



David Scobie Architects



Photo 1 Elevation to Pittwater Road of the subject property

HERITAGE IMPACT STATEMENT

166 Pittwater Rd, Manly NSW

Clients: B. & C. Laws

Design: Jim O'Brien of View Thru

David Scobie Architects Pty Ltd

July 2022

Issue C: 6.12. 2022

1 Introduction

1.1 Brief

Jim O'Brien of View Thru commissioned the Heritage Impact Statement for 166 Pittwater Rd, Manly in October 2017 on behalf of the Clients Mr. & Mrs. Laws. The purpose is to assist in the design process for the proposed alterations and additions to the property and to prepare a Heritage Impact Statement suitable for submission to Northern Beaches Council. The initial assessment provided recommendations which have largely been adopted in a revised scheme. This report documents the three reviews and provides a Conclusion in support of the revised Proposal.

1.2 Documentation

The report provides a Heritage Impact Statement for the property based on its location within the C1 Pittwater Road Conservation Area as identified by the Manly LEP 2013.

1.3 References

The report is based on the following drawings issued **10.11.2022**, which are a revision of the original drawings following a review of the Heritage Impact matters:

DA00	Cover page: Site/Location & drawing register
DA01	Perspective views/Existing property
DA02	Area & Compliance table
DA03	Area calculation plans
DA04	Site Analysis Plans
DA05	Site & entry existing plan
DA06	Site & entry plan proposed
DA07	First Floor Plan proposed
DA08	Roof plan proposed
DA09	Section proposed
DA10	Elevations North existing & proposed
DA11	Elevations South existing & proposed
DA12	Elevations East existing & proposed
DA13	Elevations West existing & proposed
DA14	Door & window schedule 1
DA15	Door & window schedule 2
DA16	Finishes schedule
DA17	Shadow diagrams June 21 9am
DA18	Shadow diagrams June 21 12 noon
DA19	Shadow diagrams June 21 3pm
DA20	Shadow diagrams Elevations
DA21	Sediment & erosion control plan
DA22	Sediment & erosion control details 1
DA23	Sediment & erosion control details 2
DA24	Sediment & erosion control details 3

1.4 Heritage Listings

The property is listed as a heritage item on the following statutory lists:

- The property is not listed as a heritage item
- The site is located within the Pittwater Road Heritage Conservation Area

1.5 Methodology

Procedures and practices are as recommended in the document '*Statements of Heritage Impact*' from the NSW Heritage Manual, Heritage Office and Department of Urban Affairs and Planning, 1996.

The report follows the methodology illustrated in '*The Conservation Plan*', J.S.Kerr, Sydney, National Trust of Australia (NSW), 1996. Definitions and procedures are as presented in the Australia ICOMOS Guidelines to the Burra Charter-Cultural Significance and Conservation Policy.

1.6 Definitions

The definitions used in the report are those presented in Article 1 of The Australia ICOMOS Charter for the Conservation of Places of Cultural Significance (The Burra Charter).

"**Place** means site, area, building or other work, group of buildings or other works together with associated contents and surrounds.

Cultural significance means aesthetic, historic, scientific or social value for past, present or future generations.

Fabric means all the physical material of the place.

Conservation means all the processes of looking after a place so as to retain its cultural significance. It includes maintenance and may according to circumstance include preservation, restoration, reconstruction and adaptation and will be commonly a combination of more than one of these.

Maintenance means the continuous protective care of the fabric, contents and setting of a place, and is to be distinguished from repair. Repair involves restoration or reconstruction and it should be treated accordingly.

Preservation means maintaining the fabric of a place in its existing state and retarding deterioration.

Restoration means returning the fabric of a place to a known earlier state and is distinguished by the introduction of materials (new or old) into the fabric. This is not to be confused with either recreation or conjectural reconstruction, which are outside the scope of the Charter.

Adaptation means modifying a place to suit proposed compatible uses.

Compatible uses mean a use which involves no change to the culturally significant fabric, changes which are substantially reversible, or changes which require a minimal impact."

1.7 Author

This report has been prepared by David Scobie, B.Arch. (Hons.) Dip. U&RP

2 Statutory Instruments

2.1 Manly LEP 2013

Clause 5.10.1 provides the Heritage Conservation Objectives, including:

- (a) *to conserve the environmental heritage of the Manly Local Government area*
- (b) *to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views,*

Clause 5.10.2 provides for Requirement for Development Consent for any of the following:

(a) *demolishing or moving any of the following or altering the exterior of any of the following (including, in the case of a building, making changes to its detail, fabric, finish or appearance):*

(iii) *a building, work, relic or tree within a heritage conservation area,*

(b) *altering a heritage item that is a building by making structural changes to its interior or by making changes to anything inside the item that is specified in Schedule 5 in relation to the item,*

(e) *erecting a building on land:*

(i) *on which a heritage item is located or that is within a heritage conservation area*

Clause 5.10.4 provides for Effect of Proposed Development on Heritage Significance:

The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned.

Clause 5.10.5 provides for the requirement of a Heritage Assessment:

The consent authority may, before granting consent to any development:

(a) *on land on which a heritage item is located, or*

(b) *on land that is within a heritage conservation area, or*

(c) *on land that is within the vicinity of land referred to in paragraph (a) or (b),*

require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.

2.2 Manly DCP 2013

The Development Control Plan includes general issues such as residential development and cultural resources and is based on localised planning matters.

Clause 3.2 deals with Heritage Considerations:

This section applies to:

- *Heritage Items and Conservation Areas listed in the LEP;*
- *development in the vicinity of heritage; and*
- *other development which may have potential heritage significance. If the property has merit as a potential heritage item the heritage controls and considerations of this plan will apply.*

Relevant DCP objectives in relation to heritage in this plan include the following:

Objective 1) To retain and conserve environmental heritage and cultural significance of Manly including:

- *significant fabric, setting, relics and view associated with heritage items and conservation areas;*
- *the foreshore, including its setting and associated views; and*
- *potential archaeological sites, places of Aboriginal significance and places of natural significance.*

Objective 2) To ensure any modification to heritage items, potential heritage items or buildings within conservation areas is of an appropriate design that does not adversely impact on the significance of the item or the locality.

Objective 3) To ensure that development in the vicinity of heritage items, potential heritage item and/ or conservation areas, is of an appropriate form and design so as not to detract from the significance of those items.

Objective 4) To provide infrastructure that is visually compatible with surrounding character and locality/visual context with particular regard to heritage buildings/areas and cultural icons.

Objective 5) To integrate heritage management and conservation into the planning development process including incentives for good heritage management, adaptive reuse, sustainability and innovative approaches to heritage conservation.

Conservation criteria are provided which define and particularise aspects of significant elements, including scale, siting, architectural form, architectural detailing, materials and finishes, use, and original fabric.

3 HISTORICAL CONTEXT

3.1 Pre-European History

The Gamaragal clan occupied the north shore of Port Jackson, from Karabilye (Kirribilli) opposite Warrane (Sydney Cove) to the cliffs of Garungal or Carangle (North Head) and the sandy bay of Kayyeemy (Manly Cove), reputedly the most densely populated part of Port Jackson (Sydney Harbour) at the time.¹

3.2 Early European History of Manly

Manly Cove was one of the earliest sites of contact between the Aboriginal people and European settlers. The area was low lying and scrubby and at times of high water the North Head was almost completely cut off from the mainland by water. Due to the isolated geography of the area and to poor access, Manly remained reasonably undeveloped, with only a few isolated huts. Early development of the area was a direct result of access by ferry boat leading to the establishment of some of the first villa homes in the area.

