



**Development Application**

**PROPOSED ALTERATIONS AND ADDITIONS TO AN  
EXISTING DETACHED RESIDENCE**

**Statement of Environmental Effects**

**19 MILDRED AVENUE, MANLY VALE**

Northern Beaches Council

February 2019

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## 1. INTRODUCTION

Brenchley Architects has been engaged to prepare a Statement of Environmental Effects for a Development Application for alterations and additions to a detached residence at 19 Mildred Avenue, Manly Vale including:

- Extension of existing vehicle crossover and driveway and construction of new carport
- Extension of existing front deck and new entry stair
- New stair to existing deck at rear of property
- New stair in front setback to access side boundary gate
- General landscape amendments to front setback

This statement is accompanied by plans and documents which include:

- Brenchley Architects, 2018-015-A00, A01, A02, A03, A04, A05
- Brenchley Architects - External Finishes
- Survey, CMS Surveyors Pty Ltd
- Dapple Designs – Landscape Plan
- Civil + Structural Engineering Design Services Pty Ltd – Preliminary Geotechnical Assessment

The site lies within the jurisdiction of Northern Beaches Council (formerly Warringah Council).

The proposal is permissible in the R2 – Low Density Residential zone and is considered to be consistent with the objectives of the zone under the Warringah Local Environmental Plan, 2011 (LEP) and Warringah Council Development Control Plan, 2011 (DCP).

The proposed alterations and additions aim to greatly improve the amenity to the existing dwelling, and provide this in an attractive and sympathetic manner to the existing context and streetscape. The proposed carport is consistent with the context of the locality with minimal impacts upon neighbouring properties.

## 2. SUBJECT SITE

The subject site is known as 19 Mildred Avenue (Lot 2 in DP 1229415). The site is of an irregular shape with a total site area of 600.6sqm, and slopes down from north to south from Mildred Avenue. The site has a north/south orientation with a street frontage of 24.1m. The subject site is not a heritage item, nor is it in a Conservation Area.

The site is currently occupied by one single storey, detached masonry residence with basement level to the rear. There is a steep rock driveway in the front setback of the property, leading to the garage at basement level. The site is bound by two storey dwellings to the east and south, with a vacant plot of land to the west, and 1/2 storey dwellings across the road to the north of varying eras and styles. There are also a number of garages bounding the street in the area.



Figure 1: Location Map, Six Maps image with outlined site area





Figure 2: Google Streetview image of 19 Mildred Avenue, Manly Vale



Figure 3: Close up of 19 Mildred Avenue, Manly Vale



Figure 4: Close up of 19 Mildred Avenue, Manly Vale

### 3. SURROUNDING DEVELOPMENT

The surrounding area is predominantly residential, with a variety of single storey, two storey and 3 storey dwellings. The streetscape is predominantly pitched, tiled roof dwellings, with a mixture of rendered and brick facades. There are also a number of contemporary, flat roofed dwellings in the vicinity.

#### East

The direct eastern neighbour of the subject site is 26 Horning Parade, a 2 storey dwelling plus basement garage and detached garage accessed from Mildred Avenue, which bounds the subject site and Mildred



Avenue. Further east, No. 40 Mildred Avenue is also a 2 storey brick dwelling, and also exhibits a garage forward of the building line.



Figure 5: Garage to 26 Horning Parade (right) and 40 Mildred Avenue (left)

#### West

The plot of land directly west of the site is currently vacant, known as 17 Mildred Avenue.



Figure 6: 17 Mildred Avenue – vacant land (shed now demolished)

Further west is 15 Mildred Avenue, a contemporary white rendered, 2 storey dwelling.



Figure 7: 15 Mildred Avenue

#### North

Across the road, to the north of the subject site, are a number of 1/2 storey brick and rendered dwellings.



Figure 8: Left to right, 28 Mildred Ave, 30 Mildred Ave, 32 Mildred Ave + 34 Mildred Ave



South

The southern neighbour at the rear of the site is 28 Horning Parade a two storey residence, with additional garage level. Further south is 30 Horning Parade, a 1 / 2 storey dwelling.



