

# Construction Traffic Management Plan

54-58 Beaconsfield Street, Newport

23082

Prepared for  
Restreet

2 December 2024



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## Document Information

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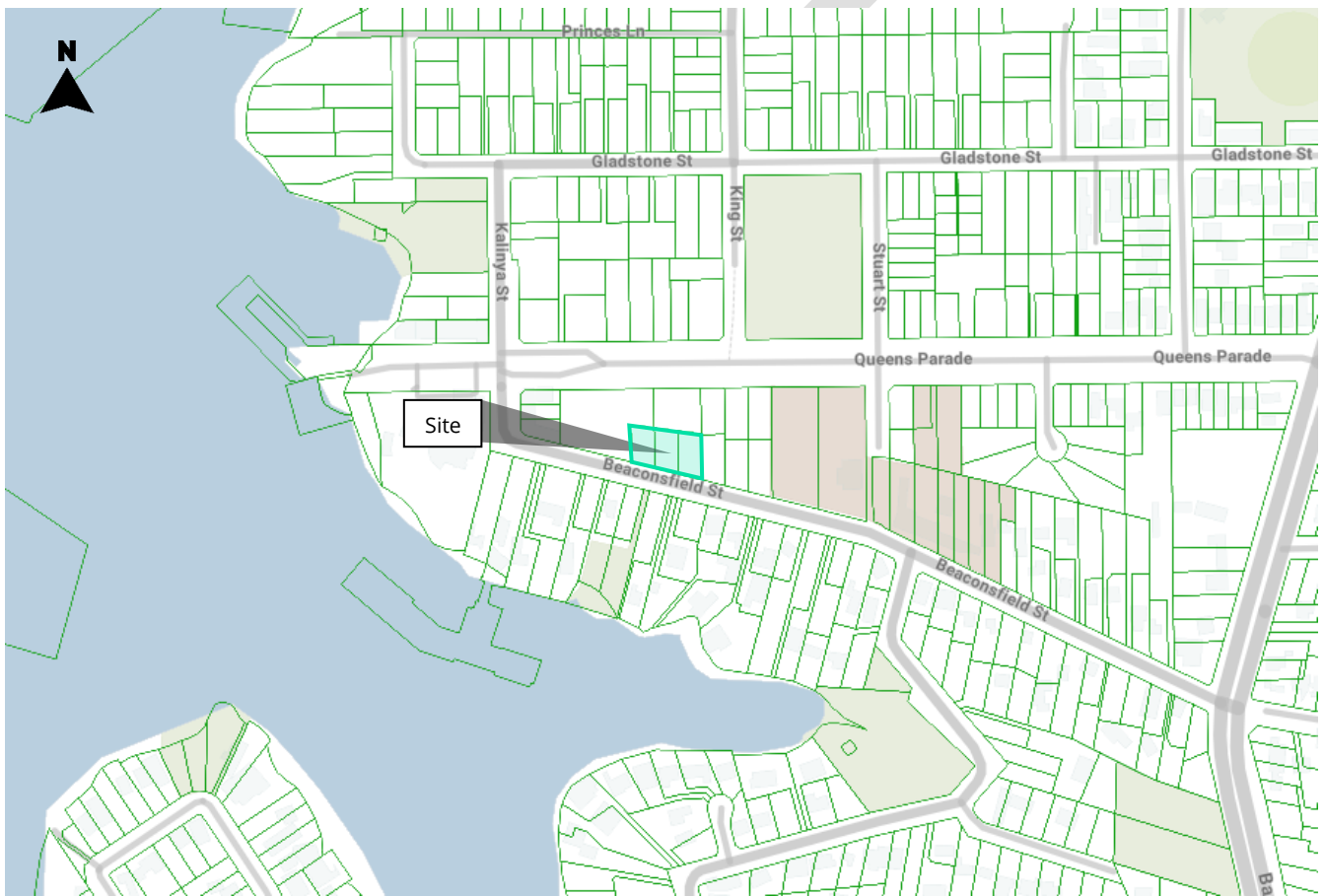


# 1 Introduction

## 1.1 Background

This report has been prepared as part of a construction documentation process in satisfaction of development consent DA2023/1869 issued by Northern Beaches Council for an approved development at 54-58 Beaconsfield Street, Newport (Figure 1-1).

Figure 1-1 Site



Source: Metromap (Modified by Genesis Traffic)

## 1.2 Scope of Works

The purpose of this report is to satisfy the requirements of consent condition No. 13 and 41 of DA2023/1869, outlined in Table 1-1:



Table 1-1 Consent Condition Requirements

Details of Consent	Assessment Reference
<b>13. Construction Traffic Management Plan</b>	
<i>A Construction Traffic Management Plan (CTMP) and report shall be prepared by a Transport for NSW accredited person and submitted to and approved by the Northern Beaches Council Traffic Team prior to issue of any Construction Certificate.</i>	
<i>The CTMP must address following:</i>	
<ul style="list-style-type: none"> <li><i>The proposed phases of construction works on the site, and the expected duration of each construction phase</i></li> </ul>	Section 4.2
<ul style="list-style-type: none"> <li><i>The proposed order in which works on the site will be undertaken, and the method statements on how various stages of construction will be undertaken</i></li> </ul>	Section 4.2
<ul style="list-style-type: none"> <li><i>Make provision for all construction materials to be stored on site, at all times</i></li> </ul>	Section 4.7
<ul style="list-style-type: none"> <li><i>The proposed areas within the site to be used for the storage of excavated materials, construction materials and waste containers during the construction period</i></li> </ul>	Section 4.5
<ul style="list-style-type: none"> <li><i>The proposed method of access to and egress from the site for construction vehicles, including access routes and truck routes through the Council area and the location and type of temporary vehicular crossing for the purpose of minimising traffic congestion and noise in the area, with no access across public parks or reserves being allowed.</i></li> </ul>	Section 4.4
<ul style="list-style-type: none"> <li><i>Where access is required across private property not in the direct ownership of the proponent, such as a private road/driveway, community title road or right of way, the CTMP is to include:</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>Evidence of the legal right and terms to use the access route or provide owners consent from the owners/strata/community association.</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>Demonstrate that direct access from a public space/road is not viable for each stage of works.</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>An assessment to be carried out of the physical constraints of the Right of Carriageway to determine the maximum size of vehicle that may access the site via the Right of Carriageway during construction.</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>Unless owner/strata/community associations consent is obtained, vehicles are not to exceed 24 tonnes or 7.5 metres in length (an assessment must be undertaken that the surface is capable of supporting up to 24 tonnes, otherwise the weight limit should be reduced in the CTMP). If consent is obtained, a copy must be included in the CTMP.</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>No construction vehicles, materials or plant are to be located or parked in the private road/driveway, community title road or right of way.</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>How any disruption to other users of the private road/driveway, community title road or right of way will be minimised and all users kept informed of likely disruption where the access will be closed or blocked for any given time.</i></li> </ul>	Section 5.1
<ul style="list-style-type: none"> <li><i>If trees are located within or overhang the access route, a tree protection plan prepared by an Arborist with minimum AQF Level 5 in arboriculture demonstrating</i></li> </ul>	For others