There were scant development in the area before 1840. In 1810, two grants for Crown land were made. Gilbert Baker was granted 30 acres of land south of the present day Corso and extended to the current Ashburner Street. Richard Cheers was granted 100 acres of land to the south adjoining Baker's grant, from Ashburner Street to the Patrick's College land. Most of the North Head had been reserved for defence purposes and the Quarantine Station. D'Arcy Wentworth purchased both the Baker and Cheers grants prior to his death in the 1820s but the area remained relatively undeveloped up until the 1880s. In 1836, there were only 43 people living in the Manly district, 13 of them "government men".

To the north of the area now known as The Corso, John Thompson was granted 100 acres in 1842. Thompson's grant was purchased in the early 1850s by an enterprising gentleman named Henry Gilbert Smith. Smith made several purchases in the area including 20 acres at North Steyne. He then set about planning his grand scheme for the private village that was to become Manly. Smith initially called the area Ellensville, then Brighton after the similarly named seaside resort in England.

Smith had visions of a great Marine Retreat and health resort, inspired by the concerns of a health-conscious community that still laid blame for ill health on miasma rather than direct contact with bacteria. In 1855 Smith prepared several plans for his marine retreat. One was for Ellensville, encompassing the area from The Corso, north to the current Pine Street. The southern portion from The Corso to Ashburner Street was named Montpelier. Every aspect was well considered, down to the plantings and manipulated views. Smith stated:

The object has been to give such a character to these marine Retreats that they may become the favourite resorts of the colonists. The promenades and Squares indicated on the plan will be the means of ensuring the health and amusement to residents and visitors and that the spaces thus reserved to recreation may be laid out and embellished, the proprietor undertakes to devote for this purpose one-sixth of the proceeds of all sales by him during the ensuing ten years.

In 1855, Smith built a pier in Manly Cove and began a ferry service between Sydney and Manly. Initially difficult to access by land, transport from Sydney by ferry was vital to the future development of Manly. Smith then set about developing one of the few private townships in New South Wales, one of the first suburbs to develop outside the walking boundary of Sydney.²

¹ <http://home.dictionarofsydney.org/gamaragal-aboriginal-people-of-manly-and-northern-sydney/> accessed 13/11/17

² <http://www.environment.nsw.gov.au/heritageapp/ViewHeritageltemDetails.aspx?ID=2020838> accessed 13/11/17

3.3 History of the Pittwater Road Conservation Area

“The First Road”

The creation of suburban streets and allotments in the Conservation Area begins with the alignment of Pittwater Road, which appears to predate European settlement. It corresponds with an early, probably Aboriginal track leading from Manly Cove to the Head of Curl Curl Lagoon and shown on an 1842 parish map of Manly Cove. The distinctive bend in the road after Collingwood Street is explained topographically as a sweep around the embankment of the lagoon which once existed directly to the north.

The Victorian Design of Streets

The other main streets originate from the design by the developer Henry Gilbert Smith for his development called Brighton, later known as Manly. This established East Steyne, Carlton Street, Belgrave Street and the alignment of Francis Street and Pine Street.

Two large subdivisions of the Brighton Estate occurred in the mid-Victorian period and contain all of the area in the Conservation Area. These two subdivisions bear the character of Victorian urban design. Rectilinear, wide streets are arranged in a grid with allotments which are rectilinear and large, of approximately 1000sq.m each. There are back lanes to serve the allotments and the layout is arranged so as to make regular the allotment areas.

The streets created run parallel variously to the coast and to Pittwater Road, thereby creating a series of out-of-square corners and junctions and small connecting streets. This street pattern is distinctive and underpins the urban character of the area. The streets remain unaltered in their alignment, although the names of Malvern, Pine and North Steyne are now names for what were Whistler, Middle Harbour and East Steyne respectively.

The Second Design of Subdivision

The majority of the allotments underwent a re-subdivision in the Edwardian period and later. These closer grained re-subdivisions created smaller allotments for smaller houses. In some cases the end allotments are turned to address the short street. In the Sunlight Estate and an estate near Carlton Street, large subdivisions of original allotments were made.

The major interference with this street pattern is the radical re-alignment of the corner of Pittwater Road and Raglan Street which was made after 1965. Also, although large villas were built on several amalgamated allotments, none of these have survived.

It is possible to see in the place today the result of the close scaled Edwardian re-subdivision of the larger scale Victorian street pattern with lanes and long blocks. Small houses and narrow shops address larger scale streets, broad enough for tree, and good footpaths, with prominent intersections where the streets meet out of square. Blocks are deep with rear lanes. All streets have a common flat topography giving a clear perceivable character.

The Village in 1893

By 1893 the northern part of Manly had been subdivided and built up as a substantial Victorian village, with some 100 houses arranged on both sides of Pittwater Road, from Raglan to Carlton Streets, and on the east side of Pittwater Road, from Carlton Street to the lagoon. With two exceptions, the development is of houses on single allotments from Victorian subdivisions. Larger estate-type houses existed in the vicinity: The Lawn and Undercliffe. The completeness of the village can be seen in early photographs from this time which illustrate not only houses, but developed gardens, fences, kerb and guttering and street trees in the gutter.

It has been possible to locate those buildings which survive from 1893 or before. About 32 buildings survive of the hundred or so which existed in the Victorian period. The greatest density and those most intact are on Pittwater Road, between Denison and Steinton Streets.

Early 20th Century Development

Almost all buildings in the conservation area were extant at 1933. These buildings include small terraces of houses, single detached houses, small apartment buildings and shops.



Photo 2 1943 Aerial photograph, courtesy SIX Maps – subject site identified

In historic terms therefore the urban fabric around Pittwater Road is, in the main, a result of an historical development over a relatively short time: about 40 years leading up to 1933. Considering that it includes domestic and commercial development it is a place of relatively unusual historic cohesion.

Later 20th Century Development

This pattern of development came into being without any particular control over the relative placement of different types of land uses. This has led to the current diversity of residential and non-residential land uses found along Pittwater Road. In part, Pittwater Road took on some service area functions for the main part of Manly and many of these activities still exist in residential zones. Reflecting this, town planning controls introduced in the 1950's established a light industrial zone on the western side of Pittwater Road south of Carlton Street. This was later changed to a general business zoning, with residential zonings elsewhere. Importantly, a road widening proposal for the east side of Pittwater Road was not removed until the 1980s.³

4 Property Description

4.1 The Context



Photo 3: Aerial photo, courtesy SIX Maps, illustrates the location and context of 166 Pittwater Road



Photo 4: The subject property is located within the C1 Pittwater Road Conservation Area and is not a Heritage item. The subject property is indicated and outlined in RED. Map courtesy: Manly LEP 2013

The subject property is a single-storey brick semi-detached brick & terra-cotta tile roofed house from the Federation period c.1900 with a timber posted verandah in the Federation style. It is located at 166 Pittwater Road, Manly.

The streetscape consists of a generally consistent series of detached and attached single storey houses on the eastern side of the road located between Pacific Lane to the north and Pine Street to the south. This row includes 14 similar buildings with traditional intact frontages to Pittwater Road and extensions to the rear of each property from various periods. The area itself is characterised by a mixture of residential and commercial premises.

Of particular interest is the listed item I208 located diagonally opposite the subject site at 167 Pittwater Road. The building has remnant inter war period elements but the character and appearance has generally been subsumed by paint colours and loss of detail.



Photo 5 The listed item I208 located at 167 Pittwater Road

The building of more substantial character having retained a considerable level of detail is 169 Pittwater Road however this is not a listed item despite its contribution.



Photo 6 The two-storey premises at 169 Pittwater Road

The adjoining property to the north is 168 Pittwater Road, is the matching part of the semi-detached building.



Photo 7 The pair of semi-detached houses, with the subject building to the right (south side)

The neighbour to the south at 190 Pittwater Road, is a similarly styled Federation house.

To the north at 170 Pittwater Road is a detached timber framed house clad in weatherboard.



Photo 8: The weatherboard house at 170 Pittwater Road.