*Figure 9: 28 Horning Parade viewed from subject site*



*Figure 10: 30 Horning Parade viewed from subject site*

#### 4. PROPOSAL

The proposal involves:

- Extension of existing vehicle crossover / driveway and construction of new carport;
- Extension of existing front deck and new entry stair;
- New stair to existing deck at rear of property and new deck in place of existing raised planter at rear;
- New stair in front setback to access side boundary gate;
- General landscape upgrades to front setback;
- Proposed removal of 2 bottle brush trees in nature strip adjacent to the new widened vehicle crossover and planting of 2 new trees in nature strip as replacement.

#### 5. COMPLIANCE TABLE

WARRINGAH LEP 2011			
DEVELOPMENT STANDARDS	REQUIREMENT	PROPOSED	COMPLIANCE
4.3 Height of Buildings	8.5m	Main House – as existing Carport – 3.4-3.8m (sloping site)	YES
4.4 Floor Space Ratio	Not adopted	N/A	N/A
5.10 Heritage Conservation	Heritage items (if any) are listed and described in Schedule 5. Heritage conservation areas (if any) are shown on the <a href="#">Heritage Map</a> as well as being described in Schedule 5.	N/A	Not applicable – the subject site is not a heritage item nor is it in a heritage conservation area.
6.1 Acid sulphate soils	1. The objective of this clause is to ensure that development does not disturb, expose or drain acid sulfate soils and cause environmental damage.	N/A	N/A – not in an acid sulphate zone
6.2 Earthworks	1. The objectives of this clause are as follows: (a) to ensure that earthworks for which development consent is required will not have a detrimental impact on environmental functions and processes, neighbouring uses, cultural or heritage items or features of the surrounding land (b) to allow earthworks of a minor nature without requiring separate development consent	The proposal does not involve any significant excavation of the site. There will be minimal excavation to extend the existing vehicular crossover and for the construction of the carport, which will be carried out to ensure the safety and protection of the site and its surrounds.	YES
6.4 Development on sloping land	(a) to avoid significant adverse impacts on development and on properties in the vicinity of development sites resulting from landslides originating either on or near sloping land (b) to ensure the impacts of storm water runoff from development on or near sloping land are minimised so as to not adversely affect the stability of the subject and surrounding land, (c) to ensure subsurface flows are not adversely affected by	Minimal excavation will be carried out to extend the existing driveway, which will not impact or undermine the overall existing topography of the site – refer to accompanying preliminary geotechnical assessment.	YES

	development so as to not impact on the stability of existing or adjoining land.		
<b>WARRINGAH DCP 2011 – Relevant Development Controls</b>			
B1 – Wall Heights	Walls are not to exceed 7.2 metres from ground level (existing) to the underside of the ceiling on the uppermost floor of the building (excluding habitable areas wholly located within a roof space)	The proposed development results in no change to existing wall heights.  Proposed carport has a maximum height of 3.4-3.8m (sloping site)	YES
B3 – Side Boundary Envelope	Buildings must be sited within a building envelope determined by projecting planes at 45 degrees from a height of 4m above ground level (existing) at the side boundaries	No change to existing building fabric and therefore no impact on existing side boundary envelopes	N/A
B5 – Side Boundary Setbacks	0.9m	No change to existing side boundary setbacks	N/A
B7 – Front Setbacks	6.5m - Development is to maintain a minimum setback to road frontages. - The front boundary setback area is to be landscaped and generally free of any structures, basements, carparking or site facilities other than driveways, letter boxes, garbage storage areas and fences	6.7 – 11m - No change to existing building setbacks  Single carport within front setback – setback 1.5m from front boundary.  Extension to front deck – setback 7.17m from front boundary	N/A  MERIT – Minimal impact and other carports/garages in front setbacks in area – see below for more details  YES
B9 – Rear Setbacks	6m - Development is to maintain a minimum setback to rear boundaries. 1. The rear setback area is to be landscaped and free of any above or below ground structures.	No change to existing rear building setbacks  New stair to existing deck  New deck in place of existing brick planted area	N/A  MERIT – improving amenity – see below for further detail  MERIT – improving amenity – see below for further detail
C3 – Parking facilities	Garage doors and carports are to be integrated into the house design and to not dominate the façade. Parking is to be located within buildings or on site.  • Parking is to be located so that views of the street from front windows are not obscured;  • Where garages and carports face the street, ensure that the garage or carport opening does not exceed 6 metres or 50% of the building width, whichever is the lesser.	Carport separate to main house  Partial obstruction of views from Bed 1, but views of street maintained from all other front facing bedrooms, dining room and entry  Existing building width = 18.27m (50% = 9.1m) Proposed carport width = 6m	MERIT – improving amenity – see below for further detail  YES  YES