<i>how any trees within the Right of Carriageway will be protected from damage by construction vehicles. Should any tree protection measures be required on private land in accordance with AS4970-2009 Protection of trees on development sites, owner's consent must be obtained.</i>	
<ul style="list-style-type: none"> <li><i>A Dilapidation report, including photographic surveys, of the private road/driveway/right of way must be included prior to any works commencing on the site. The report must detail the physical condition of the private road/driveway/right of way, and any other adjacent private property assets (including trees) or adjacent public property that may be adversely affected by vehicles servicing the development site to undertake works or activity during site works.</i></li> </ul>	For others
<ul style="list-style-type: none"> <li><i>A requirement for Post-Construction Dilapidation Reports, including photos of any damage evident at the time of inspection, to be submitted after the completion of works and prior to the Occupation certificate. The report must:</i></li> </ul>	For others
<ul style="list-style-type: none"> <li><i>Compare the post-construction report with the pre-construction report,</i></li> </ul>	For others
<ul style="list-style-type: none"> <li><i>Clearly identify any recent damage or change to the private road/driveway/right of way and whether or not it is likely to be the result of the development works,</i></li> </ul>	For others
<ul style="list-style-type: none"> <li><i>Should any damage have occurred, identify remediation actions taken.</i></li> </ul>	For others
<ul style="list-style-type: none"> <li><i>Be submitted to Council with the Occupation Certificate.</i></li> </ul>	Noted
<ul style="list-style-type: none"> <li><i>The proposed method of loading and unloading excavation and construction machinery, excavation and building materials, formwork and the erection of any part of the structure within the site. Wherever possible mobile cranes should be located wholly within the site</i></li> </ul>	Section 4.8
<ul style="list-style-type: none"> <li><i>Make provision for parking onsite. All Staff and Contractors are to use the on-site off-street parking</i></li> </ul>	Section 4.11
<ul style="list-style-type: none"> <li><i>Temporary truck standing/queuing locations in a public roadway/ domain in the vicinity of the site are not permitted unless approved by Council prior</i></li> </ul>	Section 4.4
<ul style="list-style-type: none"> <li><i>Due to the proximity of the site adjacent to Newport Public school, no heavy vehicle movements or construction activities effecting vehicle and pedestrian traffic are permitted in school zone hours (8:00am - 9:30am and 2:30pm - 4:00pm weekdays).</i></li> </ul>	Section 4.3
<ul style="list-style-type: none"> <li><i>Include a Traffic Control Plan prepared by a person with suitable RMS accreditation for any activities involving the management of vehicle and pedestrian safety</i></li> </ul>	Attachment 2
<ul style="list-style-type: none"> <li><i>The proposed manner in which adjoining property owners will be kept advised of the timeframes for completion of each phase of development/construction process. It must also specify that a minimum Fourteen (14) days notification must be provided to adjoining property owners prior to the implementation of any temporary traffic control measure</i></li> </ul>	Section 5.1
<ul style="list-style-type: none"> <li><i>Include a site plan showing the location of any site sheds, location of requested Work Zones, anticipated use of cranes and concrete pumps, structures proposed on the footpath areas (hoardings, scaffolding or shoring) and any tree protection zones around Council street trees</i></li> </ul>	Section 4.5
<ul style="list-style-type: none"> <li><i>Take into consideration the combined construction activities of other development in the surrounding area. To this end, the consultant preparing the CTMP must engage and consult with developers undertaking major development works within</i></li> </ul>	Section 5.2



<i>a 250m radius of the subject site to ensure that appropriate measures are in place to prevent the combined impact of construction activities, such as (but not limited to) concrete pours, crane lifts and dump truck routes. These communications must be documented and submitted to Council prior to work commencing on site</i>	
<ul style="list-style-type: none"> <li><i>The proposed method/device to remove loose material from all vehicles and/or machinery before entering the road reserve, any run-off from the washing down of vehicles shall be directed to the sediment control system within the site</i></li> </ul>	Section 5.4
<ul style="list-style-type: none"> <li><i>Specify that the roadway (including footpath) must be kept in a serviceable condition for the duration of construction. At the direction of Council, undertake remedial treatments such as patching at no cost to Council</i></li> </ul>	Section 5.5
<ul style="list-style-type: none"> <li><i>The proposed method of support to any excavation adjacent to adjoining properties, or the road reserve. The proposed method of support is to be designed and certified by an appropriately qualified and practising Structural Engineer, or equivalent</i></li> </ul>	N/A
<ul style="list-style-type: none"> <li><i>Proposed protection for Council and adjoining properties</i></li> </ul>	Section 4.6
<ul style="list-style-type: none"> <li><i>The location and operation of any on-site crane</i></li> </ul>	Section 4.7
<p><i>The CTMP shall be prepared in accordance with relevant sections of Australian Standard 1742 – “Manual of Uniform Traffic Control Devices”, RMS’ Manual – “Traffic Control at Work Sites”.</i></p> <p><i>All fees and charges associated with the review of this plan is to be in accordance with Council’s Schedule of Fees and Charges and are to be paid at the time that the Construction Traffic Management Plan is submitted.</i></p> <p><i>Reason: To ensure public safety and minimise any impacts to the adjoining pedestrian and vehicular traffic systems.</i></p>	Section 4.10

Table 1-2 Consent Condition Requirements

Details of Consent	Assessment Reference
<p><b>41. Demolition Traffic Management Plan</b></p> <p><i>As a result of the site constraints, limited vehicle access and parking, a Demolition Traffic Management Plan (DTMP) shall be prepared by a suitably accredited person and submitted to and approved by the Northern Beaches Council Traffic Team prior to commencing any demolition work</i></p> <p><i>Due to heavy traffic congestion throughout the area, truck movements will be restricted during the major commuter peak times being 8.00-9.30am and 4.30-6.00pm</i></p> <p><i>The DTMP must:</i></p>	
<ul style="list-style-type: none"> <li><i>Make provision for all construction materials to be stored on site, at all times</i></li> </ul>	Section 4.7
<ul style="list-style-type: none"> <li><i>The DTMP is to be adhered to at all times during the project</i></li> </ul>	Noted
<ul style="list-style-type: none"> <li><i>Specify construction truck routes and truck rates. Nominated truck routes are to be distributed over the surrounding road network where possible</i></li> </ul>	Sections 4.2, 4.4
<ul style="list-style-type: none"> <li><i>Provide for the movement of trucks to and from the site, and deliveries to the site. Temporary truck standing/queuing locations in a public roadway/domain in the</i></li> </ul>	Section 4.4



<i>vicinity of the site is not permitted unless prior approval is granted by Council's Traffic Engineers</i>	
<ul style="list-style-type: none"> <li>• <i>Include a Traffic Control Plan prepared by a TfNSW accredited traffic controller for any activities involving the management of vehicle and pedestrian traffic.</i></li> </ul>	<b>Attachment 2</b>
<ul style="list-style-type: none"> <li>• <i>Specify that a minimum fourteen (14) days notification must be provided to adjoining property owners prior to the implementation of any temporary traffic control measures.</i></li> </ul>	Section 5.1
<ul style="list-style-type: none"> <li>• <i>Include a site plan showing the location of any site sheds, location of requested Work Zones, anticipated use of cranes, structures proposed on the footpath areas (hoardings, scaffolding or temporary shoring) and extent of tree protection zones around Council street trees.</i></li> </ul>	Section 4.5
<ul style="list-style-type: none"> <li>• <i>Take into consideration the combined construction activities of other developments in the surrounding area. To this end, the consultant preparing the DTMP must engage and consult with developers undertaking major development works within a 250m radius of the subject site to ensure the appropriate measures are in place to prevent the combined impact of construction activities. These communications must be documented and submitted to Council prior to work commencing on site</i></li> </ul>	Section 5.2
<ul style="list-style-type: none"> <li>• <i>Specify soil management process and facilities to be used on site</i></li> </ul>	By others
<ul style="list-style-type: none"> <li>• <i>Specify that the roadway (including footpath) must be kept in a serviceable condition for the duration of demolition. At the direction of Council, the applicant is to undertake remedial treatments such as patching at no cost to council</i></li> </ul>	Section 5.4

### 1.3 Reference Documents

Reference has been made to the following documents when preparing this report:

- Traffic Control at Worksites Technical Manual, TfNSW (Issue 6.1, 2022)



## 2 Approved Development

The approved development scheme involves demolition works and construction of a residential flat building with an excavated basement carpark.

New vehicle access will be provided on the Beaconsfield Street frontage, providing access to the new car park.

Details of the approved development scheme are indicated in the architectural plans reproduced in part in **Attachment 1**.

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## 3 Existing Conditions

### 3.1 Site and Surrounding Context

The development site (Figure 3-1) is legally known as Lot 5B DP 18658, Lot 6 DP1096088 and Lot 7B DP 162021, located, located at 54-58 Beaconsfield Street, Newport. The site occupies an area of 2,113.5m<sup>2</sup> and has frontage to Beaconsfield Street. The site is occupied by three single residential dwellings at present, with vehicle access points located at Beaconsfield Street.

