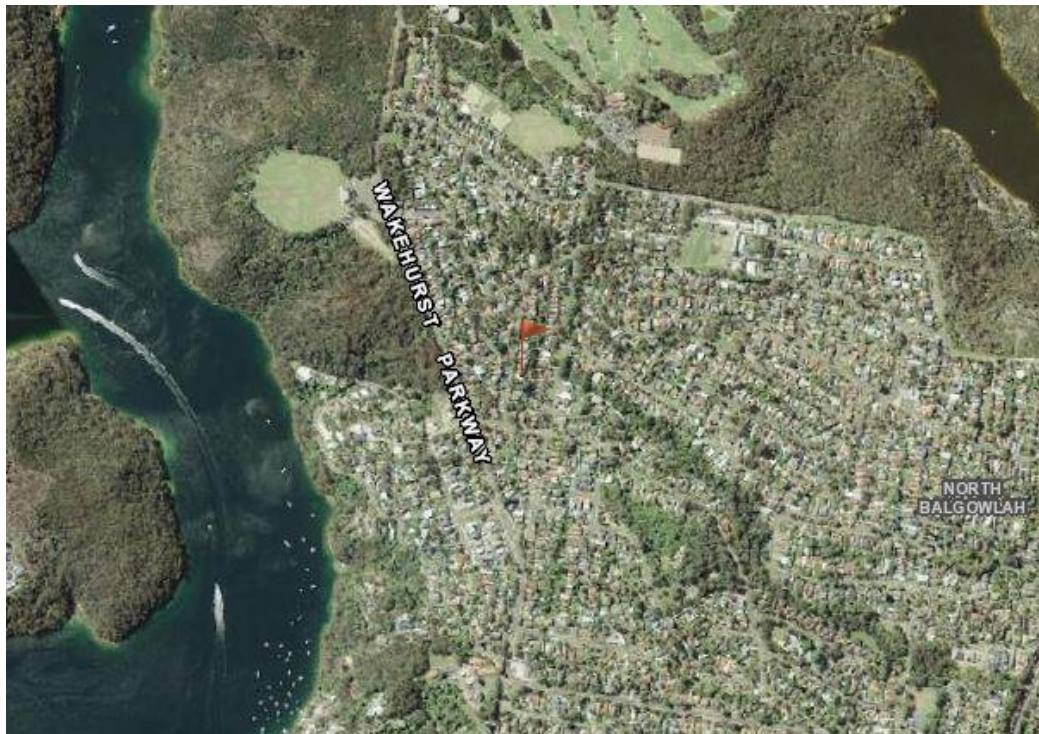


Figure 3. Aerial image of the site within the locality

3. Site Photos



Figure 4. The existing dwelling and tree on nature strip, looking east from Prince Edward Road.



Figure 5. The existing hardstand at the front of the dwelling looking east.



Figure 6. The front yard – looking north.



Figure 7. The front yard and decking – looking north-west.



Figure 8. The existing hardstand and front entrance to the property – looking west.

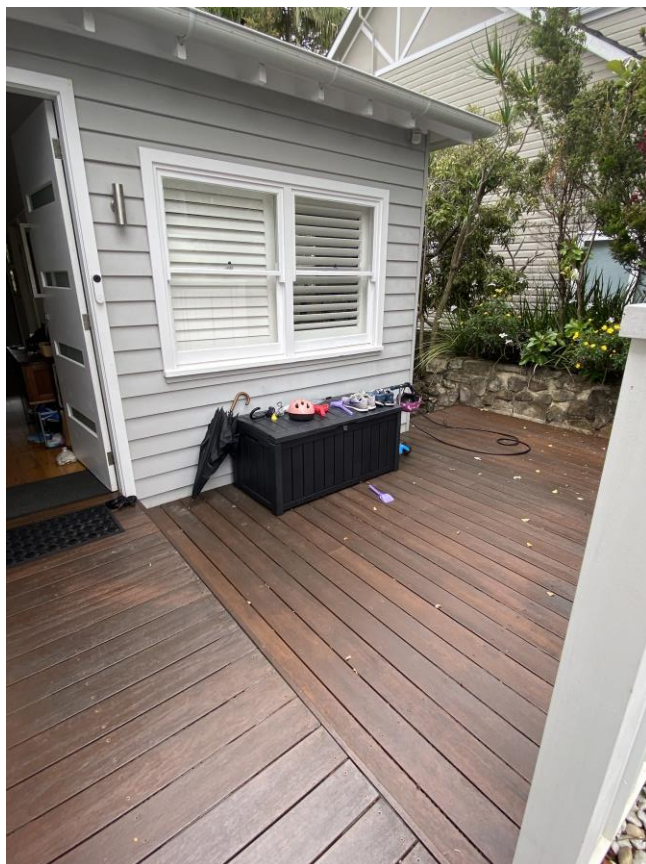


Figure 9. Front deck to the existing dwelling – looking south-east.