C4 – Stormwater	Stormwater runoff must not cause downstream flooding and must have minimal environmental impact on any receiving stormwater infrastructure, watercourse, stream, lagoon, lake and waterway or the like. The stormwater drainage systems for all developments are to be designed, installed and maintained in accordance with Council's Water Management Policy.	No change to existing stormwater infrastructure – new structures to connect to existing system	YES
C7 – Excavation + Landfill	Excavation and landfill works must not result in any adverse impact on adjoining land.	Minimal excavation (max 1m or less) proposed to extend existing driveway, and to create new graded parking area under carport and stair (refer to accompanying preliminary geotech assessment for more detail)	YES
D1 – Landscaped Open Space + Bushland setting	Min 40% of site area	251sqm landscaped open space proposed (41.8%)	YES
D2 – Private Open Space	Dwelling houses with 3 or more bedrooms - 60sqm with minimum dimension of 5m	Existing private open space: 61.4sqm  Proposed: 71.6sqm with improved access from main living areas and additional deck from secondary living area	YES
D6 – Access to sunlight	1. Development should avoid unreasonable overshadowing any public open space. 2. At least 50% of the required area of private open space of each dwelling and at least 50% of the required area of private open space of adjoining dwellings are to receive a minimum of 3 hours of sunlight between 9am and 3pm on June 21	Proposal does not overshadow or impact the solar access of neighbouring properties	YES
D7 – Views	Development shall provide for the reasonable sharing of views.	Proposal does not impact on any existing views	YES
D8 - Privacy	1. Building layout should be designed to optimise privacy for occupants of the development and occupants of adjoining properties. 2. Orientate living areas, habitable rooms and windows to private open space areas or to the street to limit overlooking. 3. The effective location of doors, windows and balconies to avoid overlooking is preferred to the use of screening devices, high sills or obscured glass.	Proposal does not impact privacy of neighbouring properties - privacy screen constructed to rear boundary to mitigate any potential overlooking	YES
D9 – Building	1.Side and rear setbacks are to be	No change to overall	YES – see below for

Bulk	<p>progressively increased as wall height increases.</p> <p>2. Large areas of continuous wall planes are to be avoided by varying building setbacks and using appropriate techniques to provide visual relief.</p> <p>3. On sloping land, the height and bulk of development (particularly on the downhill side) is to be minimised, and the need for cut and fill reduced by designs which minimise the building footprint and allow the building mass to step down the slope.</p> <p>4. Building height and scale needs to relate to topography and site conditions.</p> <p>5. Orientate development to address the street.</p> <p>6. Use colour, materials and surface treatment to reduce building bulk.</p> <p>7. Landscape plantings are to be provided to reduce the visual bulk of new building and works.</p> <p>8. Articulate walls to reduce building mass.</p>	<p>existing building mass</p> <p>New deck and roof to front of house to be contained within existing building line and under existing gutter line</p> <p>New carport height to be in line with existing building gutter line and be designed to minimise visual bulk</p>	further justification
D10 – Building colours + materials	<p>1. In highly visible areas, the visual impact of new development (including any structures required to retain land) is to be minimized through the use of appropriate colours and materials and landscaping.</p> <p>2. The colours and materials of development on sites adjoining, or in close proximity to, <a href="#">bushland</a> areas, waterways or the beach must blend in to the natural landscape.</p> <p>3. The colours and materials used for alterations and additions to an existing structure shall complement the existing external building façade</p>	Proposed materials of painted brick, colorbond roofing and timber structures complement and reflect the varied existing character of the area	YES
D11 - Roofs	<p>2. Roofs should complement the roof pitch and forms of the existing buildings in the streetscape.</p> <p>3. Articulate the roof with elements such as dormers, gables, balconies, verandahs and pergolas.</p> <p>4. Roofs shall incorporate eaves for shading</p> <p>5. Roofing materials should not cause excessive glare and reflection.</p>	<p>No change to existing roof profile of main house.</p> <p>Carport/front deck roof to be pitched to complement the existing house and to be of matching materials</p>	YES
D13 – Front fences + walls	<p>1. Fences, including side fences, located within the street setback area are to be compatible with the existing streetscape character.</p> <p>2. Where a solid fence is required it is to be articulated to provide visual interest and set back to allow for</p>	<p>Low boundary fences to front setback proposed, to maintain existing openness on the boundary.</p> <p>Infill side boundary</p>	YES



