



Corona Projects

Development Application
STATEMENT OF ENVIRONMENTAL EFFECTS

Remedial works for garage

81 Sydney Road, Manly

August 2020

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PROJECT DETAILS

Client: Mr John Vinci
Subject land: 81 Sydney Road, Manly
Lot Description: 1/DP81702
Proposed development: Remedial works for garage

The report is prepared by Emma Rogerson
Bachelor of Architecture and Environments (USYD)



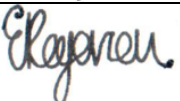
The report is reviewed by Crystal Pan
Bachelor of Design in Architecture (UTS)

Project Code: J000551

I certify that the contents of the Statement of Environmental Effects to the best of my knowledge, has been prepared as follows:

- In accordance with Section 4.12 of the Environmental Planning and Assessment Act 1979 and Clause 50 of the Environmental Planning and Assessment Regulation 2000;
- The statement contains all available information that is relevant to the environmental impact assessment of the proposed development;
- To the best of my knowledge the information contained in this report is neither false nor misleading.

Quality Management

	Name	Date	Signature
Prepared by	Emma Rogerson	10/08/2020	
Checked by	Crystal Pan	10/08/2020	
Approved for issue by	Emma Rogerson	10/8/2020	

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1.0 INTRODUCTION

This Statement of Environmental Effects has been prepared for Mr John Vinci to accompany a Development Application (DA) to Northern Beaches Council for the remedial works for garage at 81 Sydney Road, Manly.

More specifically, the proposed development comprises of the reconstruction of the western and eastern side walls of the garage. Due to structural concerns, a tree adjacent to the eastern wall will be removed as well as the paving and retaining walls next to the tree.

The proposal is permissible with consent and is suitable for the site and the area. The proposal has been designed to relate to its site and to the streetscape in terms of appearance, envelope, setbacks, bulk and scale. The proposal will operate without any significant impact to the amenity of neighbouring properties.

The purpose of this SEE is to:

- Describe the site to which the application applies and its context;
- Describe the proposed development
- Describe the legislative framework against which the application is to be assessed and determined; and
- Provide an assessment of the environmental impacts in accordance with the Section 4.15 of the EP&A Act 1979.

This Statement has been prepared in reference to the following:

Document	Author	Date
Architectural Plans	Corona Projects	July 2020
Survey	Altitude Surveys	July 2020
Structural Report	Law & Dawson Pty Ltd	October 2018

2.0 SITE ANALYSIS & CONTEXT

2.1 The Site

The site is located at 81 Sydney Road, Manly and is legally described Lot 1 in Deposited Plan DP81702. The site is located on the southern side of Sydney Road, between James Street and Tower Street.