Figure 3-1 Site Context



Source: Metromap (Modified by Genesis Traffic)



### 3.2 Road Network

The existing road network serving the site area (Figure 3-2) are detailed in Table 3-1:

Figure 3-2 Road Network



Source: TfNSW (modified by Genesis Traffic)

Table 3-1 Surrounding Road Network

Road Name	Description
<b>Barrenjoey Road</b>	<ul style="list-style-type: none"> <li>State Road</li> <li>Speed limit 60km/h</li> <li>3 lanes in each direction</li> <li>Time restricted (4P) on-street parking during the weekend along certain parts of the streets, otherwise there is a no stopping striction along both sides of the street</li> </ul>
<b>Beaconsfield Street</b>	<ul style="list-style-type: none"> <li>Local Road</li> <li>Speed limit 50km/h</li> <li>1 lane in each direction</li> <li>Unrestricted on-street parking along both sides of the street</li> </ul>





### 3.3 Traffic Controls

The traffic controls on the road system in the vicinity of the site are detailed in Table 3-2:

Table 3-2 Surrounding Traffic Controls

Traffic Control	Location
<b>Traffic Signal</b>	<ul style="list-style-type: none"><li>· Intersection(s) of:<ul style="list-style-type: none"><li>○ Beaconsfield Street and Barrenjoey Road</li></ul></li></ul>
<b>School Zone</b>	<ul style="list-style-type: none"><li>· Along part(s) of<ul style="list-style-type: none"><li>○ Beaconsfield Street</li></ul></li></ul>
<b>Pedestrian Crossing</b>	<ul style="list-style-type: none"><li>· Along part(s) of<ul style="list-style-type: none"><li>○ Beaconsfield Street and Kalinya Street</li></ul></li></ul>

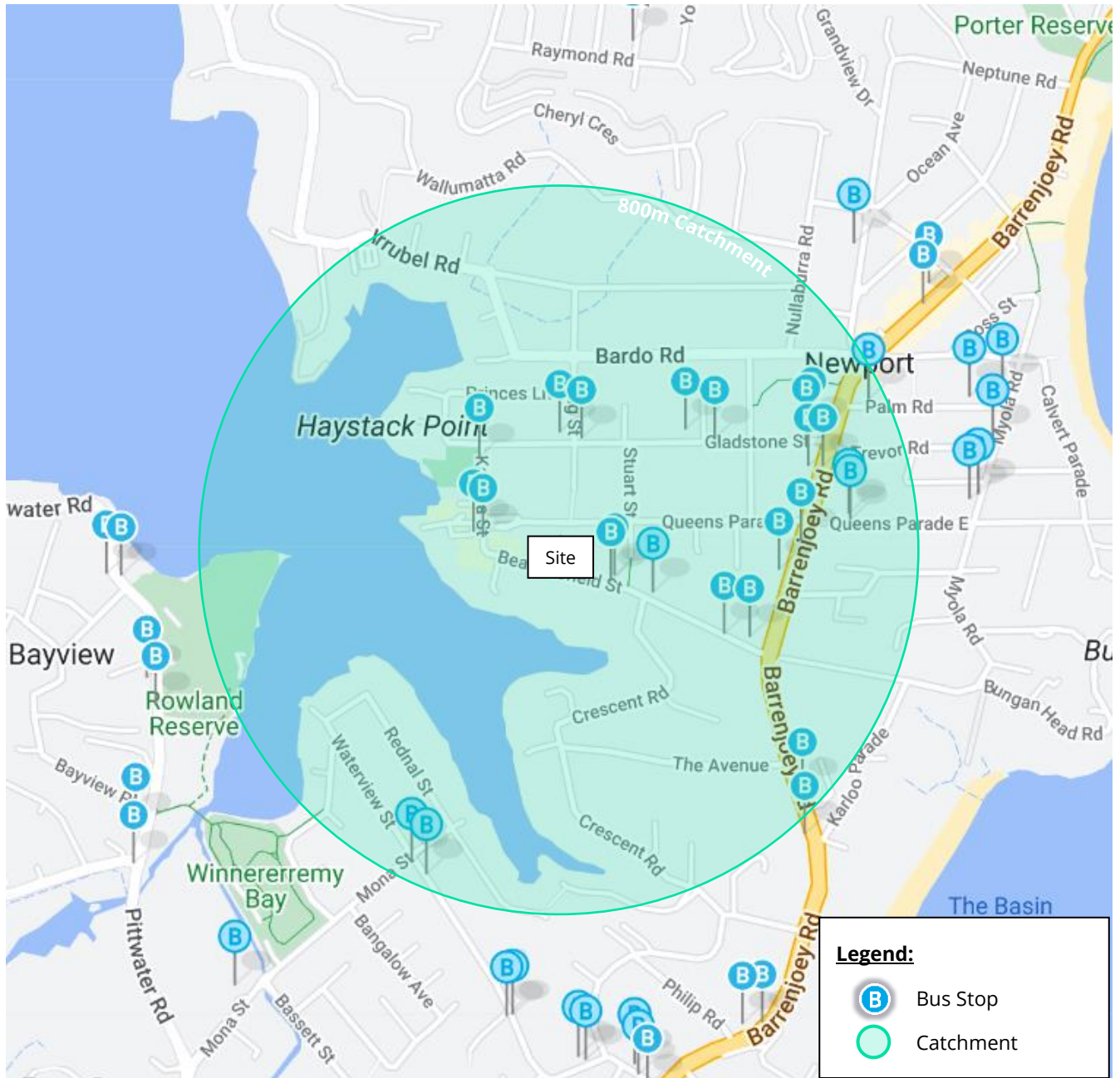
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### 3.4 Public Transport Services

The local public transport services are illustrated in Figure 3-3.

Figure 3-3 Local Public Transport Locations



Source: Google Map (Modified by Genesis Traffic)



## Bus

Local bus service(s) is within walking distance (140m) of the site, as follows (Table 3-3).

Table 3-3 Bus Services Provision

Bus Line	Bus Route
<b>190X</b>	North Avalon to City Wynyard (Express Service)
<b>199</b>	Palm Beach to Manly via Mona Vale & Dee Why

## 3.5 Existing Traffic Conditions

Observation of traffic operations in the vicinity of the site indicates minor delays due to the pedestrian crossing that links to Newport Public School. Nonetheless, these delays only occur for relatively brief periods (both AM and PM peaks). There is no apparent capacity constraint in the immediate local road network.

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## 4 Construction Traffic Management

### 4.1 Site Contact

The contact person who is to have authority without reference to other persons to comply with instructions issued by the Council's Traffic Engineer or the Police is provided below (Table 4-1).

Table 4-1 Contact Details

Details	
<b>Name</b>	Alistair Jones
<b>Contact No.</b>	0497764180
<b>Email:</b>	<a href="mailto:alistair@dallad.com.au">alistair@dallad.com.au</a>

### 4.2 Construction Program

Table 4-2 summarises the envisaged truck visitation levels for each work phase.