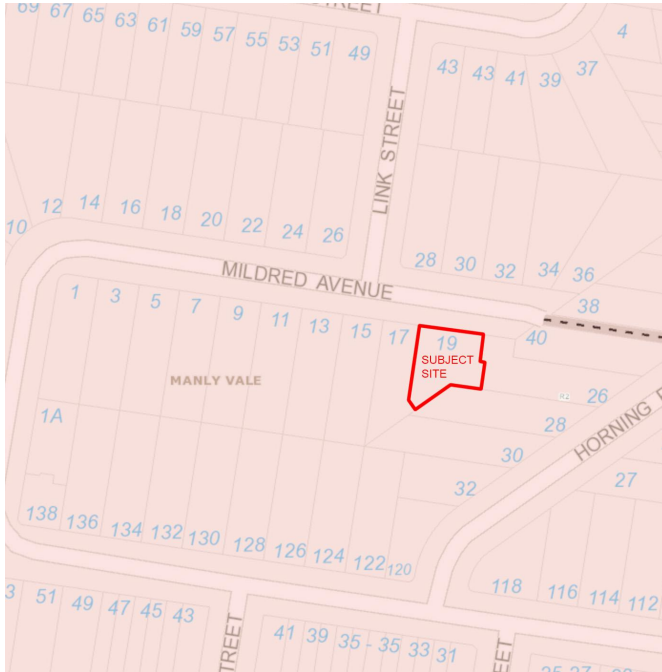
	<p>landscaping to soften and screen the appearance of the fence.</p> <p>3. Fences located within the front building setback area are to complement the existing streetscape character.</p> <p>4. Fences are to be constructed to allow casual surveillance, except where there is excessive noise.</p> <p>5. Gates are not to encroach over the property boundary when opening or closing.</p> <p>6. Fences should complement the architectural period of the building.</p>	<p>fences to match existing, remaining rear and side boundary fences are to remain as existing.</p>	
D15 – Side + rear fences	<p>No higher than 1.8 metres on level sites, or 1.8 metres measured from the low side where there is a difference in either side of the boundary.</p> <p>2. For sloping sites, the height of fences may be averaged and fences and walls may be regularly stepped.</p> <p>3. All fencing materials are to complement the existing neighbourhood. The use of corrugated metal, barbed wire or broken glass is not permitted.</p>	<p>Existing side and rear boundary fences retained, with addition of new timber privacy fence adjacent to proposed stairs to rear boundary which will complement the existing boundary treatment.</p>	YES
E1 – Preservation of trees or bushland vegetation	<p>A Vegetation Clearing Permit is required for:</p> <p>a) Removal or cutting down of any tree over five (5) metres in height;</p> <p>b) Pruning of more than ten percent (10%) of a tree canopy.</p> <p>c) The removal or cutting down of vegetation in “Bushland”.</p>	<p>No trees or bushland vegetation above 5m is proposed to be removed.</p> <p>Common Lilly Pilly hedge to rear boundary will be removed to improve access between main living areas and main outdoor space</p> <p>2 x Bottle Brush tree on council's nature strip which may be affected by new driveway/vehicle crossover extension proposed to be removed. 2 new trees will be planted in the nature strip as a replacement – refer to landscape plan for more details</p>	YES
E6 – Retaining unique environmental features	<p>Development is to be designed to address any distinctive environmental features of the site and on adjoining nearby land</p>	<p>No distinctive environmental features on site</p>	N/A
E10 – Landslip risk	<p>The proposed development must be justified in terms of geotechnical stability; and will be carried out in accordance with good engineering practice, must not cause detrimental impacts because of stormwater discharge from the land, and must not cause detrimental impact on the existing subsurface flow conditions including those of other properties.</p>	<p>Area B – Flanking slopes from 5 to 25 degrees – Refer to accompanying Preliminary Geotechnical Assessment as per DCP requirements – summarising that proposed works will not adversely affect the geotechnical stability of the site.</p>	YES