4. Proposed Development

The proposed development is for alterations and additions to the existing dwelling including a new open-sided carport, front fence, entry gate and landscaping on the subject site.

The development proposes weatherproof parking for the residents of this family sized home. The proposal is consistent with Council controls and ensures privacy is maintained for both neighbours and the subject site.

The proposed development is made up as follows:

- Demolish:
 - crossover and hardstand,
 - existing front fence,
 - front timber decking and retaining wall adjacent to deck.

- Construct:
 - New crossover, driveway and hardstand (retain existing layback),
 - New timber carport with plant room at rear, privacy screen to the south and north, and door to carport,
 - Colorbond steel roof to carport to match existing roof of dwelling,
 - Natural stone paving to replace deck to the rear of the carport and new retaining wall to replace existing on the southern boundary,
 - New masonry and timber front fence (1.2m high), gate and path,
 - New synthetic grass on the northern side of the garden with new timber decking to replace existing and new plantings in the garden,
 - Screen planting behind new front fence.

- Tree removal and replacement:
 - Removal of one (1) street tree adjacent to the existing crossover within the nature strip, and replacement with Council approved tree,
 - Removal of one (1) tree on the southern side of hardstand,
 - Additional planting within front setback, as per Landscape Plan.

5. Statutory Framework

5.1 State Environmental Planning Policies

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017 aims to protect biodiversity values and preserve the amenity of non-rural areas, through the preservation of trees and other vegetation. Although the development proposes tree removal it remains consistent with the provisions of the SEPP.

See attached Arborist Report.

State Environmental Planning Policy No. 55 – Remediation of Land

The subject site has historically been used for residential purposes and no change of use is proposed. There is no known reason to suspect the site may be contaminated and the consent authority can be satisfied that the land is suitable for the continued residential use.

5.2 Manly Local Environmental Plan 2013

The relevant clauses of the Manly Local Environmental Plan 2013 are addressed below.

Zoning

The site is zoned R2 Low density Residential pursuant to the provisions of the Manly Local Environmental Plan 2013. The proposed development is for alterations and additions to an existing dwelling which are permitted with development consent in the R2 Low density Residential zone.



Figure 10. Extract from Manly LEP 2013 zoning map

Demolition

Minor demolition works are proposed, as described above and illustrated in the attached DA plan set, to allow for the construction of the proposed alterations and additions.

Minimum Lot Size

The site is mapped with a minimum subdivision lot size of 500m². The subject site comprises of a compliant area of 518.5m² and no subdivision is proposed.

Height of Buildings

The LEP restricts the height of any development on the subject site to 8.5 metres. The existing dwelling has a compliant maximum building height and no change is proposed. The development proposes a compliant maximum building height of 4.1 metres for the new carport.

Floor Space Ratio

A maximum floor space ratio of 0.45:1 is permitted for the site which equates to 233.3m² for the site area of 518.5m². No change is proposed to the existing FSR on the site.

Heritage Conservation

The site is not a heritage item, located within a heritage conservation area, or located in proximity to any heritage items.

Acid Sulfate Soils

The site is not located in an area nominated as impacted by Acid Sulfate soils.

Development on Sloping Sites

The proposal is designed to ensure a high standard of amenity from public and private spaces.

The subject site is not identified as Landslip Risk in the LEP map.

Earthworks

No significant earthworks are proposed. Some minor earthworks will be required for the construction of the carport and associated works. All works will be undertaken in accordance with engineering specifications, Councils controls and any consent conditions.

Stormwater Management

Stormwater has been designed to comply with Council requirements. See accompanying Stormwater Plan prepared by Lone Pine Landscapes.

Essential services

All services are existing on the site.

5.3 Manly Development Control Plan 2013.

The relevant sections of the DCP are addressed below.