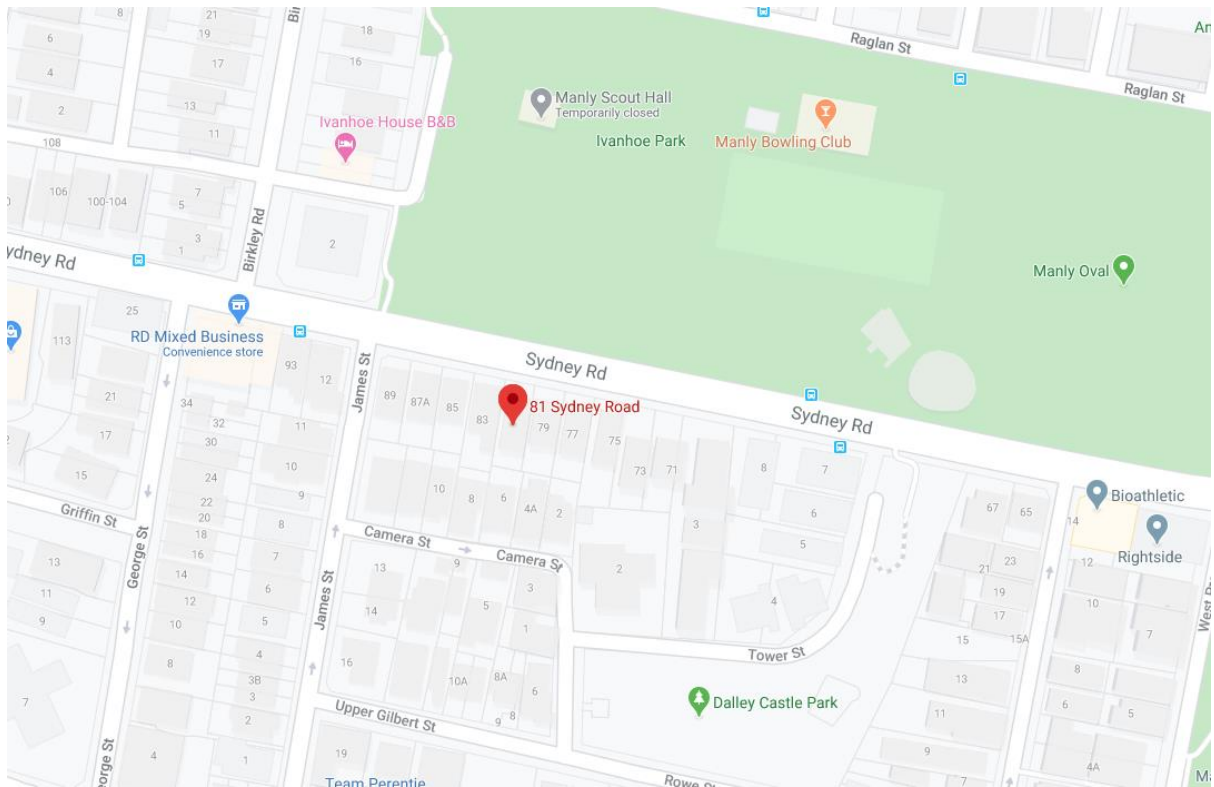


Figure 1 – Site locality map (Google Maps)



Figure 2 – Aerial map (SIX Maps)

The site is rectangular with a total area of approximately 395 square metres, with a 11.445 metre street frontage to Sydney Road. The western side boundary measures an approximate 35 metres and the eastern side boundary measures an approximate 35 metres. The rear boundary measures 11.445 metres.

The site currently contains a three-storey residential flat building with a tiled roof. The existing garage currently only services one dwelling. The rear of the site is landscaped with shrubs and grass. Vehicular access is available from Sydney Road.

The land is zoned R1 General Residential under the provisions of Manly Local Environmental Plan 2013 (MLEP2013). The site is not identified as a Heritage Item, nor is it located within a Heritage Conservation Area (HCA). It is located within the close vicinity of heritage items and is adjacent to a heritage item on 83 Sydney Road, Manly

