

Stephen Gouge
Ethos Urban
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13 June 2024

Re: Navigation assessment for Marina redevelopment comprising of small jetties, Gangways and floating pontoons extending off existing seawall including reduction of berths from 43 commercial to 9 residential at 122-128 Crescent Road Newport

Dear Stephen Gouge

I refer to your email to Transport for NSW Maritime dated 2 May 2024 regarding assessment of the above proposal.

We advise that an inspection/assessment has been conducted by the local Boating Safety Officer and there are no navigational concerns regarding the designated proposal.

Sincerely,



Drew Jones
Manager Waterways Operations
Hawkesbury River Broken Bay
Transport for NSW
Greater Sydney Maritime

OFFICIAL

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Development Director
Essex Develop
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| | | | |
|-----------------|----------------------------|---------------|-----------------------|
| Date: | 31 May 2024 | Contact name: | Matt Potter |
| Your reference: | tbc | Telephone: | 0406 931 783 |
| Our reference: | PA3775-RHD-XX-XX-CO-X-0003 | Email: | matt.potter@rhdhv.com |
| Classification: | Project related | | |
| Enclosures | | | |

RE: SIRSI MARINA, NEWPORT - NAVIGATION IMPACT ASSESSMENT ADVICE

Dear Marco,

Further to recent correspondence and our site inspection on 2 March 2023, we provide the following navigation impact assessment advice in relation to the proposed berth upgrades to the Sirsi Marina at 122 Crescent Road, Newport. It is understood that Essex Develop require advice on navigation impact matters to support their Development Application (DA) for the proposed marina upgrade.

Initial consultation was undertaken with Crown Lands and TfNSW-Maritime to obtain feedback on the original marina layout provided in **Attachment A**. Following discussions with TfNSW-Maritime on 11th April 2024, amendments were made to the original marina layout in response to feedback including the reduction of the proposed maximum vessel size and minimisation of the marina footprint. The proposed marina layout that is the subject of the assessment presented herein is provided in **Attachment B**.

1 Existing marina

The existing marina (refer **Figure 1-1**) comprises three main arms consisting of gangway access to floating pontoons. The marina caters for a range of vessel sizes from 4m to 22m length and vessel types including small dinghies, runabouts, motor yachts and sailing yachts. Based on review of aerial photographs, the marina has berthed up to a total of 35-40 individual vessels (excluding dinghy storage available on top of nearshore pontoons). The marina also has a boat launching ramp and a dock for lifting vessels onto the hardstand area for repair and maintenance and a sheltered workshop building.



Figure 1-1: Existing Sirsi Marina site (Source: NSW Government SIX Maps)

2 Proposed marina upgrade

The proposed marina upgrade is understood to be associated with the proposed redevelopment of the onshore area with the proposed marina berths to be offered to occupants of luxury residential apartments.

An overlay of the proposed marina layout¹ on an aerial photograph (dated 21 August 2022) is provided in **Attachment C**. A total of 9 marina berths are proposed and are orientated shore-normal in berthing pens accessed by gangways and floating pontoon walkways extending along the foreshore from the southern property boundary to the roadhead at the end of The Avenue. Vessels are proposed to be berthed in a 'bow in' (i.e. bow facing the shoreline) arrangement within each berth.

The proposed design vessels for the upgraded marina range from 15m to 22m in length, and comprise motor yachts. It is understood that berthing of deep keel sailing yachts is not proposed. It is considered that berthed vessels would likely be sourced from motor yacht manufacturers such as Sunseeker, Princess, Fairline and Riviera. RHDHV has reviewed the available vessel specifications from

¹ Prepared by Boxall Surveyors and supplied to RHDHV in CAD format.

manufacturer websites. This information is summarised for each design vessel size along with guidance within AS3962:2020 *Marina Design* in **Table 2-1**, **Table 2-2**, **Table 2-3**, and **Table 2-4**.

Table 2-1: 15m Vessel Specifications

| Brand | Model | Length (m) | Beam (m) | Draught (m) | Displacement (kg) | Comment |
|-------------|-----------------------|------------|----------|-------------|-------------------|-------------------------------------|
| Princess | F50 | 15.14 | 4.3 | 1.39 | 22,893 | |
| Princess | V50 | 15.35 | 4.08 | 1.09 | 19,530 | |
| Fairline | Targa 50 GT | 15.44 | 4.32 | 1.18 | 13,500 | (dry weight) |
| Fairline | Squadron 50 | 15.44 | 4.32 | 1.17 | 13,500 | (dry weight) |
| Riviera | 465 SUV | 14.3 | 4.76 | 1.33 | 21,925 | (dry weight) |
| Riviera | 50 Sports Motor Yacht | 15.43 | 4.96 | 1.45 | 25,500 | (dry weight) |
| AS3962:2020 | 15m monohull vessel | 15 | 4.9 | 1.2 | - | average draught |
| AS3962:2020 | 15m monohull vessel | 15 | 4.9 | 1.5 | - | 95 th percentile draught |

Table 2-2: 18m Vessel Specifications

| Brand | Model | Length (m) | Beam (m) | Draught (m) | Displacement (kg) | Comment |
|-------------|-----------------------|------------|----------|-------------|-------------------|-------------------------------------|
| Sunseeker | Predator 60 Evo | 18.24 | 4.70 | 1.30 | 27,900 | |
| Sunseeker | Manhattan 55 | 17.21 | 4.87 | 1.37 | 27,000 | |
| Princess | F55 | 17.68 | 4.87 | 1.41 | 30,924 | |
| Princess | V55 | 17.81 | 4.66 | 1.40 | 31,194 | (half load) |
| Fairline | Squadron 58 | 17.98 | 4.99 | 1.52 | 27,000 | (dry weight) |
| Riviera | 57 Enclosed Flybridge | 17.20 | 5.13 | 1.65 | 29,985 | (dry weight) |
| Riviera | 6000 Sport Yacht | 17.60 | 5.38 | 1.499 | 26,964 | (dry weight) |
| Riviera | 575 SUV | 17.20 | 5.13 | 1.57 | 27,100 | (dry weight) |
| AS3962:2020 | 18m monohull vessel | 18.00 | 5.50 | 1.32 | - | average draught |
| AS3962:2020 | 18m monohull vessel | 18.00 | 5.50 | 1.68 | - | 95 th percentile draught |