Table 4-2 Works Program

Phase	Program	Type of Truck	Estimated Trips per Day	Estimated Period (Weeks)	Estimated No. of Workers per Day
1	Site Establishment	8.8m MRV	2	2	5
2	Demolition	16.8m T&D	10	8	5
3	Excavation / Earthworks	16.8m T&D	20	20	10
4	Construction / Concrete Pouring	8.8m MRV	3	24	20
5	Fitout	8.8m MRV	1	24	20

Abbreviation:

T&D = Truck and Dog

MRV = Medium Rigid Vehicle



### 4.3 Construction Hours

Table 4-3 summarises the approved construction hours:

Table 4-3 Permitted Work Hours

Day(s)	Permitted Work Hours
<b>Mondays to Friday</b>	7:00am to 5:00pm Demolition and excavation works are restricted to 8:00am to 5:00pm
<b>Saturday</b>	8:00am to 1:00pm
<b>Sunday and Public Holidays</b>	No Work

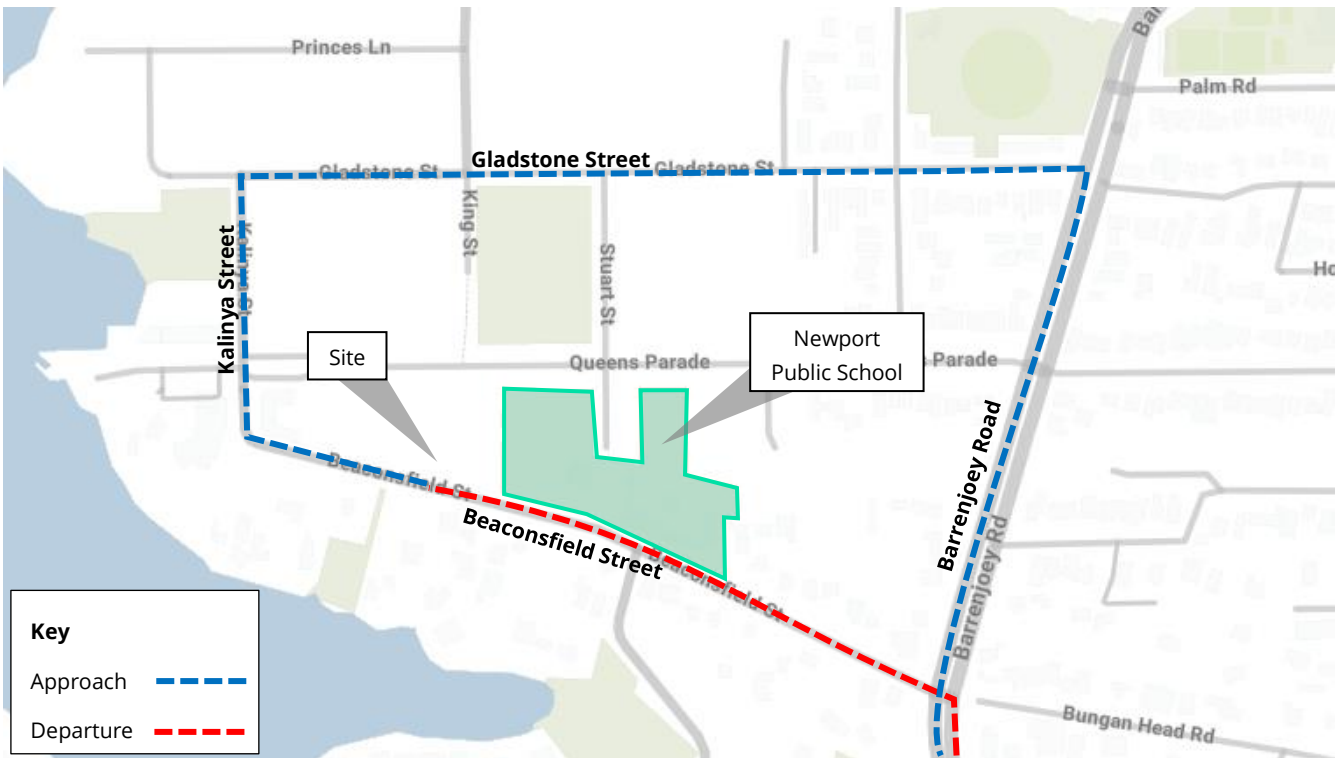
While trucks may commence deliveries/loading activities from 7.00am, it is noted that Condition 21 prohibits truck movements between 8:00am – 9:30am and 2:30pm – 4pm due to heavy traffic congestion caused by Newport Public School. The time restriction is impractical and significantly extends the works program. Recognising council's concerns, as an alternative, it is proposed that truck movements be restricted from passing along the Newport Public School frontage during school days, ensuring pedestrian safety as depicted in Figure 4-2. During school holidays and Saturdays, truck movements will be permitted to pass through the Newport Public School frontage as shown in Figure 4-1.

### 4.4 Construction Vehicle Route

Truck movements associated with the construction processes will approach and depart the site via Beaconsfield Street, as illustrated in Figure 4-1 and Figure 4-2. No queuing of heavy vehicles is to occur on the surrounding streets unless previously approved by the Council.

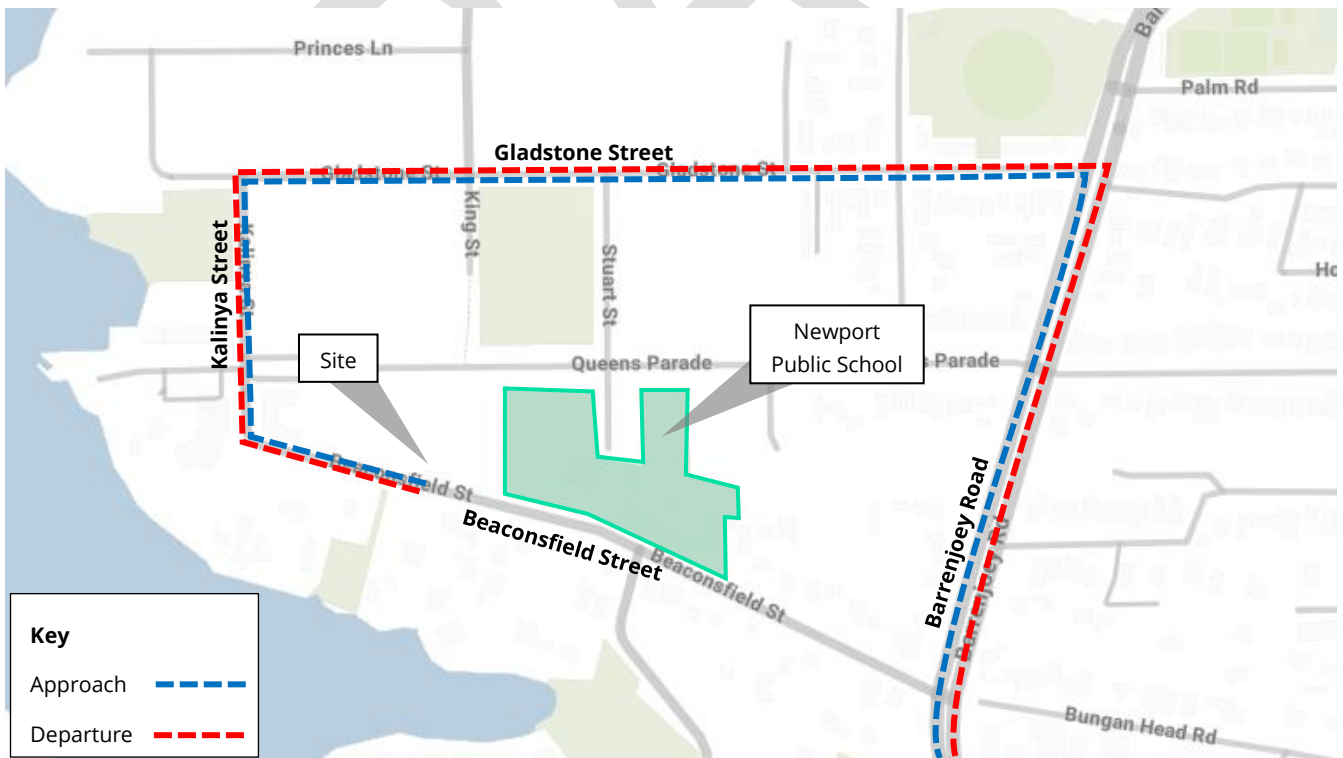


Figure 4-1 Truck Route During School Holidays and Saturdays



Source: Mecone (modified by Genesis Traffic)

Figure 4-2 Truck Route During School Days



Source: Mecone (modified by Genesis Traffic)



During school days, trucks are only permitted to exit westbound (away from Newport Public School) when there is a suitable gap in traffic. No traffic control is permitted during School Zone hours.