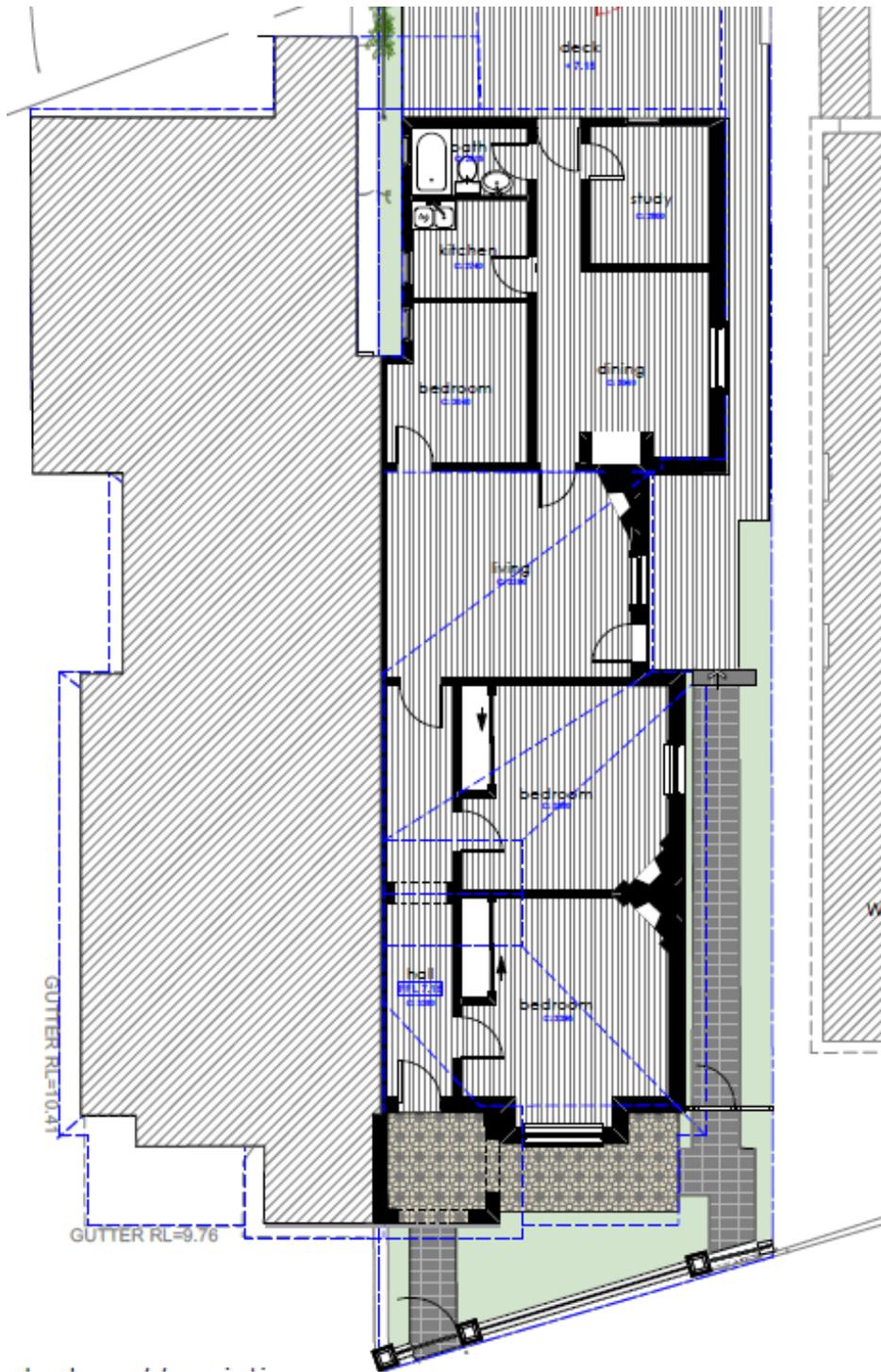


Photo 9 A detail view of the front Elevation noting the fence and original and intact distinctive features to the frontage and verandah

The front elevation of the subject site sits behind a 1.4m cast iron fence with matching gate set within a sandstone piers and low wall.

Walls	Common brown brick with stained pointing, remnant tuck pointing and arches
Verandah	Extended roof line to porch with Marseille tiles & timber valence with spindles
Fenestration	Timber three sash casements with coloured glass toplights & rendered shaped sills
Verandah	Post & details appear to be reinstated without turning or chamfers
Roof	Marseille pattern terra cotta with ridge caps
Gable	Roughcast render with bottom mould and corbel brackets
Chimneys	Cement rendered bands with rendered cap and two terra cotta pots

The style elements include a traditionally paired semi-detached floor plan, symmetry to the frontage despite minor changes to the fence and verandah, terra cotta hipped roof, prominent street facing porch entrance with return verandah, tall chimneys with pots, roughcast render to the gable infill on the porch and timber casement windows. Generally these elements relate to the Federation Queen Anne style c. 1890 – c.1915



Drawing: The existing floor plan shows the layout to be generally intact as an original set of three main rooms and modified rear service wing of five rooms. Note the variation in this drawing from the site photos which indicate a storage unit to the side of the property.

4.2 The Residence



Photo 10: View from the side of the front verandah towards the setback, garden and fence

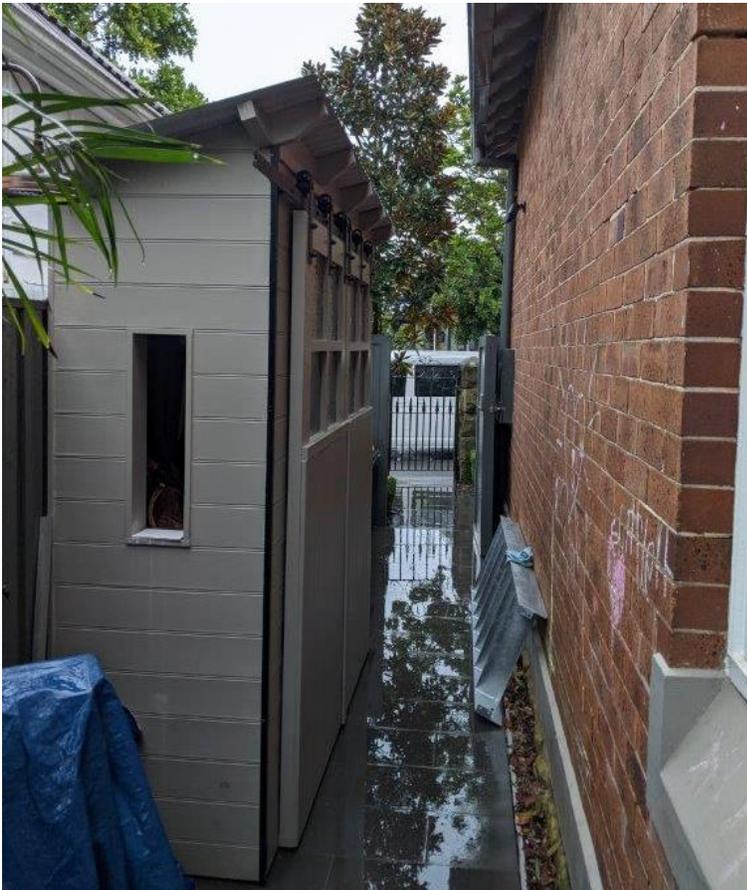


Photo 11: View from the side of the house looking towards the front with the storage element located behind the side gate. The unit is not shown on the existing and proposed plans DA//05 & 06.



Photo 12: View from the side of the house at the recessed space next to the living room and its fireplace with chimney, looking towards the protruding dining room.