Table 2-3: 20m Vessel Specifications

| Brand | Model | Length (m) | Beam (m) | Draught (m) | Displacement (kg) | Comment |
|-------------|-----------------------|------------|----------|-------------|-------------------|-------------------------------------|
| Sunseeker | Predator 65 | 20.50 | 5.10 | 1.60 | 37,810 | |
| Sunseeker | Manhattan 68 | 21.21 | 5.26 | 1.60 | 37,400 | |
| Princess | F65 | 20.30 | 5.10 | 1.63 | 40,177 | (lightship) |
| Princess | S66 | 20.32 | 5.08 | 1.47 | 30,500 | (lightship) |
| Princess | S62 | 19.17 | 4.87 | 1.46 | 27,200 | (lightship) |
| Princess | V65 | 19.34 | 5.08 | 1.47 | 36,073 | (half load) |
| Princess | V60 | 19.17 | 4.87 | 1.50 | 29,182 | (half load) |
| Fairline | 65 GT | 19.96 | 5.23 | 1.47 | 34,000 | (dry weight) |
| Fairline | Phantom 65 | 19.96 | 5.23 | 1.58 | 38,300 | (dry weight) |
| Fairline | Squadron 68 | 21.12 | 5.23 | 1.57 | 43,000 | (dry weight) |
| Riviera | 64 Sports Motor Yacht | 19.41 | 5.80 | 1.68 | 42,980 | (dry weight) |
| Riviera | 645 SUV | 19.41 | 5.80 | 1.62 | 39,970 | (dry weight) |
| Riviera | Belize 66 Daybridge | 19.42 | 5.40 | 1.64 | 38,515 | (dry weight) |
| Riviera | Belize 66 Sedan | 19.42 | 5.40 | 1.60 | 35,015 | (dry weight) |
| AS3962:2020 | 20m monohull vessel | 20.00 | 5.90 | 1.40 | - | average draught |
| AS3962:2020 | 20m monohull vessel | 20.00 | 5.90 | 1.80 | - | 95 th percentile draught |

Table 2-4: 25m Vessel Specifications

| Brand | Model | Length (m) | Beam (m) | Draught (m) | Displacement (kg) | Comment |
|-------------|---------------------|------------|----------|-------------|-------------------|-------------------------------------|
| Sunseeker | Predator 75 | 23.06 | 5.38 | 1.87 | 46,900 | |
| Sunseeker | 76 Yacht | 23.60 | 5.95 | 1.70 | 53,700 | |
| Princess | X80 | 25.17 | 6.07 | 1.77 | 72,813 | (half load) |
| Princess | S78 | 24.66 | 5.76 | 1.77 | 49,500 | (lightship) |
| Princess | V78 | 24.48 | 5.76 | 1.62 | 53,605 | (half load) |
| AS3962:2020 | 25m monohull vessel | 25.00 | 7.00 | 1.60 | - | average draught |
| AS3962:2020 | 25m monohull vessel | 25.00 | 7.00 | 2.00 | - | 95 th percentile draught |

3 Berth geometry

A review of the proposed berth geometry has been undertaken with respect to the guidance provided within AS3962:2020 for the various vessel sizes nominated at each berth. The proposed berths have been allocated to lots within the proposed development as indicated on **Figure 3-1**. The results of the berth geometry assessment are summarised in **Table 3-1** and confirm that the proposed width of the berths is the same or wider than guidance provided in AS3962:2020. Note that the berth for Lot 5 is a double berth, accommodating two vessels.

Table 3-1: Berth geometry assessment

| Berth No. | Vessel Size | Proposed Width (m) | AS3962:2020 Width (m) |
|-----------|-------------|--------------------|-----------------------|
| LOT 5 | 15m | 15.9 | 10.8* |
| LOT 3 | 18m | 7.1 | 6.5 |
| LOT 6 | 20m | 7.1 | 6.9 |
| LOT 2 | 22m | 8.1 | 7.8 |
| LOT 7 | 20m | 7.2 | 6.9 |
| LOT 1 | 22m | 8.1 | 7.8 |
| LOT 4 | 22m | 8.1 | 7.8 |
| LOT 8 | 20m | 7.3 | 6.9 |

*Double berth

AS3962:2020 provides guidance on the clear width of marina walkways and fingers which, for a marina of this small size (9 berths), comprise a primary and secondary clear walkway width of 1.8m and 1.5m respectively for accessible marina berths and a finger width of 0.9m. A 2m wide primary walkway pontoon is provided along the landward side of the marina, and 1.5m wide secondary walkways are also provided to the northernmost and southernmost berths (i.e. LOT 8 and LOT 5 berths). The internal pontoons would be classed as fingers and comprise 1.5m wide pontoons. As such, with respect to walkway and finger widths, the marina is considered to be compliant with AS3962:2020.

A clear gangway ramp width of 1.2m is recommended in AS3962:2020 for accessible marina berths. A compliant gangway ramp width of 1.5m is provided within the proposed marina layout to provide access to the 2m wide walkway pontoons from the shoreline or fixed jetty structures. A compliant gangway ramp width of 1.4m is proposed for access from the shoreline to the 1.5m wide pontoons at the LOT 8 and LOT 5 berths.

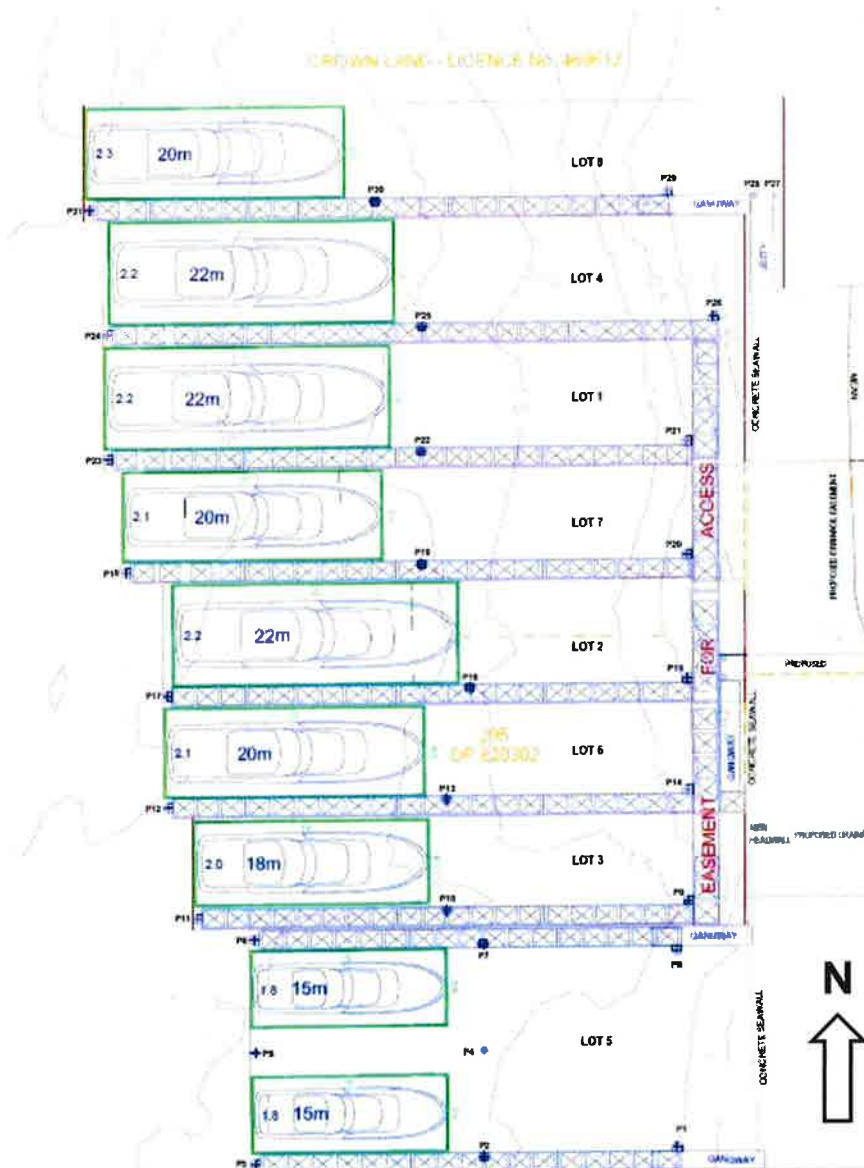


Figure 3-1: Proposed Sirsi Marina berths

4 Available water depths

AS3962:2020 *Marina Design* nominates the following components to be added to the vessel draught for calculation of the required navigable water depth for entrance channels, internal channels/fairways, and berths:

- a minimum of half the significant wave height for vessel movements resulting from wind-generated waves and vessel wake;
- an appropriate allowance where siltation is likely to occur or where it is preferred to reduce the frequency of maintenance dredging;
- a minimum under-keel clearance of 300mm or 10 percent of the vessel draught, whichever is the greater, where the base of the dredged channel consists of soft material; or,

- a minimum under-keel clearance of 500mm, where the base of the dredged channel consists of hard material such as stiff clay, gravel, or rock.