During the course of construction, all trucks will approach and depart the site in a forward direction. In the event where construction vehicles are unable to manoeuvre from/to the site, a Works Zone will be established along the site frontage. Vehicles will approach, stand within the Works Zone and depart forwards. All necessary oversized mobile crane/truck access will be subject to a separate permit issued by the Council before the planned event. If reversing is necessary, vehicle movements should be undertaken under the supervision of accredited traffic controllers.

The manner in which vehicles access and depart the site is shown on the Vehicle Movement Plan (VMP) in **Attachment 2**, while detailed swept path analysis demonstrating the nominated construction vehicles' movements at the site frontages are shown in **Attachment 3**.

#### 4.5 Site Setout

A proposed site plan indicating the expected site access location, proposed truck loading area, site office, amenity and storage, are indicated in Figure 4-3.

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Figure 4-3 Site Setup



Source: Metromap (modified by Genesis Traffic)

#### 4.6 Fencing/Perimeter Separation

Class A fencing will be erected along the construction site perimeter to cordon off the site from pedestrian movements along the site frontage and neighbouring properties where applicable.

#### 4.7 Materials Handling and Cranage

All materials must be loaded/off-loaded and stored within the site boundary at all times. During the excavation of the site, the excavated materials will be loaded/unloaded on site. No materials are to be stored outside the site boundary at any time.

All materials will be loaded/off-loaded using a mobile crane and the mobile crane will be operated within the site at all times.





## 4.8 Works Zone

A Works Zone (27m long) is required along the Beaconsfield Street frontage for loading/unloading during goods deliveries.

## 4.9 Pedestrian Movement Plan (PMP)

During the course of construction, pedestrian movements along the frontage footpath will be diverted to the opposite side of the street. All construction-related traffic movements along the frontages will occur under the supervision of on-site trained personnel, with trucks escorted between the site access and associated frontage to ensure pedestrian safety. Details of the Pedestrian Movement Plan (PMP) are provided in **Attachment 2**.

## 4.10 Traffic Guidance Scheme

The Traffic Guidance Scheme (TGS) set out a suite of site traffic management principles in accordance with the TfNSW Traffic Control at Work Sites Technical Manual Version 6.1 dated 28 February 2022. The control of traffic at work sites must be undertaken with reference to Workcover requirements and the contractor's Constructions Workplace Health and Safety Manuals.

The TGSs have been prepared by a Certified Traffic Controller in accordance with Australian Standards 1742.3. The site-specific TGSs are reproduced in **Attachment 2**.

## 4.11 Construction Worker Parking

There will be no on-site car parking available for workers during the earlier construction stage. Once the internal driveway and basement car park are built (and when construction activity is most intensive), workers can park in the basement.

Some unrestricted on-street parking will be available along the surrounding local streets for the workers. Given the proximity of the site to high-frequency public transport services, all workers will be encouraged at all times to utilise the highly accessible public transport system which exists in the vicinity of the site or to carpool wherever possible.

A tool drop-off and storage facility will be provided within the site. This would allow tradespeople to drop-off and store their tools and machinery, allowing them to use public transport to travel to/ from the site daily.

Workers will also be informed of appropriate tool/equipment drop-off and storage arrangements made within site sheds and amenities provided on-site. Bus and train schedules will be provided to all workers during site induction to demonstrate alternative modes of transport available.



## 4.12 Site Induction

All workers and visitors employed on the site by the appointed contractor (including sub-contractors) will be required to undergo a formal 'site induction' process and all inductions will be performed specifically to each trade according to SafeWork OH & S requirements.

The induction will include details of approved access routes to and from the construction site for site staff and delivery vehicles, parking arrangements, as well as standard environmental, WHS, driver protocols and emergency procedures. The agreed work hours must be included as part of this induction.

## 4.13 Other Construction Management Principles

1. Removed or damaged parking signs shall be replaced immediately.
2. Damaged trees shall be repaired / replaced to the satisfaction of Council.
3. Traffic and pedestrian control shall be in accordance with the TfNSW Traffic Control at Work Sites Technical Manual and Australian Standard AS1742.3 – Manual of uniform traffic control devices - Part 3 Traffic control for works on roads.
4. Reserving of on-street parking shall not occur without prior Council approval. All on-street parking spaces outside the site are to remain available for the use by the general public during the approved work hours unless Council signage is installed to the contrary.
5. Barricades, delineators (including bollards, witches hats, barrier boards etc.) shall not be placed in the kerbside parking lane outside or adjacent to the site to reserve on street parking spaces without the prior approval of Council.
6. A separate application to and approval from Council will be submitted for occupation of any road related area (traffic and parking lanes, verge, footpath etc.).



## 5 Construction Impact

### 5.1 Public Notification & Communication

The nominated contractor shall prepare notification letters to advise the following neighbouring properties of the proposed construction works and timing thereof. A minimum notice period of 14 days shall be applicable for all external communications.

The nominated contractor shall also engage with the surrounding building teams at the time of construction to establish the extent of truck delivery movements with an aim to minimise overlapping movements on the same routes.

The following addresses will be notified by letterbox drop prior to the start of works, providing information relating to the project schedule:

<b>Adjacent to the site</b>	52, 60 Beaconsfield Street
<b>Opposite the site</b>	57, 59, 61, 63, 65, 67A Beaconsfield Street
<b>At the rear of the site</b>	13,15,17 Queens Parade

Newport Public School is to be contacted in the event of kerbside operations to ensure the safety of staff and children. This ensures that school events will not interfere with construction

### 5.2 Surrounding Development Impact

The assessment notes there are no other notable construction works taking place within a 250m radial coverage from the site.

### 5.3 Spoil Management

Wheel wash station will be positioned at the entry/exit points to ensure that soil/excavated materials are not transported on wheels or tracks of vehicles or plant and deposited on surrounding roadways. All arriving and departing construction vehicles are to have their loads covered during demolition and excavation.

### 5.4 Road Serviceability

The nominated contractor will be responsible for ensuring that the road pavement, kerb, and gutter along each road frontage shall remain in clean and serviceable states during the course of the construction at no cost to Council.



## 5.5 Impact on Public Transport Services

The nominated heavy vehicle haulage routes will largely be limited to State Roads and Regional Roads which are designed to accommodate heavy vehicle movements. As such, there will be no adverse impact on existing public transport services.

While the nominated truck routes will overlap with bus routes during the construction period, it is not expected that estimated truck movements would have no material effect on the existing bus services.

## 5.6 Impact on Emergency Vehicle Access

Site personnel will be on-site regularly with contact details prominently displayed and visible from the road frontage. Access to the site and neighbouring sites by emergency vehicles would not be affected by the construction activities. There will be no adverse impact on emergency vehicle access to the site or other neighbouring properties as a result of the proposed activities.

DRAFT



# Attachment 1

## Architectural Plan

DRAFT





**GENERAL NOTES:**

- ALL WORKS TO COMPLY WITH BUILDING CODE OF AUSTRALIA, REQUIREMENTS OF RELEVANT STATUTORY AUTHORITIES / LOCAL GOVERNMENT & RELEVANT AUSTRALIAN BUILDING STANDARDS
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- AREA SCHEDULES SUPPLIED ARE APPROXIMATE ONLY - FUTURE ALLOWANCE FOR VERTICAL SERVICE DUCTS, STRUCTURAL WALL SYSTEMS AND CONSULTANT INPUT WILL BE REQUIRED

**LEGEND:**

- AW AWNING
- AH ACCESS HATCH
- AC A/C CONDENSER UNITS
- FH FIRE HYDRANT
- FHR FIRE HOSE REEL
- FS FIRE STAIRS
- GC GARBAGE CHUTE
- HW HIGHLIGHT WINDOW
- MV MECHANICAL RISER TO FUTURE DETAIL
- MB MAILBOX TO FUTURE DETAIL
- R 240L RECYCLING BIN
- SK SKYLIGHT
- ST STORAGE
- HWU HOT WATER UNITS