3. General Principles of Development

3.1 Streetscape and Townscapes

The subject site has frontage to and is visible from Prince Edward Road. Prince Edward Road is characterised by one and two storey residential dwellings, various community facilities including a school and places of worship and commercial premises. The proposal will remain consistent with the existing dwelling character and will complement the streetscape.

The new open sided carport has been designed to be consistent with the design of the dwelling and presents well to the street frontage. The proposed development maintains and improves the character of the area through appropriate design and architectural features.

Complementary Design and Visual Improvement

The proposal will result in an attractive and complementary addition to Prince Edward Road. The proposed carport is an open structure, minimal in scale and dwarfed by the existing dwelling. The proposed addition at the front of the subject site will allow for an attractive street front presentation.

Garbage Areas

An existing bin storage area is provided on site and no change is proposed.

Front Fences and Gates

The proposal includes a new front fence which will be a 1.2 metres high masonry wall and hard wood batten fence. A timber front entry gate is also proposed. The proposed fencing and wall materials are compatible with the overall landscape character of Prince Edward Road and the appearance of the existing dwelling and the streetscape.

See Figure 11 below.

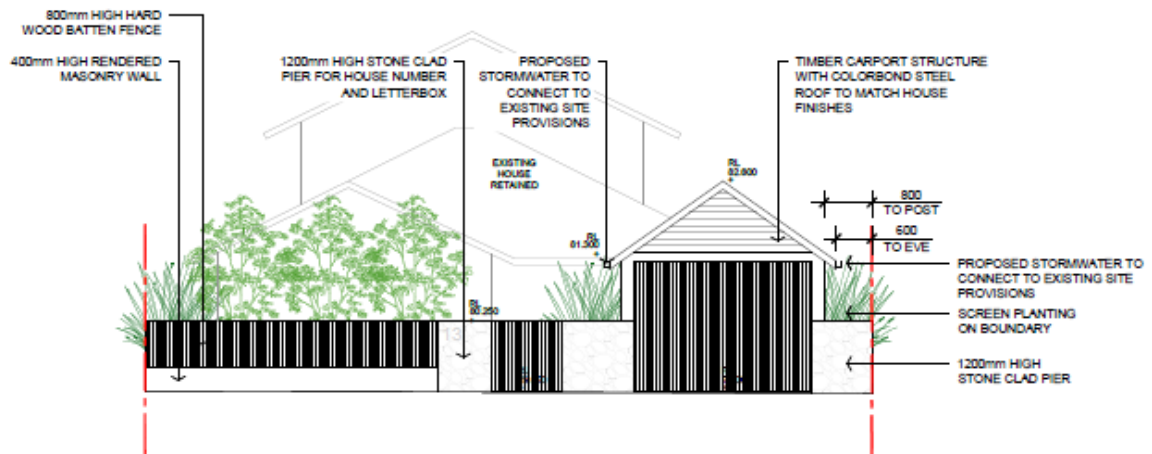


Figure 11. Extract from DA Plans – Elevation 02 - West

Garages, Carports and Hardstand Areas

A carport is proposed and a new driveway. The proposed carport will be residential timber framed with a Colorbond steel roof to match the existing dwelling.

The carport has been designed to ensure the scale and bulk is compatible with the existing dwelling and streetscape of the residential area. The new carport is similar to several neighbouring examples of carports and garages with a nil setback to Prince Edward Road. A new concrete cross over and driveway is proposed to Council specification, including a compliant gradient.

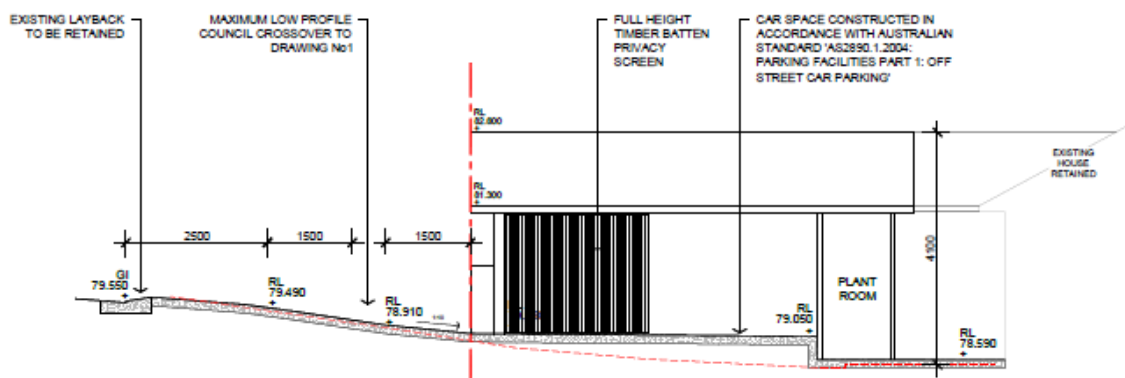


Figure 12. Extract from Plans (LPL_2730) showing the proposed crossover and driveway