## 6. ASSESSMENT UNDER RELEVANT CONTROLS

### LOCAL ENVIRONMENT PLANS

#### Warringah Local Environment Plan, 2011

##### Zoning



### Building Height

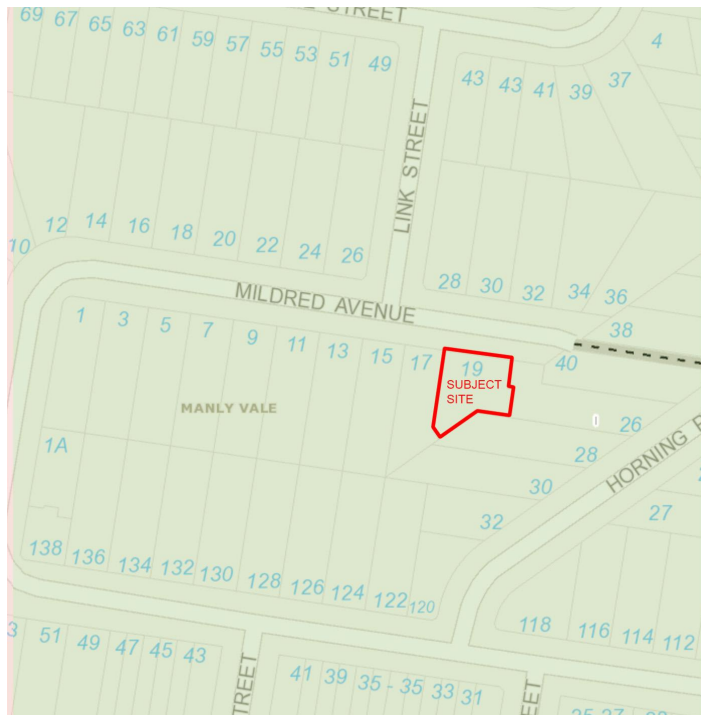


Figure 12: Warringah LEP 2011 – Building Height Map – Zone I = 8.5m

The site falls under Zone I of the Warringah LEP 2011, allowing a building height of 8.5m. The proposed development results in no change to the height of the existing building, and the height of the proposed carport is well under the 8.5m height limit at a maximum of 3.8m height.

### Heritage

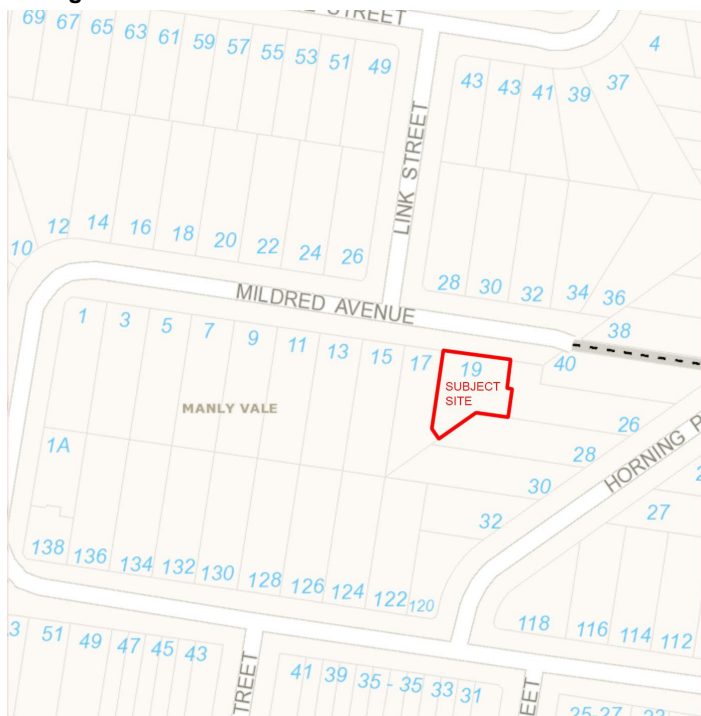


Figure 13: Warringah LEP 2011 - Heritage Map

The subject site is neither a heritage item nor in a heritage conservation area.