According to the TfNSW-Maritime boating map (9a Pittwater), Sirsi Marina is located within a 4 knot speed zone which would generally eliminate the generation of any significant vessel wake. However, the marina site may still be exposed to a relatively narrow wind fetch to the north-northwest over a distance of 850m to the Royal Prince Alfred Yacht Club marina. In accordance with the wind wave hindcasting procedures in the Coastal Engineering Manual (USACE, 2008) this fetch could generate a wind wave with significant wave height (H_s) = 0.4m and peak wave period (T_p) = 1.6 seconds in a 50 year average recurrence interval (ARI) wind event. It is noted that only the exterior berth LOT 8 (refer **Figure 3-1**) would be exposed to this wave condition, and the other berths would be sheltered by the marina pontoons such that the allowance required for wave action at these other berths would be negligible.

We are not aware of any significant siltation issues in Winji Jimmi Bay, which is a relatively sheltered embayment that is not expected to be subject to dynamic sediment transport or significant infilling.

Previous studies² and anecdotal evidence indicates that soft bottom conditions exist and that an under-keel clearance of 0.3m is therefore appropriate.

A summary of the water depth assessment at each berth is provided in **Table 4-1**, and is based on a hydrographic survey undertaken by Boxall Surveyors on 24 March 2023. Bed levels are provided relative to Chart Datum (CD), which represents the water level of the lowest astronomical tide. Chart Datum is the relevant tidal plane for assessment of required depth in berths, as vessels would be berthed at all states of the tide and should not touch bottom at low tide.

Table 4-1 indicates that the range of available water depths in each berth is generally consistent with guidance in AS3962:2020. It is noted that the proposed 'bow in' berthing arrangement has been considered by determining the minimum bed level at a position seaward of the vessel bow to account for the vessel bow profile. Review of bow profiles for motor yachts of 20-25m length indicates that the bow angle ranges from 40 to 50 degrees and vessel freeboard³ is typically greater than 2m. It was considered that an allowance of 4m was reasonable to account for the horizontal distance between the bow and maximum draught of a nominal 20m marina vessel. This horizontal distance offset (proportioned for other vessel lengths) was applied in the positioning of vessels in the proposed marina berths, to demonstrate the benefits of the 'bow in' berthing arrangement in minimising the marina footprint. It is also noted that the 95th percentile draughts adopted from AS3962:2020 are generally deeper than the draughts collated from the review of motor yacht vessel specifications (refer **Table 2-1**, **Table 2-2**, **Table 2-3**, and **Table 2-4**).

² Mapping completed in "Estuarine habitat mapping and geomorphic characterisation of the lower Hawkesbury river and Pittwater estuaries" (Industry & Investment NSW, 2010) indicated that subtidal rocky reef does not exist at the marina site.

³ Height of exposed deck edge at the bow above the waterline.

Table 4-1: Berth water depth assessment

| Berth No. | Vessel Size | Min. Bed Level* (m below CD) | Max. Bed Level (m below CD) | Design Vessel Draught** (m) | Under-keel Clearance (m) | Wave Action Allowance [^] (m) | Required Bed Level (m below CD) |
|------------------|-------------|---------------------------------|--------------------------------|-----------------------------------|--------------------------------|--|---------------------------------------|
| LOT 5 (south) | 15m | 1.8 | 1.9 | 1.5 | 0.3 | - | 1.8 |
| LOT 5 (north) | 15m | 1.8 | 2.0 | 1.5 | 0.3 | - | 1.8 |
| LOT 3 | 18m | 2.0 | 2.3 | 1.68 | 0.3 | - | 2.0 |
| LOT 6 | 20m | 2.1 | 2.5 | 1.8 | 0.3 | - | 2.1 |
| LOT 2 | 22m | 2.3 | 2.5 | 1.88 | 0.3 | - | 2.2 |
| LOT 7 | 20m | 2.1 | 2.8 | 1.8 | 0.3 | - | 2.1 |
| LOT 1 | 22m | 2.2 | 2.7 | 1.88 | 0.3 | - | 2.2 |
| LOT 4 | 22m | 2.2 | 2.7 | 1.88 | 0.3 | - | 2.2 |
| LOT 8 | 20m | 2.3 | 3.0 | 1.8 | 0.3 | 0.2 | 2.3 |

* Determined at the estimated position of maximum vessel draught, located seaward of the bow of vessels that are to be berthed in a 'bow in' arrangement

** Based on AS3962:2020 95th percentile draught

[^] Based on 50 year ARI wave condition, applicable to the exterior berth (LOT 8) only

5 Wave Climate

As noted above, the 50 year ARI wave condition for the north north-westerly wind fetch comprises a significant wave height (H_s) = 0.4m and peak wave period (T_p) = 1.6 seconds. The corresponding 1 year ARI wave condition comprises a significant wave height (H_s) = 0.3m and peak wave period (T_p) = 1.5 seconds. These wave conditions represent a 'good' wave climate for a marina, and hence complies with guidance in AS3962:2020.

6 Navigation Access

6.1 Private jetty access

To the north of the proposed marina upgrade, a private jetty exists on the water frontage of 50 The Avenue, Newport. This private berth comprises a fixed jetty providing access from the foreshore to a small pontoon and mooring piles that provide a midship tie-up point on either side of an approximately 12m (40 foot) long sailing yacht. This vessel is berthed at a perpendicular orientation to the private jetty.



Figure 6-1: View of private jetty and sailing yacht berthed at 50 The Avenue, Newport

According to guidance within AS3962:2020, vessels accessing berths via a fairway would require a minimum fairway width of 1.5 times the vessel length (1.5L) for manoeuvring into a berth. The distance between the stern of the berthed sailing yacht and the berthing position of the 20m vessel on the northernmost pontoon (LOT 8) of the proposed marina upgrade is approximately 20m (refer **Figure 6-2**). The distance between the stern of the berthed sailing yacht and the face of the northernmost pontoon is approximately 22m (refer **Figure 6-2**). This navigation access width is considered adequate for the 12m sailing yacht (1.5L=18m).