The proposed development will allow for a more accessible, compliant and weatherproof parking at the front of the lot. No street parking is lost as a result of the proposed development.

3.2 Heritage Considerations

The subject site is not a heritage item, located in a heritage conservation area and is not located in proximity to a heritage item.

3.3 Landscaping

New landscaping on the site will enhance the useability and enjoyment for the residents. Tree removal and replacement is proposed to allow for the new carport, refer to the attached Arborist Report, prepared by Urban Arbor and Landscape Plan (including a planting schedule), prepared by Lone Pine Landscapes.

3.4 Amenity (Views, Overshadowing, Overlooking/Privacy, Noise)

Sunlight Access and Overshadowing

The principal private open space of neighbours will retain compliant solar access. Appropriate levels of solar access will be maintained for the property and its neighbours.

Overshadowing adjoining open space.

The proposed development will have no impact on the private open space of neighbouring properties.

Maintaining Solar Access into Living Rooms of Adjacent Properties.

The proposed development will have no impact on the living areas of neighbouring properties.

Overshadowing Clothes Drying Areas

Drying areas will be unaffected by the proposed development.

Excessive Glare or Reflectivity Nuisance

Appropriate materials are proposed to ensure there is not excessive glare.

3.4.2 Privacy and Security

Privacy will be retained for neighbours at an appropriate level for an urban environment. The existing and proposed landscaping, aids in providing a high level of privacy for the residents and neighbours. The carport includes a full height timber batten privacy screen on the southern boundary.

Acoustical Privacy (Noise Nuisance)

The development is appropriate and will not result in noise levels inappropriate to a residential area. The site is not located in close proximity to a noise generating activity.

3.4.3 Maintenance of Views

A site visit has been undertaken and it is considered there will be no view loss impacts as a result of the proposal.

3.7 Stormwater Management

Stormwater has been designed to comply with Council requirements. See accompanying Stormwater Plan prepared by Lone Pine Landscapes.

3.8 Waste Management

Appropriate waste management will be undertaken during the demolition and construction process. All demolished materials will be recycled where possible which is detailed in the accompanying Waste Management Plan.

The development retains an existing bin storage area, with waste to be collected by Councils regular service.

3.10 Safety and Security

The proposal allows for retained and improved public surveillance with the improvement of car parking and retaining visible access to the dwelling.

Part 4 - Development Controls and Development Types

4.1 Residential Development Controls

4.1.1 Dwelling Density, Dwelling Size and Subdivision

No change is proposed to the existing density which comprises of a single principal residential dwelling.

4.1.2 Height of Buildings

4.1.2.1 Wall Height

There is no change to the existing compliant wall height of the dwelling.

4.1.2.2 Number of Storeys

A maximum of two storeys is permitted. No change is proposed.

4.1.2.3 Roof Height

The proposed development does not propose to change the existing roof height of the dwelling. The development proposes a pitched roof to the carport to match the roof of the dwelling.

4.1.3 Floor Space Ratio (FSR)

A maximum floor space ratio of 0.45:1 is permitted for the site and no change is proposed.

4.1.4 Setback (front, side and rear) and Building Separation

4.1.4.1 Street Front setbacks

A front setback consistent with the prevailing, or a minimum 6 metres, is required on the site.

The proposed carport has a nil front setback. This is considered appropriate as it responds to the characteristics of the land and is the only feasible location for on-site parking. The design of the carport ensures an appropriate presentation to the street, with similar carports having a nil front setback in close proximity to the subject site.

The proposal is consistent with the objectives of Clause 4.1.4 as outlined below.

Objectives of the control:

1. *To maintain and enhance the existing streetscape including the desired spatial proportions of the street, the street edge and the landscape character of the street.*

Comment

A variation to the front setback control allows for a better planning outcome to be achieved, through the provision of a new pedestrian entry, weatherproof and secure carparking on the site. Additionally, the new single carport will be constructed with a roof pitch to match the pitch of the existing dwelling.