## Land Slip Risk

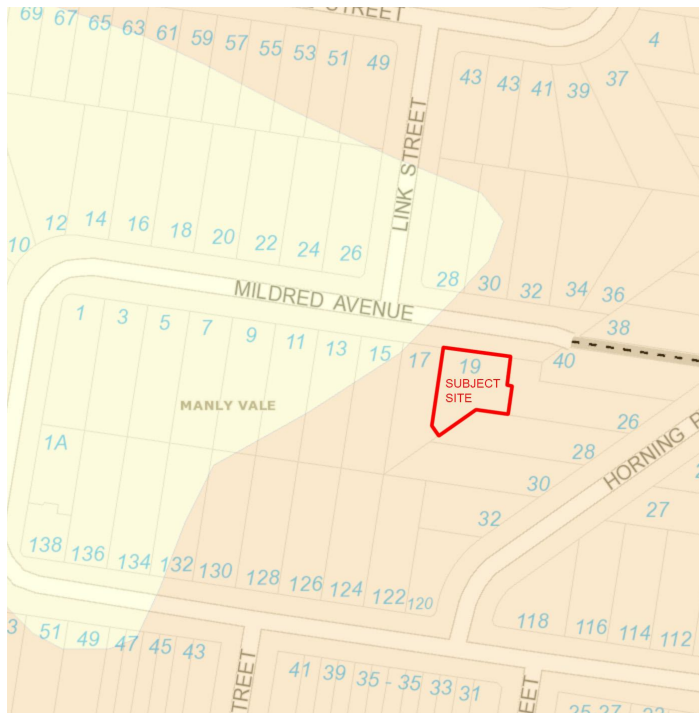


Figure 14: Warringah LEP 2011 – Land Slip Risk Map

The subject site falls under Area B – flanking slopes from 5 to 25 degrees. As such, a preliminary geotechnical assessment prepared in accordance with the requirements set out in the Warringah DCP 2011 (Section E10 Land Slip Risk) has been prepared as part of this DA, and can be referred to separately, and concludes that proposed works will not adversely affect the geotechnical stability of the site, and a full geotechnical report is not required.

## DEVELOPMENT CONTROL PLANS

### Warringah Development Control Plan 2011

#### GENERAL DESIGN PROVISIONS

##### **B1 – Wall Heights**

The proposal results in no change to the existing wall heights of the main dwelling. The new carport has a total height of 3.4-3.8m, which is well under the allowable 7.2m height.

##### **B3 – Side boundary envelope**

No change is proposed to existing side boundary envelope, and therefore there is no adverse impact.

##### **B5 – Side boundary setbacks**

No change is proposed to existing side boundary setbacks, and therefore there is no adverse impact.

##### **B7 – Front Setbacks**

The existing building is setback a minimum of 6.7m, maximum of 11m from the front boundary, and there is no change proposed to these existing setbacks and therefore no adverse impacts in this regard.

A single carport is proposed within the front setback, which is setback 1.5m from the front boundary, sitting parallel to the front boundary. Although this is not in accordance with the DCP, it is considered acceptable in that it improves amenity to the existing dwelling by providing better access from the cars to the main entrance and main house. There is currently no direct access from the garage and driveway through to the main living space on ground floor without coming back outside and up around to the main front entrance. As such, this accounts for the orientation of the carport, being parallel to the front boundary, so as to allow direct access to the main entrance from the carport. This position within the front setback still allows for views of the street to be maintained from the main bedrooms, dining and entrance of the dwelling. Furthermore, the perceived bulk of the carport has been reduced through the use of an open timber frame structure, with 50% open timber batten infill, rather than a solid structure.



It is also noted that the provision of a carport or garage within the front setback is common in the local area, with numerous examples of this existing already on Mildred Avenue and the surrounding streets. Examples can be seen below.

In summary, the size and location of the carport within the front setback is appropriate within the street context, the carport greatly improves the amenity of the existing dwelling, is designed to complement the surrounding area whilst maintaining views of the street from the dwelling, and is therefore worthy of approval.



Figure 15: Examples of adjacent houses with garages/carports in front setback. Left to right: 38 Mildred Avenue, 40 Mildred Avenue.