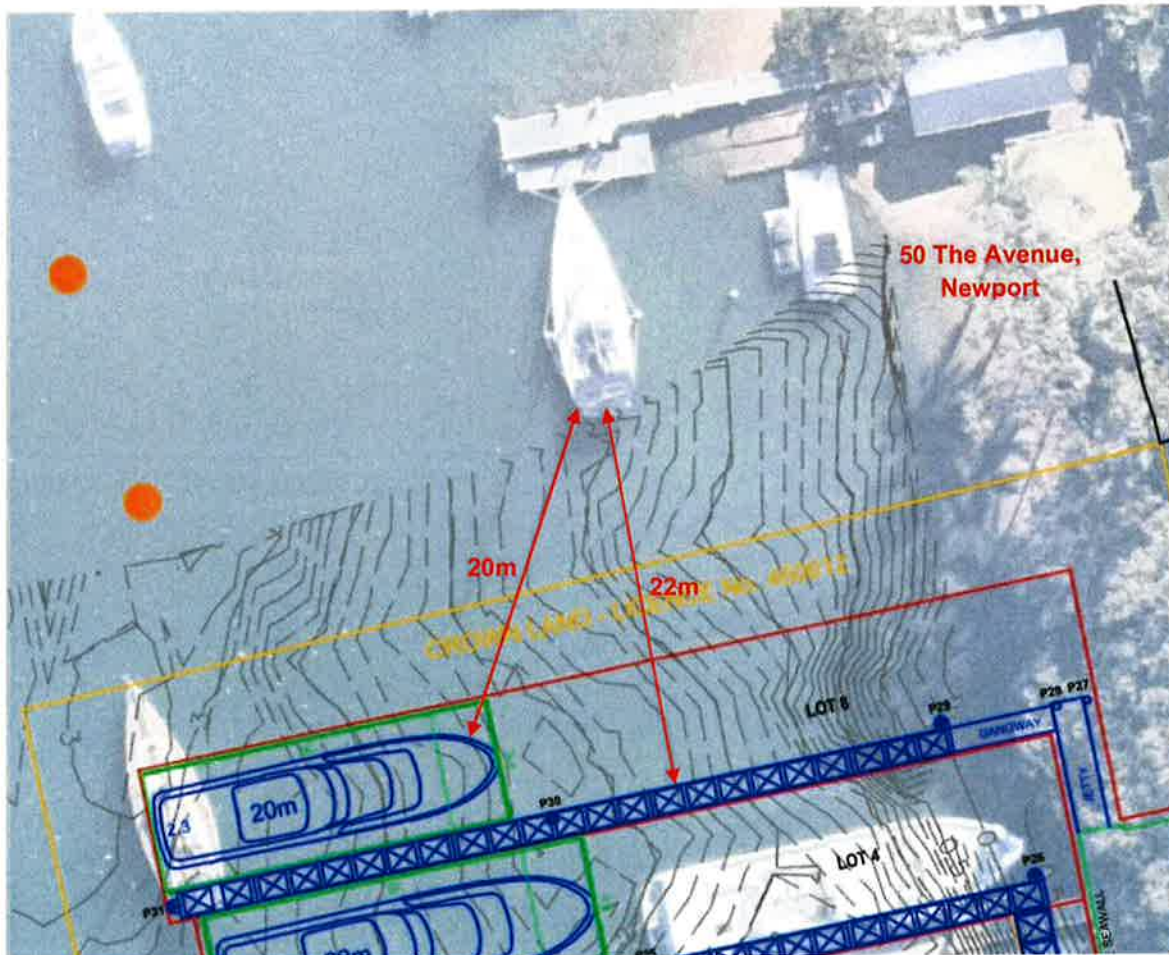


Figure 6-2: Navigation access distances for existing private jetty at 50 The Avenue, Newport

To the south of the proposed marina upgrade, the southernmost pontoon extends within an extension of the joint property boundary with 120 Crescent Road, Newport (refer **Figure 6-3**). As such, the existing level of navigation access to the private berthing structure would be maintained. The navigation access is likely to be improved given that several of the existing marina berths are currently aligned along the property boundary extension and future berthing on the southern side of the proposed marina pontoon would not occur.

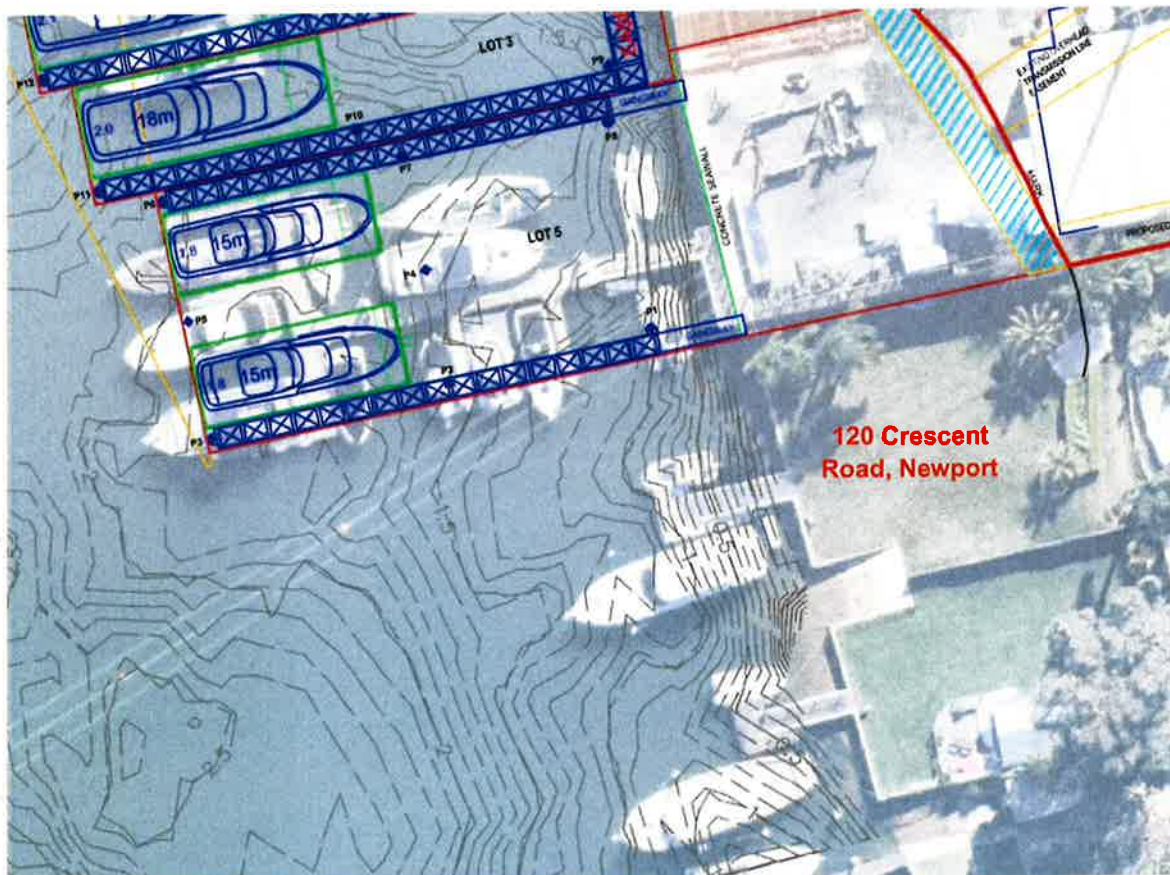


Figure 6-3: Navigation access for existing private berthing structure at 120 Crescent Road, Newport

6.2 Marina access

As noted above, the navigation access width along the marina approach adjacent to the proposed berths would also need to be a minimum of approximately 1.5 times the length (1.5L) of the design vessel nominated for each berth.