**MATERIALS LEGEND:**

- AFG ALUMINIUM FRAMED GLAZING
- LV ALUMINIUM ELLIPTICAL FIXED LOUVERS
- AW AWNING (TO FUTURE DETAIL)
- CONC CONCRETE
- FB FACE BRICK
- GB FRAMELESS TOUCHED GLASS BALUSTRADE (TO BCA/AUS STANDARDS)
- MC METAL CLADDING
- FCE METAL FENCING (TO FUTURE SELECTION)
- PC COMPOSITE PANEL CLADDING
- PSK PRIVACY SCREEN (TO FUTURE SELECTION)
- PTX PAINT FINISH TYPE-X
- RD ROLLER DOOR
- RP RIVER PEBBLES
- RW RENDERED FINISH/SELECTED PAINT FINISH
- TC TIMBER CLADDING
- VB EXTERNAL VENETIAN BLINDS

**NOTE: REFER TO LANDSCAPE PLANS BY WYER & CO.**

Issue	Date	Description
A	28/10/2023	DEVELOPMENT APPLICATION
B	7/09/2024	AMENDED DEVELOPMENT APPLICATION
C	29/05/2024	AMENDED DEVELOPMENT APPLICATION
D	12/06/2024	AMENDED DEVELOPMENT APPLICATION

CLIENT:  
JAK Newport Pty Ltd

**pbd architects**

P - 02 9698 8140  
E - info@pbdarchitects.com.au  
W - pbdarchitects.com.au  
Level 2, 52 Albion Street,  
Sunny Hills NSW 2100  
ABN 36 147 035 550  
Registered Architect:  
Paul Buljevic - NSW 7768

PROJECT:  
**RESIDENTIAL FLAT BUILDING - KENZA**  
54 - 58 BEACONSFIELD STREET NEWPORT NSW

DRAWING TITLE:  
**Ground Floor Plan**

DRAWING BY: AS	CHECKED BY: KL	DATE: 17/06/2024
SCALE: 1:100@A1	DRAWING NO: DA100	ISSUE: D
PROJECT NO: 2311		





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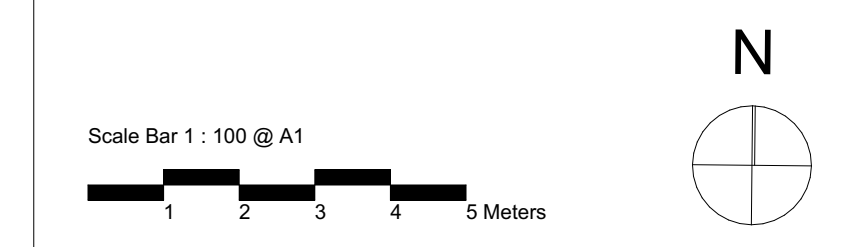
**LEGEND:**

- AW AWNING
- AH ACCESS HATCH
- AC A/C CONDENSER UNITS
- FH FIRE HYDRANT
- FHR FIRE HOSE REEL
- FS FIRE STAIRS
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- ST STORAGE
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- PTX PAINT FINISH TYPE X
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D	12/06/2024	AMENDED DEVELOPMENT APPLICATION



CLIENT:  
**JAK Newport Pty Ltd**

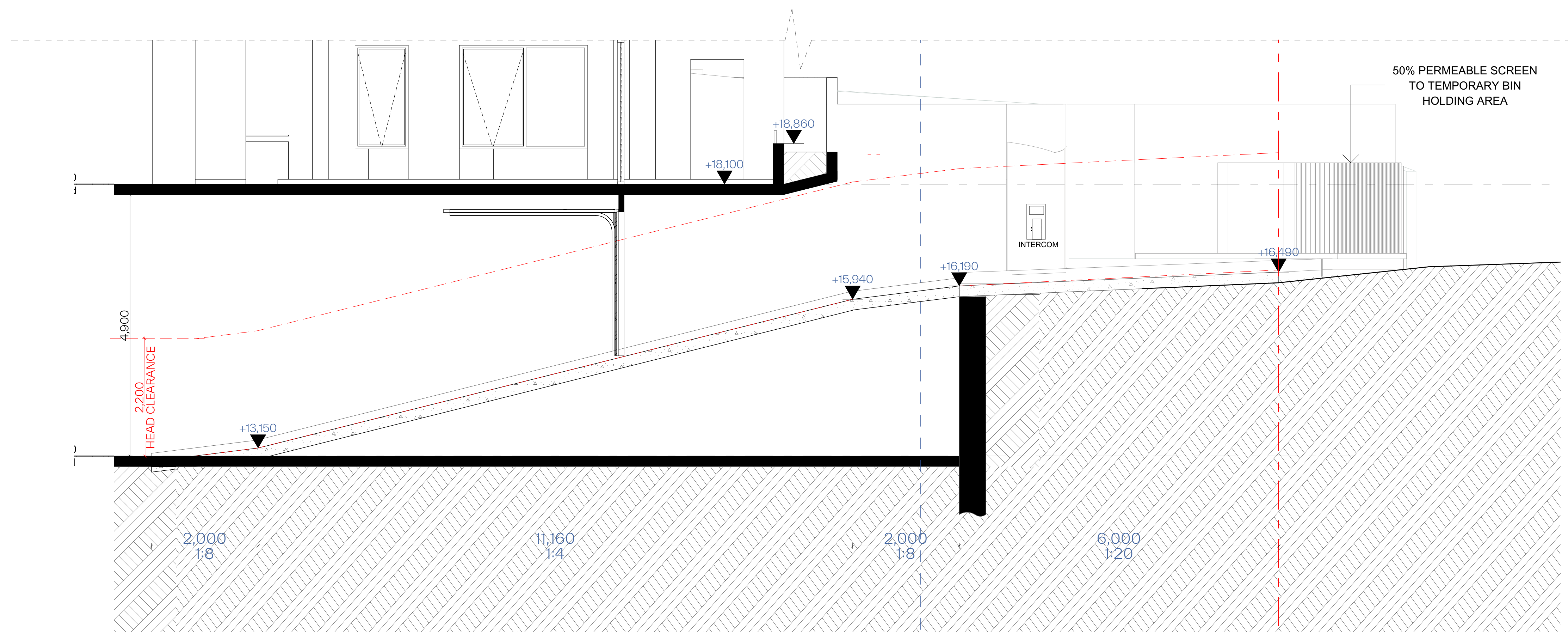
P - 02 9698 8140  
 E - info@pbdarchitects.com.au  
 W - pbdarchitects.com.au  
 Level 2, 52 Albion Street,  
 Surry Hills NSW 2010  
 ABN 36 147 035 550  
 Notarised Architect:  
 Paul Buljovic NSW 7768

PROJECT:  
**RESIDENTIAL FLAT BUILDING - KENZA**  
 54 - 58 BEACONSFIELD STREET NEWPORT NSW

DRAWING TITLE:  
**Basement Plan**

DRAWING BY: AS	CHECKED BY: KL	DATE: 17/06/2024
SCALE: 1:100@A1	DRAWING NO: <b>DA104</b>	ISSUE: <b>D</b>
PROJECT NO: 2311		





Driveway Section

1:50

**GENERAL NOTES:**

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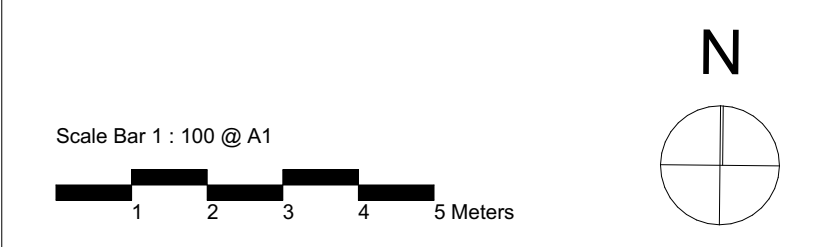
**LEGEND:**