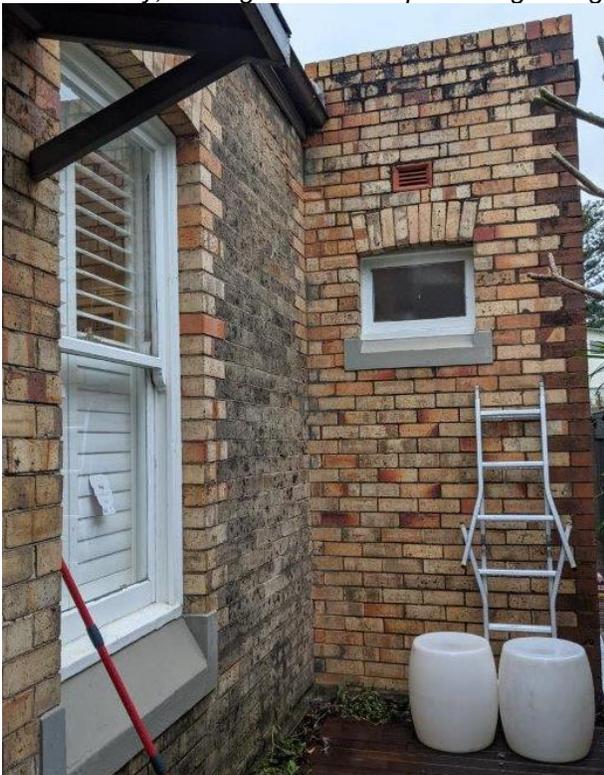


Photo 13: View of the extended portion comprising the Dining room



Photo 14: View from the rear next to the Dining room and door from the living room.



Photo 15: View of the rear elevation with pergola covered deck and barbecue and remnant northern brick wall.



Photo 16: View of the rear elevation looking north west at the end of house/deck with remnant original brick wall and the barbecue. These elements are yet to be shown on the existing plan.



Photo 17: View of the rear yard looking east to the lawn and swimming pool, noting the extensions to properties located fronting Malvern Avenue



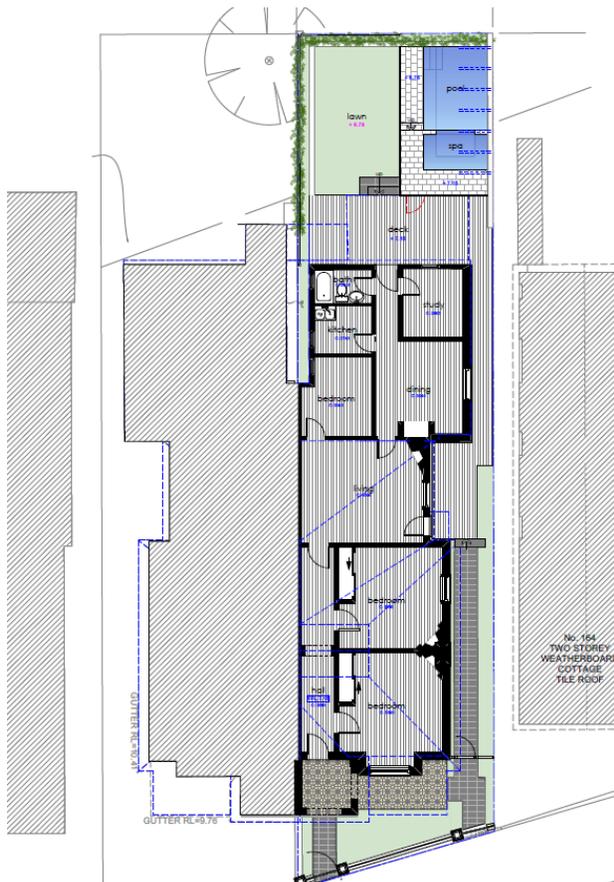
Photo 18: View of the rear yard looking south west towards the two-level extension on the adjoining property at 164 Pittwater Road. The steel structure appears to be an unfinished element designed to provide privacy to the yard and pool on the subject property.



Photo 19: View of the southern side boundary with wall and structure next to the swimming pool.



Photo 20: View looking south from the rear deck to the adjoining property at 164 Pittwater Road



Drawing DA//05 Plan of the Existing entry level/Ground floor.

ASSESSMENT OF SIGNIFICANCE

4.3 SIGNIFICANCE FRAMEWORK

The *NSW Heritage Manual* guideline, 'Assessing Heritage Significance' (NSW Heritage Office 2001) provides the framework for the following significance assessment and Statement of Significance. These guidelines incorporate the seven aspects of cultural heritage value identified in the *Australia ICOMOS Charter for Places of Cultural Significance, The Burra Charter, 2013* (Burra Charter) into a framework currently accepted by the NSW Heritage Council.

4.4 SIGNIFICANCE OF ASSESSMENT

The significance of 166 Pittwater Road has been assessed in terms of its location within the Pittwater Road Conservation Area as identified in the Manly LEP 2013. The Significance Assessment is reproduced in Table 1.

Table 1: Significance Assessment of 192 Pittwater Road, Manly

Significance Criterion	Assessment
(a) An item is important in the course, or pattern, of the local area's cultural or natural history [Historical Significance]	Local: The residence identifies the early settlement – early twentieth century period and Federation style, through the erection of a traditional house type - the semi-detached residence, during the Federation period.
(c) An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in the local area [Aesthetic Significance]	Local: The brick and terra-cotta tile roofed house in the traditional symmetrical Federation style has retained the distinctive original elements including the external face brick walls, casement fenestration, verandah, roof form, chimneys and it complements the streetscape and setting in the HCA.
(d) An item has a strong or special association with a particular community or cultural group in the area for social, cultural or spiritual reasons. [Social Significance]	Associative Local: The house appears to have built by the same builder as one of a pair.
(g) An item is important in demonstrating the principal characteristics of a class of the area's: - Cultural or natural places; or - Cultural or natural environments [Representativeness]	Local: The item is assessed as historically representative of the early twentieth century period and the style known as Federation, with period houses related to the boom in Manly.

4.5 STATEMENT OF SIGNIFICANCE

The following Statement of Significance is provided for the subject streetscape in relation to the Pittwater Road Conservation Area:

The Victorian period subdivision of the Pittwater Road Heritage Conservation Area is distinctive and underpins the urban character of the area. This street pattern comprises the alignment, detailing and silhouette of the street facades and the overall scale of building in Pittwater Road is important. The streets remain unaltered in their alignment, although the names of Malvern, Pine and North Steyne are now names for what were Whistler, Middle Harbour and East Steyne respectively. In particular, the streetscape along Pittwater Road from Pacific Lane to Pine Street is a fine example of a remaining vista of the early settlement period in the Municipality specifically its scale and architectural interest and mixed use and for its association with the tram route and the major northern transport route.

The following Statement of Significance is offered for the subject property:

The house at 166 Pittwater Road, Manly is one of an intact pair of houses built during the Federation period in the Queen Anne style and it retains the distinctive internal and external features of the type, including the return verandah, brick entry porch and terra-cotta tiled roof, making a contribution to the streetscape both singly given the stone and cast-iron fence and as one of a pair.

GRADING OF SIGNIFICANT ELEMENTS

In some cases, it is useful to indicate the relative significance of the individual elements of an item as different components may make a different relative contribution to its heritage value. Loss of integrity or condition may diminish significance. The criteria for grading individual components provided in the 'Assessing Heritage Significance' (NSW Heritage Office 2001) guideline is presented in Table 2 below along with the significance grading of individual elements of the subject property.