Review of the aerial photo overlay of the proposed marina layout (refer **Attachment C** and **Figure 6-4**) indicates that the southern half of the proposed marina (i.e. LOT 5 to LOT 2 berths) has a similar footprint within the waterway to the existing marina. The available navigation access width from the proposed marina pontoons to opposing private jetties/pontoons or vessels is approximately 34-35m, which provides adequate width for access by the proposed vessels of up to 22m length (1.5L = 33m).

Further north of the LOT 2 berth, the available navigation width increases from 34-35m to 45-48m (refer **Figure 6-4**), which provides adequate width for access by the proposed vessels of 20m length (1.5L=30m) and 22m length (1.5L=33m).

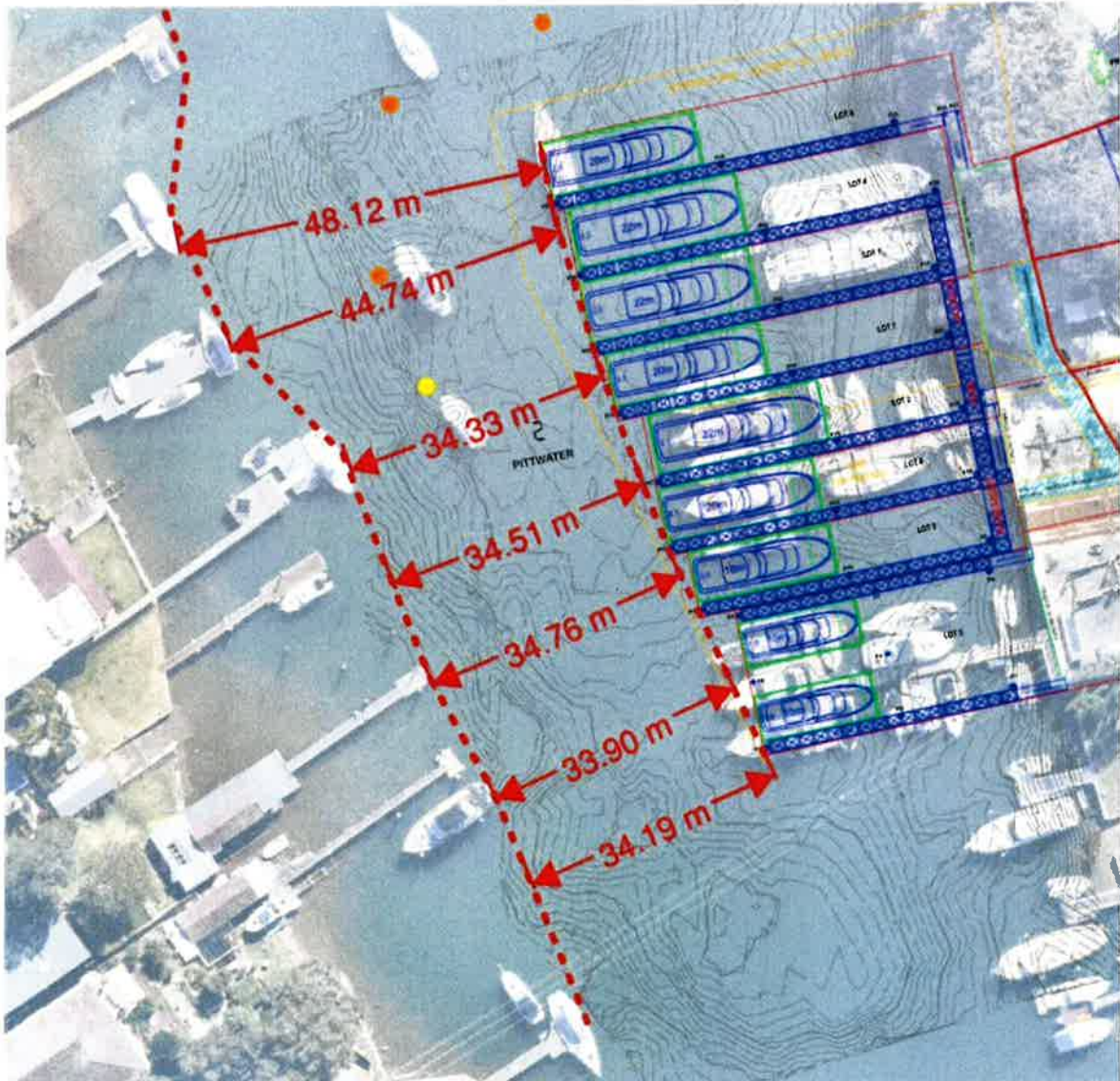


Figure 6-4: Navigation access width along proposed marina approach

6.3 Swing moorings

There are a number of existing private and commercial swing moorings located within Winji Jimmi Bay. This field of swing moorings extends from the Newport Anchorage Marina, into the entry to Old Mangrove Bay and Winji Jimmi Bay, and to the immediate north and west of the proposed marina upgrade footprint (refer **Figure 6-5**). Most of these swing moorings are commercial moorings, with a small number of private moorings.



Figure 6-5: Swing moorings in locality of Sirsi Marina (orange = commercial mooring, yellow = private mooring)

Vessels navigating into Winji Jimmi Bay are required to comply with the 4 knot speed limit and to transit through the swing mooring field with spaces between moored vessels typically being 25-30m. It is

anticipated that navigation into Winji Jimmi Bay would continue in a similar manner, with vessels carefully making their way through the swing mooring field at low speed to access the marina berths. It is noted that the existing marina has catered for a vessel size of up to 22m⁴, which is equivalent to the maximum vessel size for the proposed upgraded marina. Given the reduction in number of berths from 36 to 9 berths and the cessation of the previous boat maintenance and repair business, the frequency of boat movements to the proposed upgraded marina servicing residents of the onshore development would be expected to be reduced in comparison to the existing situation.

There are several swing moorings that are positioned in close proximity to the proposed marina (refer **Attachment C**) and will need further investigation to establish their exact position, occupation status, and any potential impacts on marina access. It is noted that the proponent currently holds licences for ten (10) swing moorings in the vicinity of the proposed marina (refer **Figure 6-6**), and that these moorings are currently vacated and not occupied by vessels (refer **Figure 6-7**). It is recommended that consultation with Crown Lands and TfNSW-Maritime is undertaken to:

- determine an accurate location for each swing mooring (mooring block survey);
- establish the configuration and status of each swing mooring (vessel type/specifications, mooring line length/swing radius, active/inactive occupation, mooring holder details); and,
- investigate options for management of the existing swing moorings.



Figure 6-6: Swing mooring licences held by proponent in the vicinity of the proposed marina

⁴ This maximum size vessel comprised a motor yacht cruiser with a length of 72 feet (22m), beam of 5.6m, and draught of 1.3m.