Figure 16: Examples of adjacent houses with garages/carports in front setback – 18 Mildred Avenue





Figure 17: Examples of adjacent houses with garages/carports in front setback - 1 Mildred Avenue



Figure 18: Examples of adjacent houses with garages/carports in front setback - 126 Kenneth Road, Manly Vale



Figure 19: *Examples of adjacent houses with garages/carports in front setback - 122 Kenneth Road, Manly Vale*

### **B9 – Rear Boundary Setbacks**

The DCP requires that buildings be setback a minimum of 6m from the rear boundary and that the rear setback is to be landscaped and free from structures. Due to the irregular shape of the site and existing levels, the existing rear setbacks vary from 1.6m-2.9m and already contain an existing deck and brick planted area accessed from the ground floor living areas. There is currently no access from the deck or raised planted area to the main garden, which is at lower ground level below. It is proposed to introduce a stair from the existing deck and introduce a new decked area in the exact position of the existing planted area in order to improve the amenity of the house in accessing primary open space to the rear of the property. Due to the constraints of the existing rear setback and irregular shape of the site, the proposed stair runs adjacent to the boundary. Whilst the area does not overlook the principle private open space of the southern neighbour, any potential privacy issues have been addressed through the introduction of a timber privacy screen where the stair occurs.

The new deck to the rear is setback from the boundary 2.37m, being in the location of an existing raised planter, and is already accessible from the living area. The area does not impact the privacy of the southern neighbour, and provides improved private open space for the dwelling, which is currently minimal and split.

The proposed new stair and upgraded deck area in place of planted area greatly improve the amenity of the house, allowing access to the main private open space from the main living areas, which is not currently available, whilst not impacting the amenity of adjacent neighbours, and is therefore an appropriate development in the rear setback.

### **C3 – Parking Facilities**

Whilst the DCP requires that garages and carports be incorporated into the main house, the proposed carport in the front setback allows improved amenity to the dwelling house by providing direct access to the dwelling entrance and providing covered parking for a second car (there is currently no direct access from the garage and driveway through to the main living space on ground floor without coming back outside and up around to the main front entrance). Furthermore, it does not restrict the majority of views of the street from the house, with views being maintained from the main entrance, dining, bath, and two front bedrooms. It also complies with the width requirements of the DCP when presenting to the street frontage, being 6m wide. Add to this the presence of other detached garages and carports in the area, as detailed previously, the location and size of the carport does not affect the amenity of the property or neighbours and is an appropriate location and orientation to improve the amenity of the dwelling.

**C4 - Stormwater**

The proposed development intends to use existing stormwater drainage systems already installed on the site.

**C7 – Excavation + Landfill**

Minimal excavation is proposed to create a graded parking area in the front setback, which will not have a negative visual impact on the streetscape. An access stair from the driveway will require some excavation near the boundary, but care will be taken to make sure that no damage is done to the neighbouring garage of 28 Horning Parade that bounds the site. Existing natural ground levels will be retained everywhere else on the site. A preliminary geotechnical assessment prepared in accordance with the requirements set out in the Warringah DCP 2011 (Section E10 Land Slip Risk) has been prepared as part of this DA, and can be referred to separately. The report concludes that the proposed works will not adversely affect the geotechnical stability of the site, and that a full geotechnical report is not required.

**D1 – Landscaped Open Space + Bushland Setting**

The DCP requires that a minimum 40% of site is landscaped area, the proposal maintains 41.8% landscaped open space, and thus complies.

**D2 – Private Open Space**

The existing private open space totals 61.4sqm, in the form of existing deck and rear garden, however the lower garden is currently not directly accessible from the living area on the ground floor. The proposed new stair to the rear deck provides direct access from the living area and thus greatly improves the existing private open space amenity. Furthermore, the new small deck area in place of existing planter further increases the private open space area, which is currently constrained due the irregular site shape and setback constraints to the rear of the property, and should be counted as additional private open space given the constraints of the site.

In summary, the new stair and deck improves access and amenity to private open space, and as such, is deemed appropriate development.

**D6 – Access to Sunlight**

Proposal does not overshadow or impact the solar access of neighbouring properties as all major development is contained to the north of the site.

**D7 - Views**

No impact to existing views.

**D8 - Privacy**

The proposal will result in no adverse impact on the privacy of adjoining neighbours. To mitigate any potential overlooking, an extension of existing timber privacy screen is proposed on the boundary where the new stair occurs.

**D9 – Building Bulk**

No change is proposed to the existing overall building bulk. The new deck and roof to the front of the house sits below the existing gutter line, is contained within the existing building line, and is set back 7.1m from the front boundary, so makes no detrimental impact to the perceived bulk of the proposal.