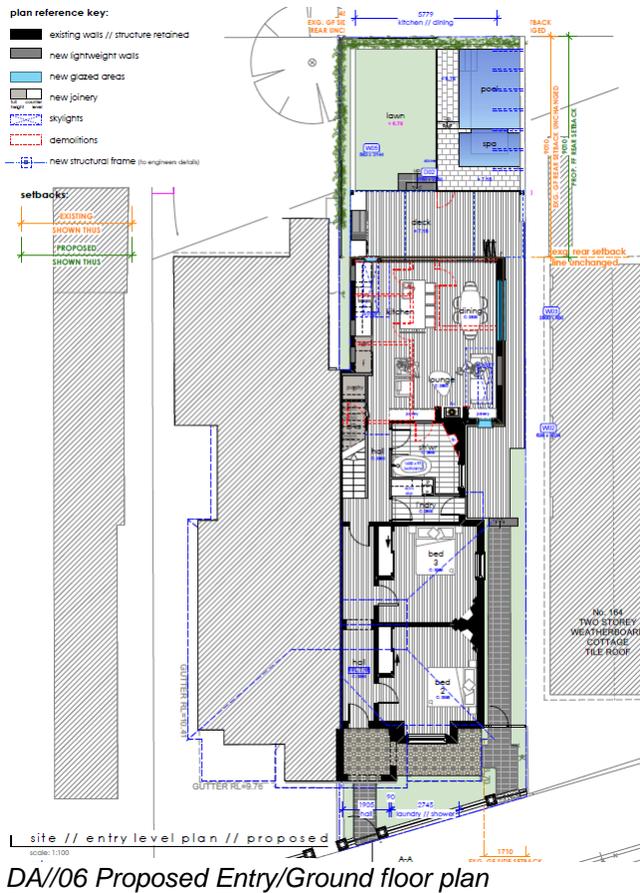
Table 2: Significance Assessment of 192 Pittwater Road, Manly

Grading	Justification	Status
<i>Exceptional</i>	A high degree of intactness & easily interpreted.	Original asymmetrical layout of the floor plan, the brick form and character with the porch and verandah: The street facing gable, terra-cotta tiled roof with details, chimneys, verandah and matching adjoining dwelling which forms the semi-detached building.
<i>High</i>	A high degree of original fabric & demonstration of the significance. Alterations do not detract from the significance.	The main building and in particular the front three rooms with related external character including the chimney on the southern side of the building serving the front two bedrooms
<i>Moderate</i>	Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.	The modified rear portion of the five rooms with intact southern brick elevation and external chimney serving the living/dining rooms
<i>Little or None</i>	Alterations detract from the significance & interpretation is difficult.	The current changes are generally sympathetic including the side decking and storage unit
<i>Intrusive</i>	Elements which detract from the heritage significance of the item & its interpretation.	The rear elevation with deck, pergola, yard and swimming pool are contemporary changes which appear incomplete. They are only viewed from the rear yards of adjoining properties and not visible from Pittwater Road.

5 HERITAGE IMPACT ASSESSMENT

5.1 INTRODUCTION

The following sub-sections provide a more detailed description of the proposed alterations and additions, and assesses the potential impacts of the works on the Pittwater Road Conservation Area.



5.2 PROPOSED WORKS

Drawings DA05 & 06 do not illustrate the existing rear of the building with remnant brick wall to north side of deck and do not show the existing storage unit to the front southern side of the building

Drawing DA06 illustrates the extent of the demolition work proposed

- Demolish the fireplace in the living room and doors to the rear of the house and third bedroom
 - The revised Floor plan indicates retention of the fireplace in the Lounge Room and retention of the door to the third bedroom while the original door from the Living room into the dining room is infilled to accommodate the bathroom and Lounge room joinery. The original opening is however capable of interpretation within the finishes.
- Demolish the fireplace to the dining room and wall defining the third bedroom
 - The revised Floor plan shows retention of the fireplace to the dining room. While door to the rear of the original house is removed for access next to the proposed stair, this can be interpreted with a bulkhead
- Demolish the walls defining the rear study, kitchen and bathroom
 - The loss of these walls, which have been modified in successive changes, do not affect the external character of the original dwelling.

Externally:

- Demolish the original brick chimney serving the former living and dining room
 - Drawing DA//11 has been revised and indicates retention of the distinctive chimney
- Demolish the windows to the living room, dining room and study on the south elevation
 - Drawings DA//06 Plan and 11 have been revised to show retention of the original window to the Living room has been retained while the original window to the Dining room is to be infilled to provide a solid wall for the TV now that the fireplace is to be retained and the

window to the Study is replaced on the East with glazing to address the rear outdoor living area. The large original window to the original Dining room is capable of interpretation within the infill detailing.

- Demolish the windows to the third bedroom, kitchen and bathroom on the north elevation
 - The revised drawings DA//06 and 10 indicate that these original windows are to be infilled to accommodate the new kitchen. This intent is taken from the plan as the Elevation DA//10 does not show these original external features as they are largely concealed from view by their location as part of the nature of the light well and adjoining semi-detached property. The three windows are capable of interpretation within the infill detailing.
- Demolish the rear external eastern wall to the deck
 - The existing brick wall to the northern end of the deck is not indicated on the existing or proposed plans. Despite it being a minor element, it is clearly original material and forms part of the dwelling as is indicated by the adjoining similar semi-detached house and is capable of retention as part of the existing/proposed barbecue
- The aerial view in the SIX maps view shows a metal clad hipped roof over the living room and skillion roof over the rear five rooms while the terra-cotta tiled roof exists over the front portion of the hipped roof
 - The roof materials indicated on Drawing DA//07 & DA//08 do not show any change to the metal clad hipped roof over the front bedroom 3 and the terra-cotta tiled roof over the front portion of the hipped roof

New works

- Construct hall, staircase, laundry and bathroom within former living room
- Construct open plan kitchen within former third bedroom, kitchen and bathroom
- Construct open lounge and dining area in the former dining and study

Externally

- Install three new windows in the south elevation and infill one existing window opening
 - The modified drawings indicate two new windows and the one infill
- Install a fixed window and set of bi-folding doors to the rear east elevation to the deck
- Install infills to three former windows in the north elevation
- New first floor to accommodate three bedrooms, stair and bathroom

There is no drawing which shows the existing roof plan. Drawings DA10 Existing and new north elevation, DA11 shows the existing and new south elevations, DA12 shows the existing and new east elevations, DA13 shows the existing and new west elevations and DA01 & DA16 shows a 3D view of the proposed first floor form, bulk and scale

Drawing DA07 illustrates the following works. The area aligns with the existing footprint

- Construct a staircase with void with fourth bedroom over the former living room
- Construct a study, ensuite and void over the former dining and third bedroom
- Construct a master bedroom, void and robes over the former study, bathroom and kitchen

Drawing 08 illustrates the proposed roof

- Construct a pitched roof sloping to the rear/East with skylight to void and interpreted dormer over master bed
- Construct a metal clad gable to the primary element on the South Elevation recessed 1800mm from the boundary
- Construct pitched roofs with skylights and traditional valley to the SW area with interpreted dormer over Bed 4 and retained chimney
- Construct a parapet dividing wall to the north side at the common boundary with the adjoining property and minimal pitched roof and sloping roof with skylight to stair & hall

Amendment: November 2022

Revised drawing DA/06 illustrates the revised extent of demolition work

- The corner fireplace is demolished in Living room to accommodate the bathroom while the fireplace in the Dining Room is retained for the new Lounge Room
- The level of demolition of the internal walls remains as per the previous proposal
- Externally, two windows are modified to accommodate the new bathroom and joinery in the lounge while the new long window remains previously proposed in the Dining Room.
- The primary change for the first floor and the roof plan is the use of pitched roof elements to accommodate the dormers to the south and east elevations. These are appropriate as forms and the detail will ensure that they blend and suit the character of the house while remaining contemporary in material and form
- The proposed South Elevation now offers a more conventional and traditional approach to the roof area and accommodation. The chimney is retained and fits comfortably between the two roof elements. The aluminium sliding glass window to the bathroom is uncharacteristic of the elevation. The use of a timber framed awning window is recommended as an alternative
- While the aluminium window to the lounge remains uncharacteristic, the low level will ensure that it remains concealed by the fence
- The East Elevation provides a suitable combination of gabled dormer element with sloping roof to ridge line beyond
- The expressed eave detail provides a detail to provide shadow and definition to the element and is an appropriate
- The window, shutters and roof-light provide modelling to the form and materials and are appropriate
- The West Elevation provides traditional sloping roof elements evident beyond the frontage. These will be visually recessive in the context of the original materials and details. The retained chimneys will remain visually expressed.