Figure 6-7: Aerial photograph of waterway area in vicinity of marina site (source: Nearmap, dated 12 March 2024)

7 Overwater Lease Boundary

The proposed overwater lease boundary is shown on the aerial photo overlay of the proposed marina layout (refer **Attachment C**, red line indicates proposed overwater lease boundary).

The size of the proposed lease area footprint in the alongshore direction is dictated by the berth width requirements of AS3962:2020 (refer **Section 3**) that are necessary to accommodate the vessel sizes nominated at each of the 9 berths. The size of the lease area footprint in the offshore direction is dictated by the water depths in the nearshore area that require vessels to be berthed away from the shoreline. As noted previously, the footprint of the marina has been minimised by nominating a 'bow in' berthing arrangement to enable vessels to be positioned closer to shore.

The alignment of the offshore lease boundary (western lease boundary) generally follows the proposed extent of pontoons and berthed vessels, whilst still providing adequate waterway width (clearance) to the existing jetties and berths along the opposite shoreline for access of the nominated vessel sizes into the proposed marina berths (refer **Section 6.2**).

The alignment of the southern lease boundary follows the alignment of the adjacent onshore private property boundary, which is approximately shore normal (perpendicular to the shoreline) at this location.

The alignment of the northern lease boundary follows the outside perimeter of the northernmost berth proposed for a 20m length vessel, and is approximately shore normal. At this location the onshore cadastral boundaries adjacent to The Avenue are at an angle to the shoreline that is not shore normal. As such, projection of these cadastral boundaries overwater would be contrary to the typical practice of constructing foreshore structures (jetties and wharfs) perpendicular to the shoreline, as has been adopted for the layout of the proposed marina upgrade and for the existing adjacent marine structures in Winji Jimmi Bay. As noted in **Section 6.1**, the alignment of the northern lease boundary provides adequate waterway width to the north for access by the existing sailing yacht to the private jetty on the water frontage of 50 The Avenue, Newport.

8 Concluding Remarks

This assessment of navigation impacts in relation to the proposed berth upgrades to the Sirsi Marina at 122 Crescent Road, Newport indicates that the proposed marina is generally compliant with guidance provided in *AS3962:2020 Marina Design*.

The extent and scale of the upgraded marina has been minimised by selection of design vessels that are appropriate for the available water depths and comparable to the maximum size of vessels that were berthed at the existing marina. A 'bow in' berthing arrangement has been nominated to enable vessels to be positioned closer to shore along the floating marina pontoons. This berthing arrangement will be mandated in the documentation of Marina Rules and Berthing Agreements (or similar), and will also be clearly indicated with signage⁵ positioned along each berthing pontoon at the location where available water depths reduce to less than that required for safe all-tide mooring of the vessels allocated to each berth.

The management of existing swing moorings in the vicinity of the marina will need to be confirmed in consultation with Crown Lands and TfNSW-Maritime representatives.

We trust that the above advice is suitable for your current needs. It is understood that this assessment will form part of initial discussions ahead of formal lodgement of a DA with Council and referral to relevant government agencies as part of that process.

Please do not hesitate to contact the undersigned if you have any questions or clarifications.

Yours Sincerely,



Matt Potter

Principal Coastal Engineer
Water & Maritime

⁵ Indicating the berth number, maximum vessel draught, and warning of shallow water depths closer to shore. Signage shall have high visibility for berthing vessels and marina users with 50mm high letters in contrasting colours.

Attachment A: Original Marina Layout Plan

No 50
TWO STOREY
BRICK & GLASS
RESIDENCE

CROWN LAND - LICENCE No. 460612

THE AVENUE

PROPOSED VISTA POINT

LOT 1

LOT 9

LOT 2

LOT 8

LOT 3

LOT 7

LOT 6

LOT 5

LOT 4

22
DP 553811

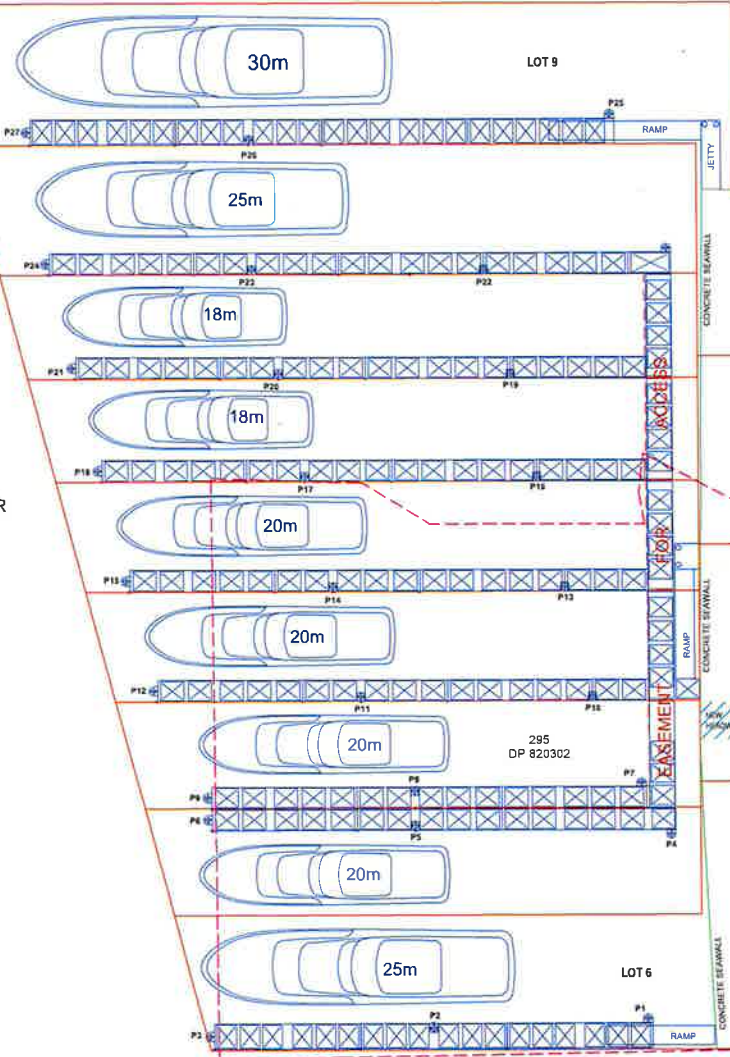
No 120
1&2 STOREY
RENDERED RESIDENCE

No 119
1&2 STOREY
RENDERED RESIDENCE

21
DP 553811

MGA

2
PITTWATER



ESSEX DEVELOPMENTS
122-128 CRESCENT ROAD
NEWPORT
NSW 2107
TEL: (02) 9611 1111
WWW.ESSEXDEVELOPMENTS.COM.AU

PRINT IN COLOUR

LOT AND DP
LOT 18 36.9M DP 218340
LOT 19 30.0M DP 242200
LOT 21 30.0M DP 461338
LOT 22 30.0M DP 553811
LOT 23 30.0M DP 422300