The design remains complementary to the existing dwelling, the streetscape of Prince Edward Road and is consistent with the desired character for the location. It is noted that other existing examples of carports are present in the locality. See Figures 14 to 17 below.

2. *To ensure and enhance local amenity by:*
 - *providing privacy;*
 - *providing equitable access to light, sunshine and air movement; and*

- *facilitating view sharing and maintaining adequate space between buildings to limit impacts on views and vistas from private and public spaces.*
- *defining and adding character to the streetscape including the provision of adequate space between buildings to create a rhythm or pattern of spaces; and*
- *facilitating safe and adequate traffic conditions including levels of visibility around corner lots at the street intersection.*

Comment

As described above, the design and location of the carport ensures privacy and solar access is maintained for both the subject site and the neighbouring residential development to the south. Privacy screening is provided along the southern side of the carport to minimise acoustic and privacy impacts to the adjoining neighbour. There will be no view impacts and the proposal remains consistent with the residential density in the locality. The wide nature strip at the front of the subject site will also soften the visual impact of the carport when viewed from Prince Edward Road.

The variation will allow for the provision of weatherproof car parking on the site. The new crossover and driveway will be constructed in accordance with Council requirements and Australian Standards.

3. To promote flexibility in the siting of buildings.

Comment

Flexibility in the application of the front setback control is considered appropriate, in this case, as the development proposes weatherproof onsite carparking for this family sized home.

The development allows for the modernisation of the residential use of the site and the front setback variation maintains the amenity of adjoining neighbours.

4. To enhance and maintain natural features by:

- *accommodating planting, including deep soil zones, vegetation consolidated across sites, native vegetation and native trees;*
- *ensuring the nature of development does not unduly detract from the context of the site and particularly in relation to the nature of any adjoining Open Space lands and National Parks; and*
- *ensuring the provisions of State Environmental Planning Policy No 19 - Urban Bushland are satisfied.*

Comment

The proposed front setback variation will have a minor impact on the landscaped area in this location as it is being constructed over an existing area of paving within the front

setback. Overall, the site provides compliant open space and landscaped area as demonstrated on the attached Landscaped Area Plan.

The development involves tree removal, replacement and enhancement to ensure that the landscaped setting is retained. Refer to the attached Arborist Report for further details.

5. *To assist in appropriate bush fire asset protection zones.*

Comment

Not relevant. The subject site is not bushfire prone land.

It is concluded that the proposed variation will have a minimal impact on neighbouring properties and the streetscape, thereby remaining consistent with the objectives of this clause, despite the non-compliance.



Figure 14. An existing carport located in the front setback at No. 33 Prince Edward Road



Figure 15. An existing double carport located in the front setback at No. 7 Prince Edward Road



Figure 16. An existing carport located in the front setback at No. 3 Prince Edward Road



Figure 17. An existing double garage located in the front setback of No. 19 Prince Edward Road.

4.1.4.2 Side setbacks and secondary street frontages

A side boundary setback equivalent of $\frac{1}{3}$ of the wall height is required on the site and there are no changes to the existing side setbacks to the dwelling.

The development proposes compliant side setbacks.

The maximum wall height on the southern side of the carport is 2.2 metres (post height), which equates to a side setback of 733mm to the carport posts. The proposed carport proposes a compliant 800mm setback to the southern boundary. The plant room wall height equates to 2.6m, requiring 866mm, and provides a compliant 1 metre. A side setback of 8.2 metres to the carport is provided to the northern boundary.

4.1.4.4 Rear Setbacks

A minimum rear setback of 8 metres is required on the site and no change is proposed.

4.1.5 Open Space and Landscaping

4.1.5.1 Minimum Residential Total Open Space Requirements

The DCP requires a total of 55% of the site to be open space with a minimum 35% of that open space to be landscaped area. This equates to a minimum 285.18m² of total open space for the site area of 518.5m² and a minimum of 99.81m² of landscaped area.

The development proposes a compliant total open space area of 59.18% (306.87m²) and landscaped area of 62.3% (177.68m²).

The DCP requires a minimum 18m² of principal private open space and an easily compliant area is provided in the rear yard.

4.1.6 Parking, Vehicular Access and Loading (including bicycle facilities)

The DCP requires 2 car parking spaces for dwellings. The proposal will provide 1 covered space under the new carport. This is considered sufficient to meet the needs of the residents, with on-street parking also available.