- AW AWNING
- AH ACCESS HATCH
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- FH FIRE HYDRANT
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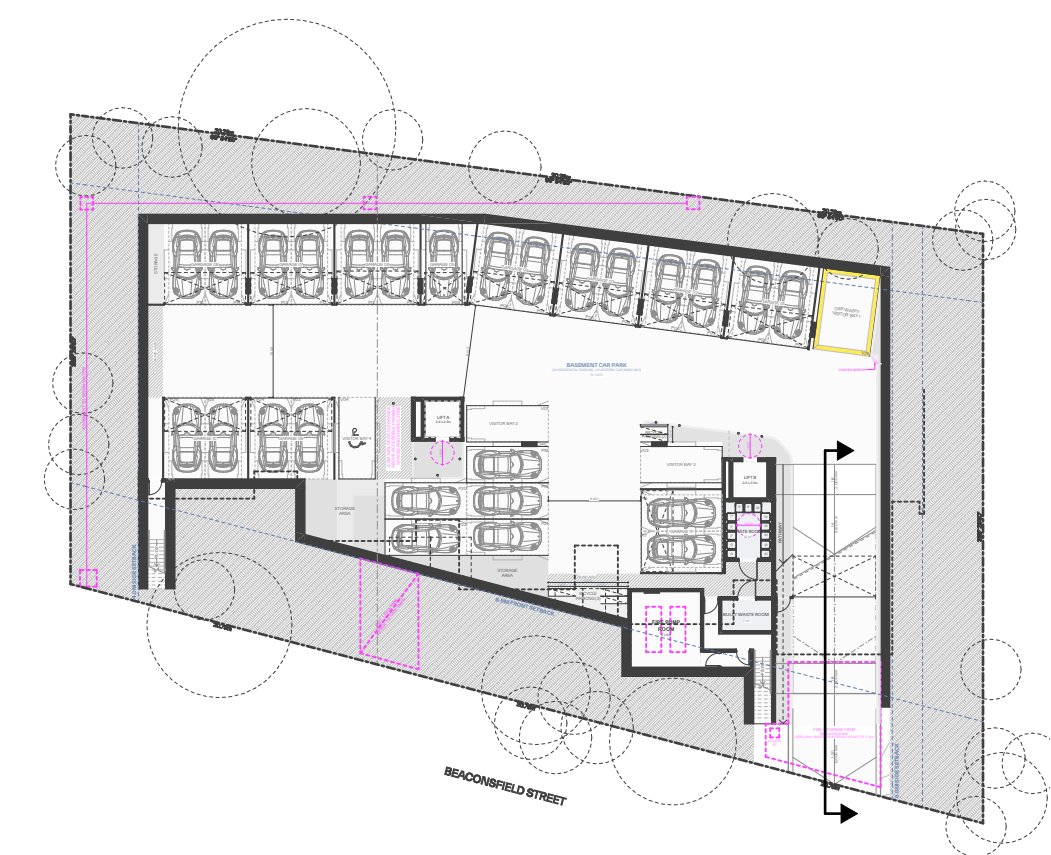
CLIENT:  
JAK Newport Pty Ltd

P - 02 9698 8140  
 E - info@pbdarchitects.com.au  
 W - pbdarchitects.com.au  
 Level 2, 52 Albion Street,  
 Surry Hills NSW 2010  
 ABN 36 147 035 550  
 Notarised Architect:  
 Paul Buljevic: NSW 7768

PROJECT:  
**RESIDENTIAL FLAT BUILDING - KENZA**  
54 - 58 BEACONSFIELD STREET NEWPORT NSW

DRAWING TITLE:  
**Driveway Section**

DRAWING BY: AS	CHECKED BY: KL	DATE: 17/06/2024
SCALE: 1:500, 1:50@A1	DRAWING NO: <b>DA302</b>	ISSUE: <b>D</b>
PROJECT NO: 2311		



1 Basement Plan 1:500





# Attachment 2

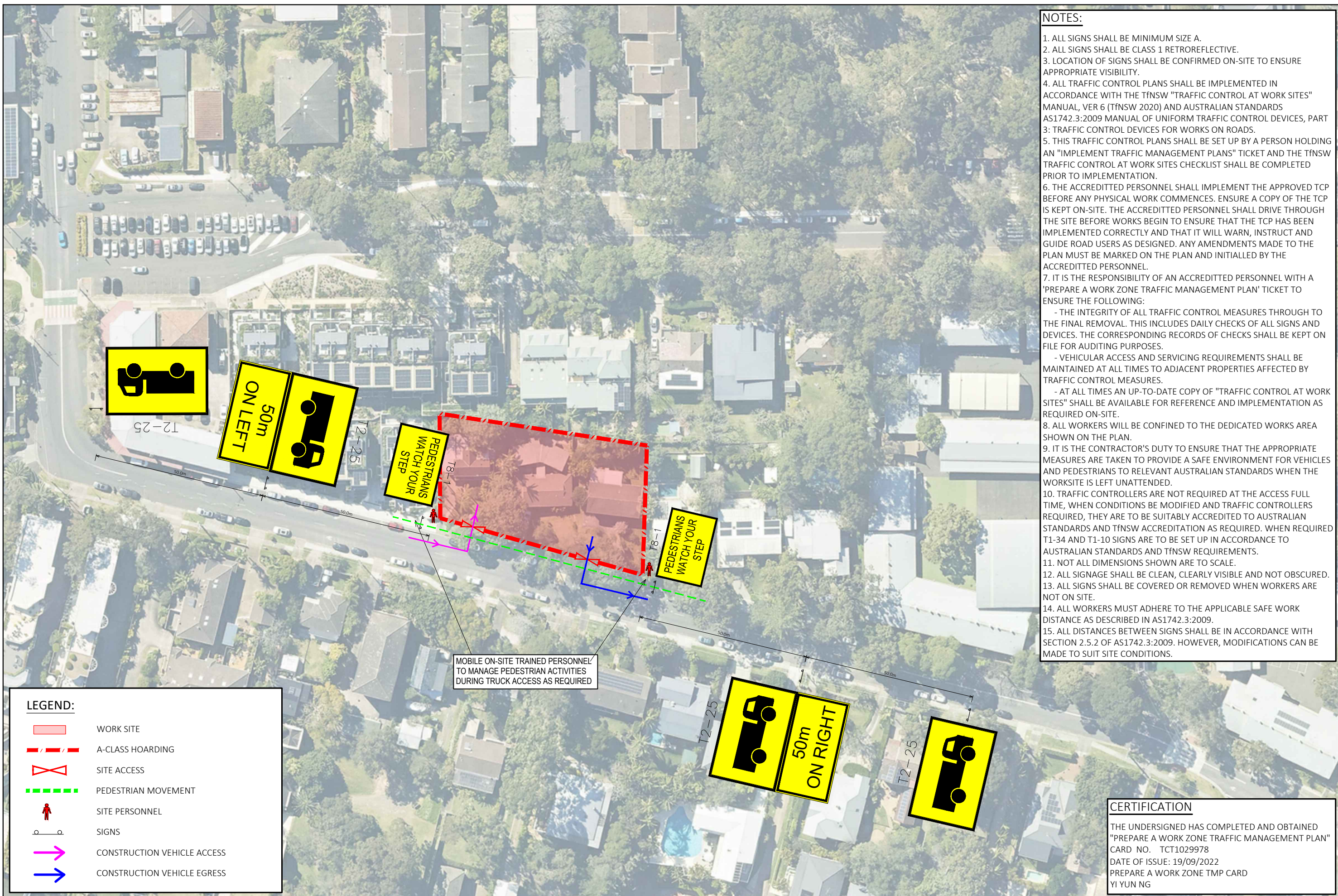
Traffic Guidance Scheme (TGS)

Vehicle Movement Plan (VMP)

Pedestrian Movement Plan (PMP)