5.3 ASSESSMENT OF IMPACTS

The following activities have the potential to impact the character of the Conservation Area:

5.3.1 Demolish the internal walls defining the living room, dining room, third bedroom, rear study, kitchen and bathroom

- The impact is the loss of a major portion of the original intact dwelling. The portion is not generally visible from the main street however the rear portion is partly visible from the southern side access. The change to the fenestration will be considered in relation to the significance of the original south elevation. Generally, this portion of the south elevation is not visible to the streetscape although the materials and details contribute overall to the integrity of the HCA.
 - Following the modification, the level of demolition has been substantially reduced with the living and dining rooms capable of interpretation
 - The existing & proposed ground floor plans and East Elevation should be corrected to indicate retention of the original northern brick wall bounding the retained/replaced barbecue as this element is external and interprets the original part of the pair of semi-detached houses

5.3.2 Demolish the fireplace in the living room and the fireplace to the dining room

- The two original fireplaces defined the important living spaces consisting of the central living room and protruding dining room. The living room fireplace is demolished to accommodate the family bathroom while the Dining room fireplace is demolished to accommodate the open plan Lounge room. While both these elements have historic significance, they are not visible externally other than providing support to the chimney. Refer to the comment below in relation to the potential for retention of the Dining room fireplace.

- Following the modification, the two fireplaces and their associated external chimneys are to be retained

5.3.3 Demolish the brick chimney serving the former living and dining rooms

- The main chimney to the front of the building which serves the two bedrooms is to be retained, while the chimney in the Lounge Room which also serves the Dining room is to be removed. The chimney location is to be subsumed within the upper floor addition. The chimney at the front of the building makes a major contribution to the character and is clearly visible in the streetscape while the second chimney is also visible within the streetscape although the prominence is reduced given the additional setback from the front of the building.
- The chimneys are visual evidence in the streetscape and contribute both visually and historically to the Heritage Conservation Area. Evidence is provided, consistent with the standard HIS procedure, that retention has been considered. Retention of the two chimneys is reasonable and recommended while the loss of the Living room fireplace is acceptable given the lack of visibility and the need to adapt the interiors for accommodation. It is apparent that the chimney could be retained within the study space, in relation to the proposed first floor layout
- It is apparent that the rear fireplace could be retained as part of the proposed open plan lounge room however this is not a recommendation related to the impact on the significance within the HCA
 - Following the modification, the external brick chimney is retained

5.3.4 Demolish the windows to the living room, dining room and study on the south elevation

- The indented area which adjoins the former Living room is an original part of the construction and has a level of visibility while generally being concealed from the frontage. The door and double hung sash window are intact and may be capable of being retained without impacting the layout and design. The character indicated on the proposed South Elevation drawing DA11 illustrates loss of the door and fanlight and replacement of the double hung window with a horizontal aluminium framed highlight window.
- One option for consideration would be retention of the door and the double hung window or part of the double hung window given the uncharacteristic character of the sliding window.
- Should this not prove feasible then the existing openings may be retained as evidence only and infills provided with a nominal 20mm recessed plane to allow for the original openings and details such as sills to be visible for interpretation
- The side and south elevation of the dining room is not visible and has a low level of significance and hence the contemporary aluminium sliding window does not detract from the heritage significance
 - Following the modification, the original window to the former Living room is to be retained within the bathroom
 - Following the modification, the original door and fanlight to the former Living room are to be retained within the bathroom
 - The original window in the former Dining Room and proposed Living Room is to be infilled. In view of the proposed uses and other retentions, the infill will prove acceptable subject to a nominal 20mm recessed plane be provided at the infill to allow for the original opening and details such as sills to be visible for interpretation. This is to be noted on the drawings.

5.3.5 Demolish the windows to the third bedroom, kitchen and bathroom on the north elevation

- The side and north elevation is not visible and has a low level of significance and hence the proposed infills do not detract from the heritage significance

- Following the modification, the provision of the kitchen in this open plan area of the house is appreciated while the location is generally concealed from within the HCA. The infill would be acceptable subject to a nominal 20mm recessed plane be provided at the infill to allow for the original openings and details such as sills to be visible for interpretation. This is to be noted on the drawings.

5.3.6 Demolish the rear external eastern wall to the deck

- The rear and east elevation is only visible to the rear yard, has been the subject of previous alterations and additions and has a low level of significance and hence the proposed replacement bi-folding doors, highlights and pergola framing illustrated on DA12 do not detract from the heritage significance

5.3.7 Reconfiguration of ground floor to accommodate bathroom, stair, pantry, kitchen and living/dining space

The reconfiguration of the ground floor will update the floor plan for contemporary living. The impact is the loss of the original five rooms including the chimney and the visual impacts on Pittwater Road, the level of heritage significance as indicated with the external impacts on the windows and the general character within the HCA.

- Appropriate mitigations for the impacts on significance would be the mitigations and their options discussed:
 - Retain the chimney
 - Retain the southern door and window
 - Consider the recessed detailing for infills where required
 - Following the modification, the external brick chimney is retained, the door and fanlight are retained and the recessed detailing should prove to be acceptable and without loss of amenity for the dwelling and occupants.
 - An additional minor mitigation which may prove of benefit would be the use of a bulkhead to interpret the original wall between the hall and rear open plan living spaces. This is an internal matter and has no impact on the HCA.

5.3.8 New first floor to accommodate a stair, three voids, a study, fourth bedroom, master bedroom and ensuite bathroom

The new first floor is set back from the front building alignment commencing at the northern edge of the main hipped roof. It aligns with the ground floor walls below on the east, south and north sides.

- The vertical wall faces of the new floor rises to a parapet a nominal 3000mm above the proposed first floor level. The vertical faces of the first floor are evident on the south elevation as is evident on DA13 & 01. A sloping portion of the first roof is located over the south western void and is evident on DA11 and 01. On the east elevation shown on DADA01 & 12 the elevation is vertical with shutters while there is a pitched roof element over the void in the north-east corner. This is shown on the section DA09 and elevation DA10. The sides of the first floor are not set back from the northern boundary and 1900mm from the ground floor but include therefore the same setbacks from the boundaries. In summary the bulk is mitigated by three sloping roof elements at the corners of the form and two of these at the NE and SW reduce the apparent bulk.
- In terms of the building heights, the sections and elevations on DA09, 10 and 12 illustrate a difference of 500-600mm between the existing ridge height of the retained portion of the building and the parapet levels around the first-floor extension. The impact of this reduced level in terms of mitigation is reduced by the impact of the parapet around the raised portion. This is made clear in DA01 and 13 which show the rear, west and north elevation and the contrast with the predominant pitched roofs on the existing building and

the pitched roofs on the extension of the adjoining property to the south. One form of mitigation would be to reduce the floor to ceiling height from 2700mm to 2400mm which remains an acceptable level. A second alternative would be pitched roofs with a primary ridge line running east-west so as to reduce the bulk created by the parapet and its associated flat roof.

- In relation to the height of the first floor, the section on DA illustrates a floor to ceiling height of 2700mm which is similar to a standard floor to ceiling height but greater than a traditional attic ceiling height of 2400mm

The additional roof element will be visible from the Pittwater Road streetscape as is evident in drawings DA01 & 13. While the impact is reduced by the sloping element over the void and the single retained chimney, the remaining parapet elements are generally uncharacteristic of roofs to extensions within the HCA.