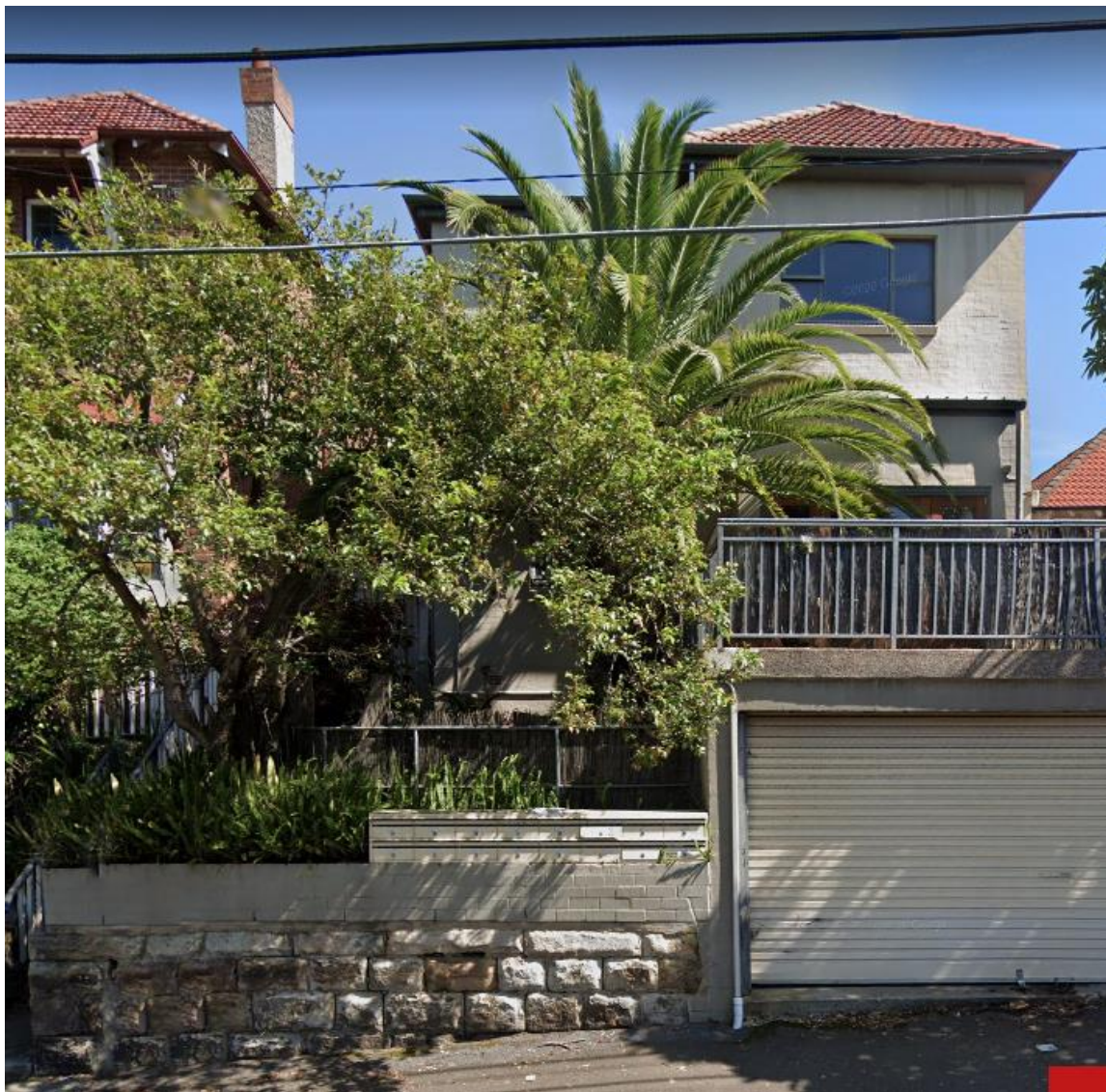


Figure 3 – Subject site as viewed from Sydney Road (Google Maps, July 2020)

2.2 The Locality

The site is located within the local residential area of Manly. The locality comprises of mostly residential development of various buildings heights.

The site adjoins a one storey dwelling with a tiled roof to the east at 83 Sydney Road. The site adjoins a two-storey brick dwelling to the west at 79 Sydney Road. The rear of the site is adjoined by a two-storey brick dwelling with a tiled roof building at 6 Camera Street.

2.4 Development History

A search on Council's DA Tracker returned the following results for development applications associated with the site.

[Construction Certificate CC 442/2004](#)

Alterations to Dwelling to facade & eaves & front bay window to doors (Submitted: 30/11/2004)

[Development Application DA 442/2004](#)

Alterations to Dwelling to facade & eaves & front bay window to doors (Submitted: 22/09/2004)

[Development Application DA 5083/1991](#)

DA5083/91 (Submitted: 26/09/1991)

3.0 THE PROPOSAL

3.1 Overview

The Development Application proposes remedial works to an existing garage. More specifically, the proposed development comprises of the eastern and western walls to be reconstructed with reinforced concrete or H blockwork. The tree and retaining walls to the east of the garage will be removed. Removal of the paving adjacent to the garage will also be removed as per recommendation from the structural engineer.

The proposal has been designed to relate to its site and to the streetscape in relation to appearance, envelope, setbacks, bulk and scale. The contemporary design utilises high quality materials and detailing to present an unobtrusive visual outcome for the locality.

Please refer to plans prepared by Corona Projects.

3.3 Numerical Overview

A brief numerical overview of the development parameters for the proposed development is included in the following table

Table 1: Key development components

Component	Proposal
Site area	Approximately 395 m ²
Boundary setbacks	
• Front	Nil
• Side	1m
• Side	4.8m
Car spaces	2

4.0 STATUTORY PLANNING FRAMEWORK AND ENVIRONMENTAL ASSESSMENT

This Chapter provides an environmental assessment in accordance with Section 4.15 of the Environmental Planning and Assessment Act 1979.

4.1 Statutory and Policy Compliance

The relevant matters for consideration under Section 4.15(a) of the EP&A Act, 1979, are identified as:

- State Environmental Planning Policy No. 55 – Remediation of Land
- Manly Local Environmental Plan 2013
- Manly Development Control Plan 2013

The primary statutory document that relates to the subject site and the proposed development is Manly Local Environmental Plan 2013. The primary non-statutory plan relating to the subject site and the proposed development is Manly Development Control Plan 2013.

4.1.1 State Environmental Planning Policy No. 55 – Remediation of Land

This Policy is to provide for a state-wide planning approach to the remediation of contaminated land. Subject to Clause 7, considerations should be given to the suitability of land in terms of contamination.

The subject site has a long history of being used for the residential purpose. Thus, there is no further consideration required under Clause 7(1)(b) and (c) of SEPP 55.

4.1.2 Manly Local Environmental Plan 2013

The development complies with the provisions of Manly Local Environmental Plan 2013 (MLEP 2013).