Furthermore, the detached carport is also of appropriate scale to the existing development, with the proposed ridge line sitting below the level of the existing house gutter line. The perceived bulk of the carport is further broken down through the use of an open frame construction and 50% open timber batten infill. Therefore the proposed development is deemed appropriate as it makes no detrimental impact to the building bulk on the site.

**D10 – Building Colours + Materials**

The existing area is a mixture of traditional and contemporary dwellings as well as a variety of building materials and finishes, including render, brick, pitched metal/tiled roofs and timber detailing. The proposal seeks to unify and modernise the material palette of the existing house through the use of painted white brick, grey metal roofing, and accents of black stained timber structures, bringing the house up to date and contributing positively to the already varied materiality of the area.

**D11 – Roofs**

No change to existing house roofs. The proposed new roof over the deck and carport is to be pitched to complement the existing house roof, and to be mid grey colorbond metal to match the existing roof.



**D13 – Front Fences + Walls**

Low 400mm high rendered front boundary walls are proposed, retaining the existing openness of the site to the streetscape whilst adding definition to the boundary. Front fences in the area are of varying heights and finishes, so the new low front boundary wall is considered to be appropriate in the streetscape.

**D15 – Side + Rear Fences**

Existing side and rear boundaries are to be retained as existing. It is proposed to infill the gap in the existing fence on western boundary, to match existing, 1.8m high.

**E1 – Preservation of Trees or Bushland Vegetation**

There is no native or bushland vegetation above 5m, or that requires a vegetation clearing permit under the SEPP Vegetation in non-rural areas (2017) proposed to be removed onsite – all trees on site are proposed to be retained.

A common lilly pilli hedge to the rear boundary is proposed to be removed to improve the access from the main living areas to the main outdoor open space, which is currently obstructed by the close proximity of the hedge to the existing deck, making access difficult. A timber privacy screen in place of the hedge is proposed to mitigate any potential impacts to the neighbours. The removal of this common species will not pose any detrimental impact to the landscaping and will improve amenity by widening the access path and further connecting the main living areas with the main outdoor space, and should therefore be deemed acceptable under this control.

Additionally, it is proposed that two bottle brush trees in council's nature strip to the front of the property be removed due to their proximity to the new extended driveway, and the danger that the structural root zone may impact the new driveway/vehicle crossover surface. Both trees are proposed to be removed as they are very close together, so the removal of the tree closest to the driveway is likely to cause damage to the roots of the second tree. It is proposed that two new trees are planted in the nature strip, away from the driveway, so that there is no environmental loss to the nature strip and surrounding area, and because of this, it should be deemed acceptable to remove the trees. Refer to accompanying landscape plans for further detail.

**E6 – Retaining unique Environmental Features**

There are no distinctive features on the site that are proposed to be removed. Predominant existing site levels are retained as existing, and existing trees to the front setback are also retained.

**E10 – Landslip Risk**

The site falls under Zone B of the Landslip Risk Area map, and as such requires that a preliminary geotechnical assessment be submitted as part of the DA. This document accompanies the DA and concludes that proposed works will not adversely affect the geotechnical stability of the site, and therefore a full geotechnical report is not required.

## 7. CONCLUSION

This report has addressed the requirements of all relevant planning documents in relation to the proposed alterations and additions to 19 Mildred Avenue, Manly Vale.

The development is permissible within the LEP zoning of the Warringah Local Environment Plan, 2011 and satisfies the objectives of that zone as well as other LEP requirements.

Additionally, as demonstrated, the proposed alterations and additions mostly comply with the objectives of the Warringah Development Control Plan 2011, including the numeric controls with regard to building height, and landscaping. The provision and location of the proposed carport has been justified as an appropriate deviation from the DCP controls, and as a result there are no adverse impacts upon the public domain or neighbouring properties, with adequate access to sunlight, privacy and views being maintained.

The proposed alterations and additions are considered to be a sympathetic and appropriate response within the context, with positive outcomes for the amenity of the dwelling and no adverse affects to the community.

Based on these considerations, the proposed development is considered to be appropriate for the site and worthy of approval.

If you require any additional information, or clarification on any of the above matters, please contact me on 02 9662 3800.

Regards,

A handwritten signature in dark ink, appearing to read 'K Ball', with a long horizontal flourish extending to the right.

Katherine Ball