SUBJECT
ESSEX DEVELOP
ADDRESS
122-128 CRESCENT ROAD
NEWPORT

boxall

TITLE
PLAN OF PROPOSED MARINA
SUBDIVISION
OPTION 3

DATE
18/04/2023

PROPOSED LIND SUBMISSION ADDRESS
122-128 CRESCENT ROAD NEWPORT NSW 2107

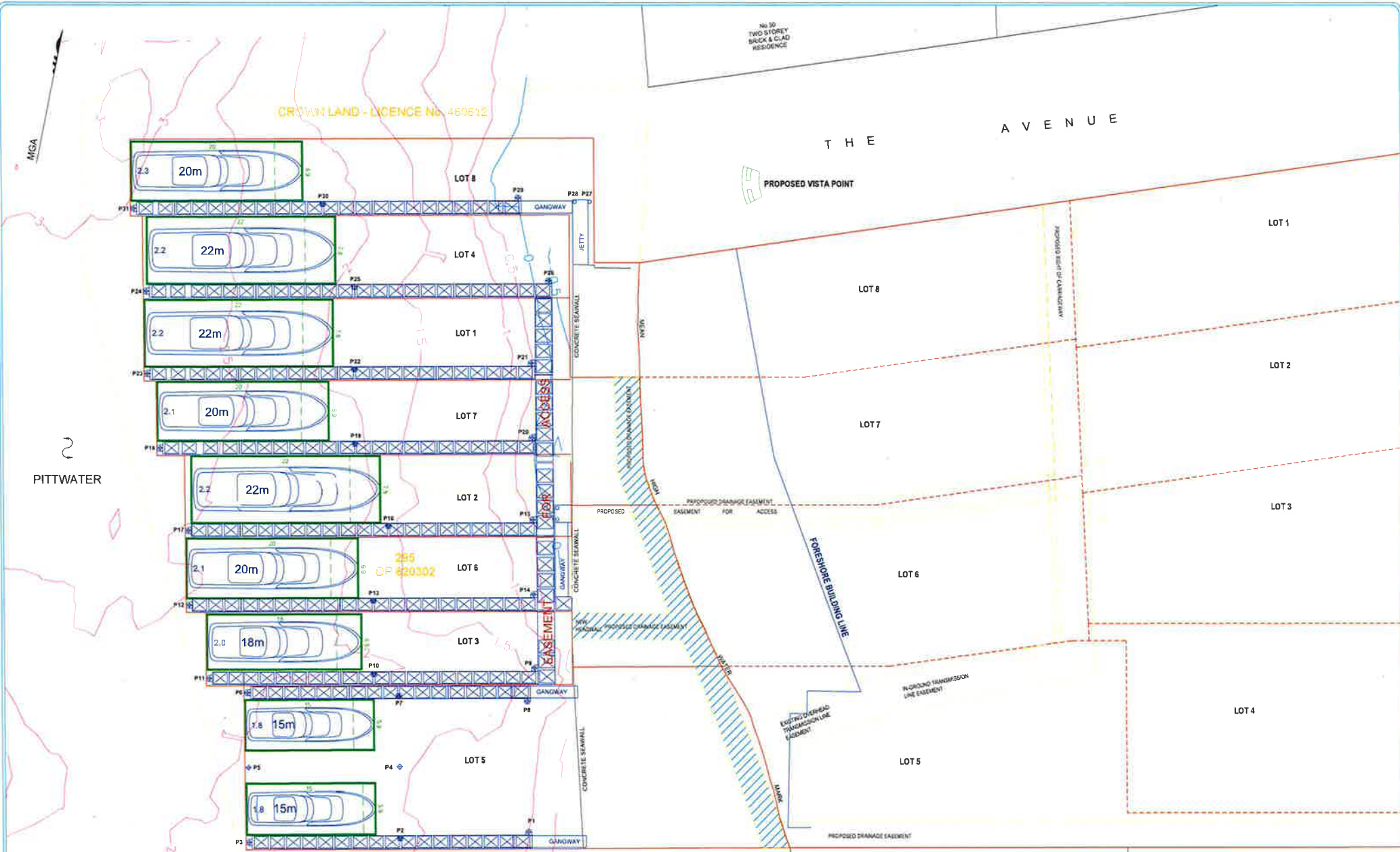
LOCAL GOVERNMENT
NEWCASTLE LOCAL GOVERNMENT

SCALE
1:500

DATE
18/04/2023

REV
A

Attachment B: Proposed Marina Layout Plan



CROWN LAND - LICENCE No. 460612

No 30
TWO STOREY
BRICK & CLAD
RESIDENCE

THE AVENUE

PROPOSED VISTA POINT

LOT 1

LOT 8

LOT 2

LOT 7

LOT 3

LOT 6

LOT 4

LOT 5

MCA

PITTWATER

- NOTES**
1. Contour interval generated using sounding dataset at 0.5m intervals.
 2. The site has been inspected at the scale shown for the proposed moorings and should be representative of that shown when available data.
 3. Due to environmental factors, safety and concerns are noted at the time of survey only.
 4. This plan should not be relied upon for any other purpose or used by any party outside of the project team, as the plan may not contain sufficient information for that purpose of use.
 5. Proposed pit locations and number are indicative only. Final pit specifications by approved engineering plans.
 6. Berths are bow to bow.