Zoning and permissibility

The site is located in Zone R1 General Residential.



Figure 4 – Land Zoning Map (NSW Planning Portal)

The development is identified to be *remedial works for garage*, which is permitted with consent in the R1 zone.

The objectives of the zone are:

- *To provide for the housing needs of the community.*
- *To provide for a variety of housing types and densities.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*

The proposal will allow for a structurally stable garage that will benefit the livelihood of the occupants. Through retaining the original dimensions and car parking spaces of the garage, it will allow for the amenity of the site to be maintained.

Clause 4.3 Height of buildings

The Height of Building Map stipulates that the maximum building height permitted for the site is 11 metres. The proposal will not alter the maximum height of the site.

Clause 4.4 Floor space ratio

The Floor Space Ratio Map stipulates that the maximum floor space ratio permitted for the site is 0.75:1. The proposal will not alter the FSR of the site.

Clause 5.10 Heritage Conservation

The site is not identified as a Heritage Item, nor is it located within a Heritage Conservation Area (HCA). It is located within the close vicinity of heritage items and is adjacent to a heritage item on 83 Sydney Road, Manly



Figure 5 – Heritage Map (NSW Planning Portal)

Clause 6.1 Acid sulphate soils

The Acid Sulphate Soils Map stipulates that the site is Class 5. The proposed works will not be below 5 metres Australian Height Datum, and are unlikely to lower the water table below 1 metre Australian Height Datum on adjacent Class 1,2,3 or 4 land.

Clause 6.9 Foreshore scenic protection area

The Foreshore Scenic Protection Area Map stipulates that the site is classified as a “Foreshore Scenic Protection Area”. The proposed works will not detrimentally impact the scenic qualities of the locality and will not result in any loss of views.

4.1.3 Manly Development Control Plan 2013

The development achieves a high level of compliance with the provisions of Manly Development Control Plan 2013.

Control	Comment	Compliance
Part 3 General principles of Development		
3.1 Streetscapes and Townscapes		

Control	Comment		Compliance
3.1.1 Streetscape (Residential Areas)			
3.1.1.1 Complementary Design and Visual			
a) i)	complement the predominant building form, distinct building character, building material and finishes and architectural style in the locality;	The reconstruction of the front garage complements the streetscape by utilising appropriate materials and colour schemes, and retaining the sympathetic size of the garage.	Yes
ii)	ensure the bulk and design of development does not detract from the scenic amenity of the area (see also paragraph 3.4 <i>Amenity</i>) when viewed from surrounding public and private land;	The sympathetic bulk and design of the garage ensures the streetscape is enhanced.	Yes
iii)	maintain building heights at a compatible scale with adjacent development particularly at the street frontage and building alignment, whilst also having regard to the LEP height standard and the controls of this plan concerning wall and roof height and the number of storeys;	An appropriate height is applied to the garage.	Yes
Vi)	visually improve existing streetscapes through innovative design solutions	The existing streetscape is enhanced through the sympathetic materials and colour scheme of the garage.	Yes
vii)	incorporate building materials and finishes complementing those dominant in the locality. The use of plantation and/or recycled timbers in construction and finishes is encouraged.	Appropriate building materials and finishes are applied to the garage to complement the locality visually.	Yes
3.1.1.4 Garages, Carports and Hardstand Areas			
a	Garages, carports and hardstand areas must be designed and sited in a manner that does not to dominate the street frontage by: i) its roof form, material choice and detailing by being subservient to the associated dwelling; and ii) being compatible with the streetscape and the location in relation to front setback criteria.	The flat roof will be maintained as to not detract the streetscape, with appropriate materials being utilised for the reconstruction.	Yes
3.3 Landscaping			
3.3.1 Landscaping design			
a	In low density areas: (including LEP Zones R2 Low Density, E3 Environmental Management and E4 Environmental Living) open space should dominate the site. Setbacks of buildings from open space should also be maximised to enable open space to dominate buildings,	Open space is maintained at the front of the site.	Yes

Control	Comment	Compliance
	especially when viewed to and from Sydney Harbour, the Ocean and the foreshore	

3.3.2 Preservation of trees or Bushland Vegetation

3.3.2.2 Requirements for other Das

a	Development is to be sited and designed to minimise the impact on remnant native vegetation, including canopy trees, understorey vegetation and remnant native ground cover species.	The proposed reconstruction does not impact any native vegetation.	Yes
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Part 4 Development Controls and Development Types