The carport is provided over an existing hardstand space, requiring minimal earthworks. The design of the carport is compatible with the existing dwelling on the site and retains the existing crossover from Prince Edward Road. New landscaping within the front setback will also soften the impact of the carport when viewed from the street.

4.1.8 Development on Sloping Sites

The proposal is designed to ensure a high standard of amenity from public and private spaces.

The subject site is not identified as Landslip Risk in the LEP map.

4.1.10 Fencing

The proposal includes a new front fence which will be masonry wall and timber slates to at a maximum compliant height of 1.2 metres, as detailed above. The low line fence is detailed on the plans. The proposed fence is consistent with the existing dwelling and proposed carport.

6. Numerical Control Table

The following table provides a summary of the development proposal, in accordance with the relevant numerical planning controls contained in the Manly LEP 2013 and the Manly DCP 2013.

| | Standard | Proposed | Compliance |
|---------------------------------------|--|--|--|
| Manly LEP 2013 | | | |
| Lot Size | 275m ² | 518.5m ² | Yes – No change |
| Building Height | 8.5 metres | Carport 4.1m | Yes |
| Floor Space Ratio | 0.45:1 (234m ²) | No change to existing FSR | Yes – no change |
| Manly DCP | | | |
| Residential Density/ Dwelling Size | Density Area D5 1/500m ² | Existing 1 dwelling | Yes – no change |
| Wall Height | Dependant on slope / site gradient. Maximum height of 6.5m on flat land, and 8m where gradient is greater than 1:4. | 2.2m (carport) – 2.6m (plant room) | Yes |
| Number of Stories | 2 | No change to existing | N/A |
| Roof Height and pitch | Pitched roof structure must be no higher than 2.5m above the actual wall height. The maximum roof pitch must be generally no steeper than 35 degrees. | 1.335m pitch height (carport) and compliant slope | Yes |
| Side Boundary Setbacks | 1/3 wall height Wall height to carport, which equates to 2.2m – 2.6m which equates to a required side setback (south) of 733mm - 866mm | <u>North</u> : 8.2m <u>South</u> : 800mm – 1m | Yes |
| Primary Front Boundary Setback | Prevailing building line or 6m | Nil front setback | Compliance with objectives achieved |
| Rear Boundary Setbacks | 8 metres | No works proposed at the rear | N/A |
| Parking | 2 spaces The maximum width of any garage, carport or hardstand area is not to exceed a width equal to 50 percent of the frontage, up to a | New open sided carport above existing 1 hardstand space 3m width opening to carport | Yes Yes |

| | | | |
|-----------------------------------|---|--|---------------------------------------|
| | maximum width of 6.2m. | Site frontage 12.23m (50% = 6.115m) | |
| Open Space O/S Area 3 | Total O/S – 55% of site area = 285.18m ² Landscaped Area 35% of O/S = 99.81m ² Above Ground - Maximum 25% of O/S = 71.3m ² | 59.18% or 306.87m ² 62.3% or 177.68m ² No change to existing | Yes Yes Yes – No change |
| Solar Access (adjoining P o/s) | 2/3 sunlight retained between 9am and 3pm on June 21. | Will comply | Yes |
| Solar Access (Living rooms) | Change accordingly to orientation from DCP east-west orientation, the level of solar access presently enjoyed must be maintained to windows or glazed doors to living rooms for a period of at least 2 hours from 9am to 3pm on the winter solstice (21 June) | Will comply | Yes |

7. Section 4.15 Considerations

The following matters are to be taken into consideration when assessing an application pursuant to section 4.15 of the Environmental Planning and Assessment Act 1979 (as amended). Guidelines to help identify the issues to be considered have been prepared by the former Department of Urban Affairs and Planning. The relevant issues are:

7.1 **The provision of any planning instrument, draft environmental planning instrument, development control plan or regulations**

This report clearly and comprehensively addresses the statutory regime applicable to the application and demonstrates that the proposed land use is complimentary and compatible with adjoining development. The proposal achieves the aims of the Manly LEP and DCP.

The development is permissible in the zone.

7.2 **The likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality**

Context and Setting

What is the relationship to the region and local context in terms of:

- *the scenic qualities and features of the landscape?*
- *the character and amenity of the locality and streetscape?*
- *the scale, bulk, height, mass, form, character, density and design of development in the locality?*
- *the previous and existing land uses and activities in the locality?*

These matters have been discussed in detail in the body of the statement.