Therefore, a substantial visual impact to the conservation area will occur from the additional character height and bulk of the proposal.

There are two key issues which affect the expressed bulk and scale of the first floor:

- The character created by the use of the flat roof and the associated parapets. There is some flexibility in the design to explore the use of pitched roofs for the first floor;
- The adoption of a 2400mm floor to ceiling height would have some effect on reducing the impact of the bulk of the flat roof while it would not remove this uncharacteristic element;
- A series of other mitigating actions could be taken to reduce the impact of the works on the heritage significance of the views from Pittwater Road, the significance of the HCA and the significance within the building:
 - Retain the second chimney
 - Retain and interpret the door and windows in the south elevation
- The colour of the timber wall cladding and steel roofing requires careful selection to ensure that it does not visually dominate the setting. As the roof and wall elements are the primary new element and they are seen against the sky, the colour needs to be a mid-range Grey such as Basalt, Windspray or Wallaby. The use of Black – Dulux Domino while produce a sharp contrast between the existing roofs in the vicinity and the HCA generally. A mid-grey colour would be more sympathetic, respond to the local historic setting and other buildings in the vicinity.
- The use of Black aluminium windows will not impact on heritage significance at the rear and side of the building, however retention of the original door and windows in the retained south elevation with colour to match the existing, is recommended as a response to acknowledge the heritage significance of these materials and details.
 - Following the modification, the West Elevation which presents to Pittwater Road includes the retained chimneys and a roofscape using traditional forms and a recessive colour and appropriate material;
 - The proposed South Elevation utilises a traditional roof form as a gable and terminates the contemporary metal cladding to a level of brickwork which relates to the three key elements along that Elevation. A recessed metal detail intercedes between the retained brick and the metal gable and reflects the gutter alignment on the main building;
 - The proposed dormer on the South Elevation is a suitable element in this context and models the roof form;
 - While the sliding window is an uncharacteristic element, it largely remains out of view and allows for an appropriate level of cross ventilation;
 - The loss of the original large window in this elevation may be interpreted as recommended using the recess details;
 - The rear East Elevation reflects the use of traditional roof forms and the interpreted dormer offers an element suitable for the purpose while mitigating the bulk;

- The interpreted pergola element is a suitable mitigation for the loss of the rear verandah;
- The retained brick ends are a suitable mitigation for the loss of the rear ground floor brick wall and the amenity of the doors & glazing which develop the relationship between the interior and outdoor recreation space;
- The additional mitigation is the retention of the original brick wall and integration with the barbecue element

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The proposed roofing is Lysaght Enseam in Basalt or similar

- The cladding and colour are suitable given the context of the existing materials and colours
 - It is recommended that the 'or similar' be deleted, given the need to provide an appropriate heritage impact and the lack of clarity associated with or similar for a Certifier
- Gable elements are Timber in Basalt paint or similar. These are suitable and accepted
 - It is recommended that the 'or similar' be deleted, given the need to provide an appropriate heritage impact and the lack of clarity associated with or similar for a Certifier
- Domino is nominated for the contemporary windows and framing. This very contemporary colour is acceptable on these elements

The 'Statements of Heritage Impact' guidelines (NSW Heritage Office and Department of Urban Affairs & Planning 1996, revised 2002) post a range of statements for consideration when assessing impacts for various types of developments. Relevant considerations for each potential impact are addressed below.

5.3.9 "The following aspects of the proposal respect or enhance the heritage significance of the conservation area, for the following reasons:"

- The new ground floor alterations and first floor addition will provide contemporary standards of accommodation and will generally support the ability of houses in the Conservation Area to sustain practical living subject to amendments. The proposal has the capacity to respect the heritage significance of the conservation area with an acceptable impact on the streetscape.

5.3.10 "The following aspects of the proposal could detrimentally impact on the heritage significance of the conservation area:"

- While the bulk and height of the first-floor addition will be visible from Pittwater Road, the retention of the primary building and associated roof form will ensure that the impact will be secondary. The use of traditional roof forms, the details such as the dormer elements and the set back from the street frontage will ensure that the impact on the streetscape and the

immediate setting of the HCA will be acceptable. The retention of the original fabric in particular the second chimney and localised building elements on the south elevation will be suitable mitigations for the changes proposed.

5.3.11 “The following sympathetic solutions have been considered and discounted for the following reasons:”

- The retention of the chimney, the retention and re-use of the side door and double hung window, the use of mid-grey coloured wall cladding & roofing and the use of a lower attic style and scaled pitched roof to the first-floor extension will acknowledge the scale and significance of the original house within the streetscape. These strategies are recommended and have been adopted. The revised proposal has the capacity to prove sympathetic with the Heritage Conservation Area.
 - The modified proposal as evident in the November 2022 drawings have adopted the recommendations for retention of the distinctive elements. These included the chimney, external door and windows and the use of traditional roof forms and interpreted elements. The revised proposal is therefore assessed as having an appropriate level of impact on the significance of the building and that of the immediate setting within the HCA.
 - The one minor change is to the new window in the bathroom for the South Elevation. This window should be an awning window to avoid the sliding elements. An aluminium frame is acceptable given that it is a new element in this wall. While not essential, a window with a greater vertical dimension is recommended for both light/air and aesthetics.

6 CONSIDERATION OF LEP ISSUES

The Manly LEP 2013 provides objectives for the management of Heritage Conservation. They, and our consideration of them, are as follows:

- (a) *To conserve the environmental heritage of Manly,*
 - (b) *To conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views.*
- The proposed development aims to conserve and enhance the environmental heritage of Manly and the Conservation Area by:
 - Respecting the Heritage Conservation Area and setting of the subject site and providing alterations and additions which complement the character and significance of the existing building;
 - Providing mitigation strategies for ensuring that the new works complement the significant fabric – fenestration, chimneys and views – form and colours of the proposed first floor addition.

7 CONSIDERATION OF DCP ISSUES

The Manly DCP 2013 provides controls for its Conservation Areas. They, and our considerations of them, are addressed below.

7.1 Development in the vicinity of heritage items, or conservation areas

- b) *Proposed development in the vicinity of a heritage item or conservation area must ensure that:*
 - i) *it does not detract or significantly alter the heritage significance of any heritage items, conservation area or place;*
- The works proposed in the modified set of drawings will generally remain visually recessive in the streetscape and setting;

- The bulk and scale will not dominate the house or the adjoining building due to the reduced and stepped scale of the proposed attic style form and elevations to the West (Pittwater Road, East to the rear yard and South – to the adjoining property);
- The materials and details will complement and interpret those in the setting;
- Mitigation will be provided in the form of retained fabric, retained re-used materials and colour scheme.
 - ii) the heritage values or character of the locality are retained or enhanced; and*
 - The values and character offered by the extension, the form and setbacks will be supported.
 - iii) any contemporary response may not necessarily seek to replicate heritage details or character of heritage buildings in the vicinity, but must preserve heritage significance and integrity with complementary and respectful building form, proportions, scale, style, materials, colours and finishes and building/street alignments.*
 - The contemporary elements including the traditional roof forms, window screens and dormers reflect the scale and character through interpretation and not replication;
 - The final selection of colours and materials are sympathetic to those of the building and others in the immediate setting – noting that Monument is not an acceptable colour in the context and this HCA;
 - Mitigation is offered within the proposal through the modification of the roof form to the extension, the retention of key external and distinctive elements, a reduced bulk and height and a modified colour scheme;
 - Further minor changes to address the loss of detail are the standard recess detailing associated with the infill of windows and the use of the original brick from the works.

7.2 Potential Heritage Significance

If the property is assessed as having merit as a potential heritage item, the heritage controls and considerations in this plan will apply.