4.1 Residential Development Controls

4.1.6 Parking, Vehicular Access and Loading (Including Bicycle Facilities)

4.1.6.1 Parking Design and the Location of Garages, Carports or Hardstand Areas

a	The design and location of all garages, carports or hardstand areas must minimise their visual impact on the streetscape and neighbouring properties and maintain the desired character of the locality.	The design and location of the garage will be maintained to respect the existing and desired character of the locality. The appropriate materials utilised for the garage ensure to relate to the streetscape.	Yes
b	Garage and carport structures forward of the building line must be designed and sited so as not to dominate the street frontage. In particular garages and carports adjacent to the front property boundary may not be permitted if there is a reasonably alternative onsite location	The garage is appropriately designed and located to not dominate the street frontage.	Yes
c	the maximum width of any garage, carport or hardstand area is not to exceed a width equal to 50 percent of the frontage, up to a maximum width of 6.2m	The width of the garage does not exceed a width equal to 50 percent of the frontage.	Yes
d	In relation to the provision of parking for dwelling houses, Council may consider the provision of only 1 space where adherence to the requirement for 2 spaces would adversely impact on the streetscape or on any heritage significance identified on the land or in the vicinity.	The existing two car parking spaces are retained.	Yes

4.1.6.4 Vehicular Access

c	Vision of vehicles entering and leaving the site must not be impaired by structures or landscaping.	Adequate vision is allowed for vehicles to enter and exit the site.	Yes
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Schedule 3 Parking and Access

Part A1 Parking Rates and Requirements for Vehicles

Control	Comment	Compliance
2 parking spaces for each dwelling house, semi-detached dwellings and secondary dwellings.	Two car parking spaces are retained for the site.	Yes

4.2 Impacts of the Development

As noted in the above assessment against the provisions of the relevant Environmental Planning Instruments and Development Control Plan, the development is of a minor scale and nature, and does not present unreasonable environmental, social and economic impacts.

Impacts on Natural and Built Environment

The proposed reconstructed walls will utilise high quality materials and an appropriate colour scheme to visually enhance the locality and existing dwelling. The original dimensions of the garage will be retained to respect the aesthetics of the site, and streetscape. The development does not impact upon native vegetation, soil conditions, foreshore environment or air quality. The development involves the removal of a tree with minor landscape value. The proposal is considered to be of acceptable environmental impact.

Social and Economic Impacts

The proposal will allow for a structurally stable garage that is essential for the lifestyle of the residents. The retaining of the original dimensions of the garage allows for adequate car space for the existing dwelling. The proposal does not engender or offer any negative social or economic outputs.

4.3 Suitability of the Site

The subject site is considered suitable in size and shape to accommodate the remedial works for a garage. The proposal does not introduce any incompatible uses to the site. The works are permissible under the R1 zone.

4.3.1 Access to Services

The site is located within an established residential area with excellent access to services and public transport. As the site is within an established urban area, electricity, sewer, telephone, and water services are readily available to the subject site.

4.3.2 Parking and Access

The on-site parking provided complies with the parking requirements as prescribed by Manly Development Control Plan 2013. It provides a logical and considered approach to the provision of off-street car parking.

4.3.3. Hazards

The site is not in an area recognised by Council as being subject to flooding, landslip, bushfire or any other particular hazards. The proposed development is not likely to increase the likelihood of such hazards occurring and is considered appropriate in this instance.

4.4 The Public Interest

The proposal is considered to be in the public interest through an appropriate design and scale. Thus, the proposal contains nominal negative environmental, social and economic impacts.

The proposed development has been designed to relate to the size, shape and context of the site and has been designed in accordance with the desired future character for development in the area.

In addition, the proposal has been designed to minimise as far as practical any adverse effects on existing and future neighbouring properties. The proposal is consistent with the applicable LEP and DCP provisions except where identified and justified in this Statement of Environmental Effects. Accordingly, the proposed development is considered to be in the public interest.

5.0 CONCLUSION

The Statement of Environmental Effects (SEE) has been prepared to consider the environmental, social and economic impacts of the remedial works for garage at 81 Sydney Road, Manly. The report has addressed the applicable policies and plans, and has provided an environmental assessment in accordance with Section 4.15 of the Environmental Planning and Assessment Act 1979.

The application proposes a permissible development within the subject site locality. The proposal incorporates appropriate design considerations to minimise any adverse impacts on the natural and built environment, and the amenity of the surrounding neighbourhood.

Given the benefits of the development and compliance with the relevant policies and plans, we conclude that the proposed development at 81 Sydney Road, Manly as described in this application is reasonable and supportable, and worthy of approval by Northern Beaches Council.