What are the potential impacts on adjacent properties in terms of:

- *relationship and compatibility of adjacent land uses?*
- *sunlight access (overshadowing)?*
- *visual and acoustic privacy?*
- *views and vistas?*
- *edge conditions such as boundary treatments and fencing?*

The proposed development, including the new carport, have been designed to complement the site and its surrounds. The alterations are appropriate and will have reasonable and appropriate impacts for adjacent properties.

Access, transport and traffic

Would the development provide accessibility and transport management measures for vehicles, pedestrians, bicycles and the disabled within the development and locality, and what impacts would occur on:

- *travel demand?*
- *dependency on motor vehicles?*
- *traffic generation and the capacity of the local and arterial road network?*
- *public transport availability and use (including freight rail where relevant)?*
- *conflicts within and between transport modes?*
- *traffic management schemes?*
- *vehicular parking spaces?*

The proposed development allow for weatherproof and compliant on-site parking for residents.

Public domain

The proposed development will have a positive impact on the public domain as the proposal is consistent with the character of the local streetscape.

Utilities

There will be no impact on the site, which is already serviced.

Flora and fauna

There will be no significant impact on flora and fauna. See the attached Arborist Report, prepared by Urban Arbor and Landscape Plan.

Waste

There will be no impact.

Natural hazards

The site is not affected by any natural hazards.

Economic impact in the locality

There will be no impact, other than the possibility of a small amount of employment during construction.

Site design and internal design

Is the development design sensitive to environmental conditions and site attributes including:

- *size, shape and design of allotments?*
- *the proportion of site covered by buildings?*
- *the position of buildings?*
- *the size (bulk, height, mass), form, appearance and design of buildings?*
- *the amount, location, design, use and management of private and communal open space?*
- *landscaping?*

The proposed development is highly appropriate to the site, with regards to all of the above factors. The development fits well within the context of the surrounds and the buildings on the site will remain of minimal scale and well suited to its residential surrounds.

How would the development affect the health and safety of the occupants in terms of:

- *lighting, ventilation and insulation?*
- *building fire risk – prevention and suppression/*
- *building materials and finishes?*
- *a common wall structure and design?*
- *access and facilities for the disabled?*
- *likely compliance with the Building Code of Australia?*

The proposed development will comply with the provisions of the Building Code of Australia. Additionally, finishes, building materials and all facilities will be compliant with all relevant Council controls.

Construction

What would be the impacts of construction activities in terms of:

- *the environmental planning issues listed above?*
- *site safety?*

Site safety measures and procedures compliant with relevant legislation will ensure that no site safety or environmental impacts will arise during construction.

7.3 The suitability of the site for the development

Does the proposal fit in the locality?

- *are the constraints posed by adjacent developments prohibitive?*

- *would development lead to unmanageable transport demands and are there adequate transport facilities in the area?*
- *are utilities and services available to the site adequate for the development?*

The development does not impose any unusual development constraints.

Are the site attributes conducive to development?

The site is appropriate for the additions proposed.

7.4 Any submissions received in accordance with this Act or the regulations

It is envisaged that the consent authority will consider any submissions made in relation to the proposed development.

7.5 The public interest

It is considered that the proposal is in the public interest as it allows for appropriate and positive additions to an existing residential site.

Section 4.15(1) of the Environmental Planning and Assessment Act has been considered and the development is considered to fully comply with all relevant elements of this section of the Environmental Planning and Assessment Act 1979.

8. Conclusions

- 8.1 The proposed development, including a single carport, and associated landscaping works at 13 Prince Edward Road, Seaforth is appropriate considering all State and Council controls.
- 8.2 When assessed under the relevant heads of consideration of s4.15 of the Environmental Planning and Assessment Act, the proposed development is meritorious and should be granted consent.
- 8.3 Considering all the issues, the development is considered worthy of Council's consent.

Planner Declaration

This report was prepared by:

Senior Planner: Susan May-Roberts

Report Version: Final

Document Control Table

| Document Purpose: | Statement of Environmental Effects | |
|-------------------|-------------------------------------|----------------------------|
| Date | Prepared by | Approved by |
| 18/12/2024 | Susan May-Roberts Senior Planner | Sarah McNeilly Director |

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