22
DP 553811

No.120
1&2 STOREY
RENDERED RESIDENCE

No.118
1&2 STOREY
RENDERED RESIDENCE

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DP 553811

PRINT IN COLOUR

LOT 1 AND 2
LOT 1 IN DP 503390
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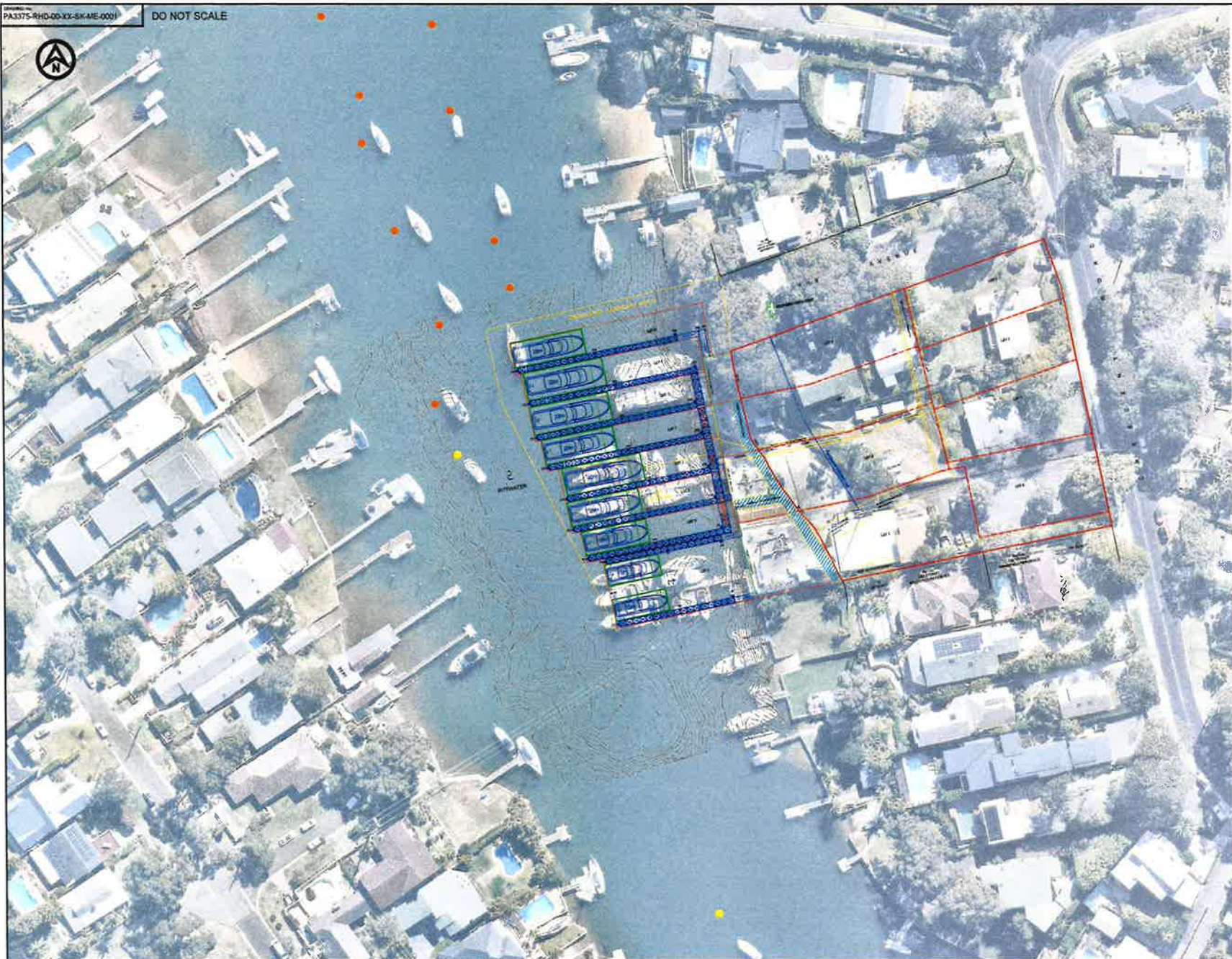
Client
ESSEX DEVELOP
Address
122-128 CRESCENT ROAD
NEWPORT



| DATE | DESCRIPTION | BY |
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PLAN OF PROPOSED MARINA
 11300-012
 11300-012

Attachment C: Aerial photo overlay of proposed marina upgrade



NOTES

1. ALL WATER DEPTHS SHOWN ARE RELATIVE TO CHART DATUM (CD).
2. AERIAL PHOTOGRAPH IS SOURCED FROM NEARMAP DATED 30 AUGUST 2023.
3. MARINA LAYOUT WAS PROVIDED TO RHDHV IN CAD FORMAT BY BOXALL SURVEYORS ON 2 MAY 2024.
4. SWING MOORING LOCATIONS WERE PROVIDED BY TFNSW SPATIAL INFORMATION FROM THEIR GIS DATABASE ON 6 MARCH 2023.

LEGEND

- COMMERCIAL MOORING
- PRIVATE MOORING
- WATER DEPTH BELOW CHART DATUM

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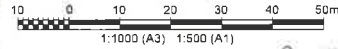
PROJECT
SIRSI MARINA
122 CRESCENT ROAD, NEWPORT

TITLE
MARINA LAYOUT

Royal HaskoningDHV
Enhancing Society Together

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NOT FOR CONSTRUCTION **S0 WORK IN PROGRESS**



Jo Ziesche

From: Craig Blount <craig.blount@dpi.nsw.gov.au>
Sent: Monday, 13 May 2024 2:37 PM
To: ocean environmental; Karthika Krishna Pillai
Cc: Marco Silva; Matt Potter; Tfnsw ExternalContact52; Stephen Gouge
Subject: RE: Request for Comment - Proposed Marina, 122 Crescent Rd, Newport
Attachments: General information requirements for environmental assessment .pdf

You don't often get email from craig.blount@dpi.nsw.gov.au. [Learn why this is important](#)

Thankyou for consulting us Katie,

We have no objection to the submission of the DA for a Proposed Marina at 122 Crescent Rd, Newport. In the event the proposal is referred to us by Council, or an agency, we have attached our general requirements for an assessment.

We note that the site is located within a shallow, narrow bay. Given the facilities proposed, we suggest that the AEA determines whether these would change hydrology or e-folding time within the bay and if so, whether this would adversely affect water quality and marine ecology within the bay.

We also note that marine vegetation is located on both sides of the head of the bay. Given that large vessels are proposed to be berthed at the facilities, we suggest that the AEA determines whether prop wash to the seabed (that could potentially constitute dredging) would occur at the berths or during ingress and egress to the bay, and if so, whether this would significantly increase impacts (from present levels) to marine vegetation (e.g. from turbidity, wash, mobilised contaminants) caused by vessel movements generally (ie would impacts from the proposal significantly contribute to cumulative impacts on marine vegetation?).

We hope this is of assistance,
Kind regards, Craig & Karthika

Craig Blount | Senior Fisheries Manager – Coastal Systems Unit
NSW Department of Primary Industries | Fisheries
66 Harrington St, The Rocks, Sydney, NSW, 2000
M: 0418 925 757 | E: craig.blount@dpi.nsw.gov.au

PERMIT APPLICATION FORMS & FISH HABITAT POLICIES AVAILABLE AT:

<https://www.dpi.nsw.gov.au/fishing/habitat/protecting-habitats/toolkit>

Submit permit applications via email to: ahp.central@dpi.nsw.gov.au

Turnaround times: from date of receipt of application, please allow up to 28 days for Land Owners Consent, Permits and Consultations. Please allow up to 40 days for Integrated Development Applications.



ABORIGINAL FISHING AND MARINE AND COASTAL ENVIRONMENTS



DPI Fisheries acknowledges that it stands on Country which always was and always will be Aboriginal land. We acknowledge the Traditional Custodians of the land and waters, and we show our respect for Elders past, present and emerging. We are committed to providing places in which Aboriginal people are included socially, culturally and economically through thoughtful and collaborative approaches to our work.

From: ocean environmental <oceanenviron@gmail.com>

Sent: Thursday, 2 May 2024 3:53 PM

To: Karthika Krishna Pillai <karthika.krishnapillai@dpi.nsw.gov.au>; Craig Blount <craig.blount@dpi.nsw.gov.au>

Cc: Marco Silva <marco@essexdevelop.com.au>; Matt Potter <matt.potter@rhdhv.com>; Mike Jarvin <mj@advancedmm.com.au>; Stephen Ethos jBA Gouge <sgouge@ethosurban.com>

Subject: Request for Comment - Proposed Marina, 122 Crescent Rd, Newport

Dear Karthika and Craig,

Following on from our discussion on 15th April, I have attached a formal letter of request for DPI Fisheries to provide comments / requirements for assessment for the proposed marina upgrade at 122 Crescent St, Newport.

It would be appreciated if confirmation of your requirements and any feedback, as well as outlining no objection to the submission of the DA, could be provided by Friday 10 May 2024 if possible.

Thanks in advance,
Katie.

Kind Regards,
Dr Katie Smythe

**Ocean Environmental
Marine Consulting Services**

e: oceanenviron@gmail.com

p: 0425325410

a: 124 Mitchell St, Merewether, NSW, 2291