- The building is not considered as reaching the threshold for listing as a heritage item.
- The building is not considered rare to meet the social and historic criteria. The building has distinctive aesthetic character at a level capable of being retained through the mechanisms available as part of the Conservation Area listing.
- The building is capable of adaptive re-use while complementing the streetscape and Pittwater Road Conservation Area.

7.3 First Floor Additions

a) First floor additions must complement the architectural style of the ground floor and where possible retain existing roof forms. Notwithstanding setback provisions, the addition may follow the existing ground floor wall setbacks providing adjoining properties are not adversely impacted by overshadowing, view loss or privacy issue.

- The original proposal has proven to be capable of modification using a modified height and form to present an attic scaled addition to the streetscape with mitigating materials and the re-use of significant external elements.
- The modified scheme utilises traditional forms and interpreted details suited to the attic styled extension
- The set back of the first-floor elements from both the Pittwater Road frontage and to a lesser degree from the southern boundary, will ensure that the addition is sympathetic with the HCA and adjoining properties.

b) The dwelling and the form of alterations and additions must retain the existing scale and character of the street and should not degrade the amenity of surrounding residences or the

aesthetic quality of the former Manly Council area. In this regard, it may be preferable that the addition be confined to the rear of the premises or be contained within the roof structure.

- The original proposed form, character and height proved to be capable of reconsideration. The revised design reduced the visual impact of the first floor and ensures that the bulk and scale of the proposal do not dominate views to the site at the front in the vicinity of Pittwater Road and to a lesser extent with the rear and side elevations.

7.4 Complementary Form and Scale that Distinguishes Heritage Significance

a) Alterations or additions to heritage items or buildings within a conservation area will not necessarily seek to replicate, overwhelm, dominate or challenge heritage details or character of the building or structure of heritage significant buildings. However, a contemporary response which complements and respects the form and scale of the original buildings may be considered if the heritage significance is retained.

- The contemporary form and materials of the new works at the first floor are subtle and will not detract from the federation period, character and scale, while the detail and colour of the new works will prove sympathetic with the existing building and the setting.
- The colours and materials are sympathetic with the brick and tiled roof of the original & modified building subject to the adoption of the Conclusion and recommendations.

b) Consideration should be given to whether making a house bigger will ruin its appearance. Additions to small houses can easily overwhelm them and use up garden space needed for private open space and impact the setting and pattern of development in the locality. Modest additions work best and can be organised as wings or pavilions to the existing house. All additions must be at the back of the house, not the front.

- No changes are proposed to the rear yard
- The former changes – currently incomplete, will be finished and provide suitable shade, privacy and detailing for the deck and pergola and setting to the proposed first floor
- A suitable set of two spaces is retained at the rear for outdoor recreation.

7.5 Retaining Significant Features and Landscape Setting.

Note: Significant features in relation to this paragraph include roofs, detailing, brickwork, colours and original windows (size, proportion and type).

Alterations or additions to heritage items or buildings within a conservation area must:

a) retain original and traditional roof form, roof pitch with any alterations to the roofs to be sympathetic to the style of the heritage item or building within a conservation area;

- The new roof element for the proposed attic styled first floor utilises the relevant pitch and character and interprets the traditional scale and character. Suitable colours have been recommended and specified to suit the view angles and context.
- A minimum of two chimneys were recommended for retention based on their significance and prominence in the Pittwater Road streetscape and this approach has been adopted and illustrated in the drawings.

b) retain original architectural detailing such as barge board, finial trim, window awnings and front verandas. New detailing must be complementary to the character of the item or place;

- Original architectural detailing to the front portion of the residence will be retained.
- Additional minor retention of original details is recommended to mitigate the alterations and additions.

- c) *retain original wall treatments and original cladding (including slate). Modifications to face brick dwellings must use the original style of bricks, window heads, mortar joints and other building details;*
- Existing brick and wall treatments will be retained. New face brick will be sympathetic to the existing to interpret the original in a suitable contemporary manner.
 - The retention of the sloping brick parapet on the south elevation is recommended for consideration as suitable matching bricks will prove difficult to source. This modification has not been adopted however the use of the recessed metal detailing to align the brick elements should prove suitable and avoid the inclination of the former skillion in the extended built form while it is likely that there will remain sufficient red face brick for the additional wall and infills. The proposed South Elevation is therefore acceptable in terms of the heritage impact on the HCA.
- d) *no render or paint original face brickwork. In particular face brickwork where already so treated should be restored, where practical, to its original un-painted state;*
- Existing brickwork will not be rendered or painted.
- e) *avoid removal of original fabric in order to retain the integrity of the heritage item or conservation area;*
- The recommendations for the materials and details such as the door and windows on the south elevation were recommended for retention in the original assessment and these have been adopted and are evident in the current drawings.

7.6 Fences for Heritage Items and Conservation Areas

- a) *Modifications to the front fence and garden of a heritage item or buildings within a conservation area must be designed and constructed in materials that contribute to and not detract from the historic style of the building and character of the streetscape.*
- b) *Original fences must be retained and refurbished, where possible. New fences will be sympathetic in colour, material, height and design and will not detract from the heritage significance of the building or locality.*

7.7 Development fronting Pittwater Road

In relation to development fronting Pittwater Road, Council must be satisfied that DAs will not:

- a) *adversely affect the amenity of the locality;*
- There will be no adverse affects on the Pittwater Road area from the proposed development in terms of the heritage matters: character, scale, bulk, materials and details
- b) *result in excessive vehicular movements to and from the site or in adjacent residential streets;*
- It is proposed that the three-bedroom residence will become a four-bedroom residence. This will not result in excessive vehicular movements to and from the site.
- c) *involve signage or other non-structural change in the appearance of the exterior of the building that is inconsistent with the preservation or restoration of the heritage streetscape in the vicinity;*

- There will be no signage or non-structural changes in the appearance of the exterior of the building.

d) change in the appearance of the exterior of a building without being in keeping with the preservation or restoration of the heritage streetscape.

- The change in appearance and character will be sympathetic with the Conservation Area and the setting of the subject building in particular.

8 CONCLUSION

The proposed works, as revised in the November 2022 proposal drawings, to the existing semi-detached dwelling at 166 Pittwater Road will have an acceptable impact upon the heritage significance of the immediate setting and streetscape of the Heritage Conservation Area. The proposed revisions with the new elements are sympathetic with the character of the federation period of the subject building and meet the guidelines within the relevant DCP.

The mitigation measures proposed in the original assessment are documented in this report and have largely been adopted. The following final measures are recommended to ensure that the proposal will be sympathetic with the character of the HCA:

- All brick infills to the windows are to be provided with a nominal 20mm recessed plane to allow for the original openings and the retained details such as sills to be visible for interpretation. The use of the brick and the detail needs to be noted on the drawings in particular Drawing DA11 for the South Elevation;
- Original brick from the elements being removed, is to be utilised for the window infills to ensure that the materials and details are sympathetic;
- Metal roofing is to be similar to Lysaght Enseam with a maximum rib height of 38mm and pan width of 465mm so as to ensure that visual impact of the roofing does not detract from the character of the predominant tile in the traditional buildings which dominate the HCA. The low rib profile and the broad pan is regarded as being visually comparable with the texture of tiling in federation period buildings while the high standing seam roofing with closely spaced ribs is contemporary, highly textured and therefore not characteristic of roofing in an HCA;
- Contemporary paint colours such as shades of Grey are not to be applied to the original materials of the traditional building including the chimneys or details on the West and East elevations and any changes to these materials and details are to be selected from the appropriate heritage palette and applied to each building element consistent with traditional practice for that element so as to ensure that the character of the building remains with integrity within its conservation area setting;
- The one minor change is to the new window in the bathroom for the South Elevation. This window should be an awning window to avoid the sliding elements. An aluminium frame is acceptable given that it is a new element in this wall. While not essential, a window with a greater vertical dimension is recommended for both light/air and aesthetics.

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