DRAFT





- NOTES:**
1. ALL SIGNS SHALL BE MINIMUM SIZE A.
  2. ALL SIGNS SHALL BE CLASS 1 RETROREFLECTIVE.
  3. LOCATION OF SIGNS SHALL BE CONFIRMED ON-SITE TO ENSURE APPROPRIATE VISIBILITY.
  4. ALL TRAFFIC CONTROL PLANS SHALL BE IMPLEMENTED IN ACCORDANCE WITH THE TfNSW "TRAFFIC CONTROL AT WORK SITES" MANUAL, VER 6 (TfNSW 2020) AND AUSTRALIAN STANDARDS AS1742.3:2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, PART 3: TRAFFIC CONTROL DEVICES FOR WORKS ON ROADS.
  5. THIS TRAFFIC CONTROL PLANS SHALL BE SET UP BY A PERSON HOLDING AN "IMPLEMENT TRAFFIC MANAGEMENT PLANS" TICKET AND THE TfNSW TRAFFIC CONTROL AT WORK SITES CHECKLIST SHALL BE COMPLETED PRIOR TO IMPLEMENTATION.
  6. THE ACCREDITED PERSONNEL SHALL IMPLEMENT THE APPROVED TCP BEFORE ANY PHYSICAL WORK COMMENCES. ENSURE A COPY OF THE TCP IS KEPT ON-SITE. THE ACCREDITED PERSONNEL SHALL DRIVE THROUGH THE SITE BEFORE WORKS BEGIN TO ENSURE THAT THE TCP HAS BEEN IMPLEMENTED CORRECTLY AND THAT IT WILL WARN, INSTRUCT AND GUIDE ROAD USERS AS DESIGNED. ANY AMENDMENTS MADE TO THE PLAN MUST BE MARKED ON THE PLAN AND INITIALED BY THE ACCREDITED PERSONNEL.
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  15. ALL DISTANCES BETWEEN SIGNS SHALL BE IN ACCORDANCE WITH SECTION 2.5.2 OF AS1742.3:2009. HOWEVER, MODIFICATIONS CAN BE MADE TO SUIT SITE CONDITIONS.

**LEGEND:**

- WORK SITE
- A-CLASS HOARDING
- SITE ACCESS
- PEDESTRIAN MOVEMENT
- SITE PERSONNEL
- SIGNS
- CONSTRUCTION VEHICLE ACCESS
- CONSTRUCTION VEHICLE EGRESS

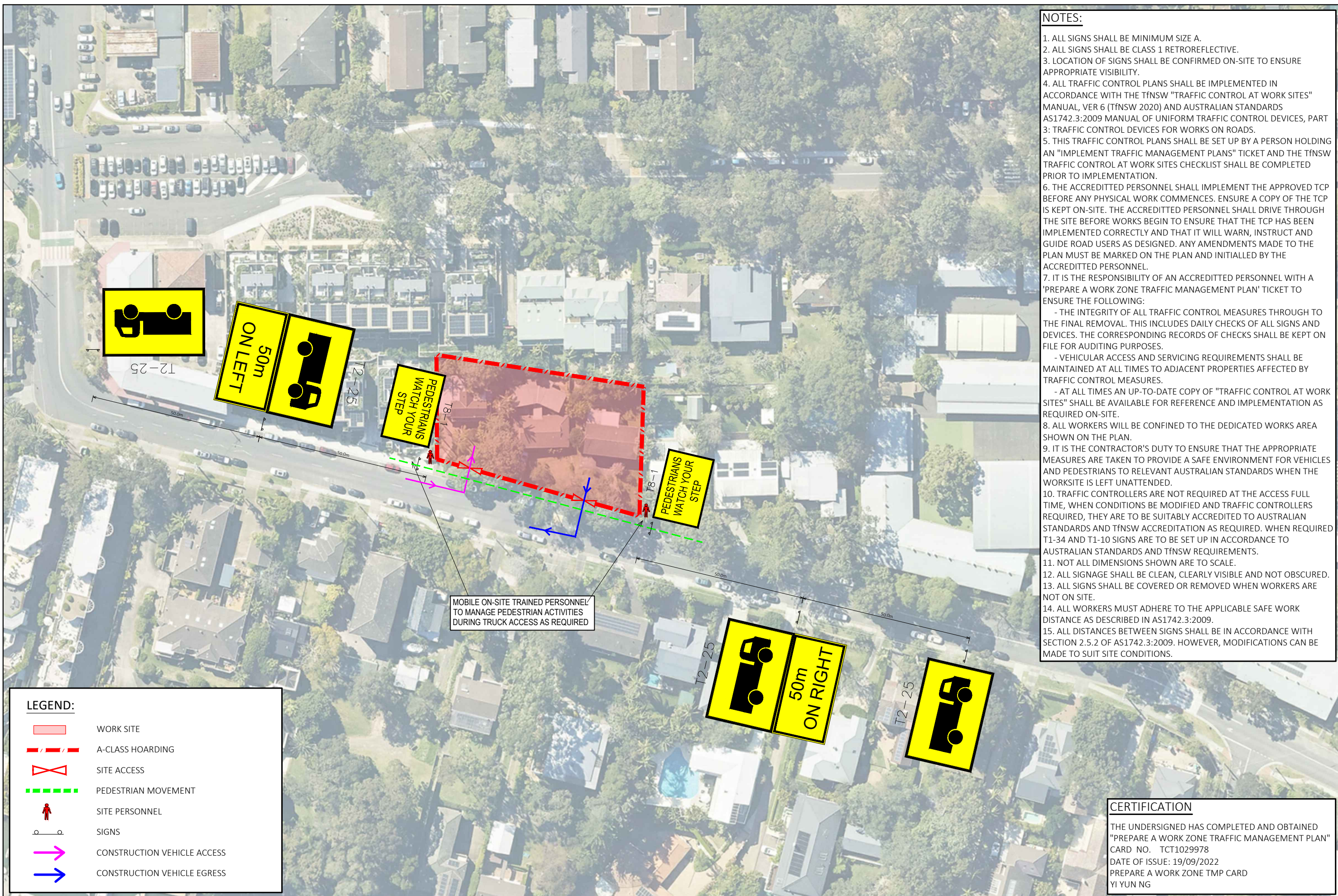
MOBILE ON-SITE TRAINED PERSONNEL TO MANAGE PEDESTRIAN ACTIVITIES DURING TRUCK ACCESS AS REQUIRED

**CERTIFICATION**

THE UNDERSIGNED HAS COMPLETED AND OBTAINED "PREPARE A WORK ZONE TRAFFIC MANAGEMENT PLAN" CARD NO. TCT1029978  
 DATE OF ISSUE: 19/09/2022  
 PREPARE A WORK ZONE TMP CARD  
 YI YUN NG

Plotted by Brian Bui G:\2023\23082 - 54-58 Beaconsfield Street, Newport\Drawings\CC\23082-V1.3-TGS.dwg





- NOTES:**
1. ALL SIGNS SHALL BE MINIMUM SIZE A.
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**LEGEND:**

- WORK SITE
- A-CLASS HOARDING
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- SIGNS
- CONSTRUCTION VEHICLE ACCESS
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  - WHEN TRAFFIC CONTROLLER/S ARE NOT ON SITE, TRAFFIC CONTROLLER (T1-34) AND PREPARE TO STOP (T1-18) SIGNS SHALL BE COVERED OR REMOVED.
  - ROADWORK SIGNS SHALL BE COVERED OR REMOVED WHEN WORKERS ARE NOT ON SITE.
  - NOT ALL DIMENSIONS SHOWN ARE TO SCALE.
  - ALL WORKERS MUST ADHERE TO THE APPLICABLE SAFE WORK DISTANCE AS DESCRIBED IN AS1742.3:2009.
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**LEGEND:**

- ▬▬▬ WORK SITE
- - - - - A-CLASS HOARDING
- X SITE ACCESS
- ▬▬▬ WORK ZONE
- - - - - PEDESTRIAN MOVEMENT
- 👤 TfNSW ACCREDITED TRAFFIC CONTROLLER
- 👤 SITE PERSONNEL
- SIGNS
- ➔ CONSTRUCTION VEHICLE ACCESS
- ➔ CONSTRUCTION VEHICLE EGRESS

MOBILE ON-SITE TRAINED PERSONNEL TO MANAGE PEDESTRIAN ACTIVITIES DURING TRUCK ACCESS AS REQUIRED

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6:\2023\23082 - 54-58 Beaconsfield Street, Newport\Drawings\CC\23082-V1.3-TGS.dwg  
 Plotted by Brian Bui



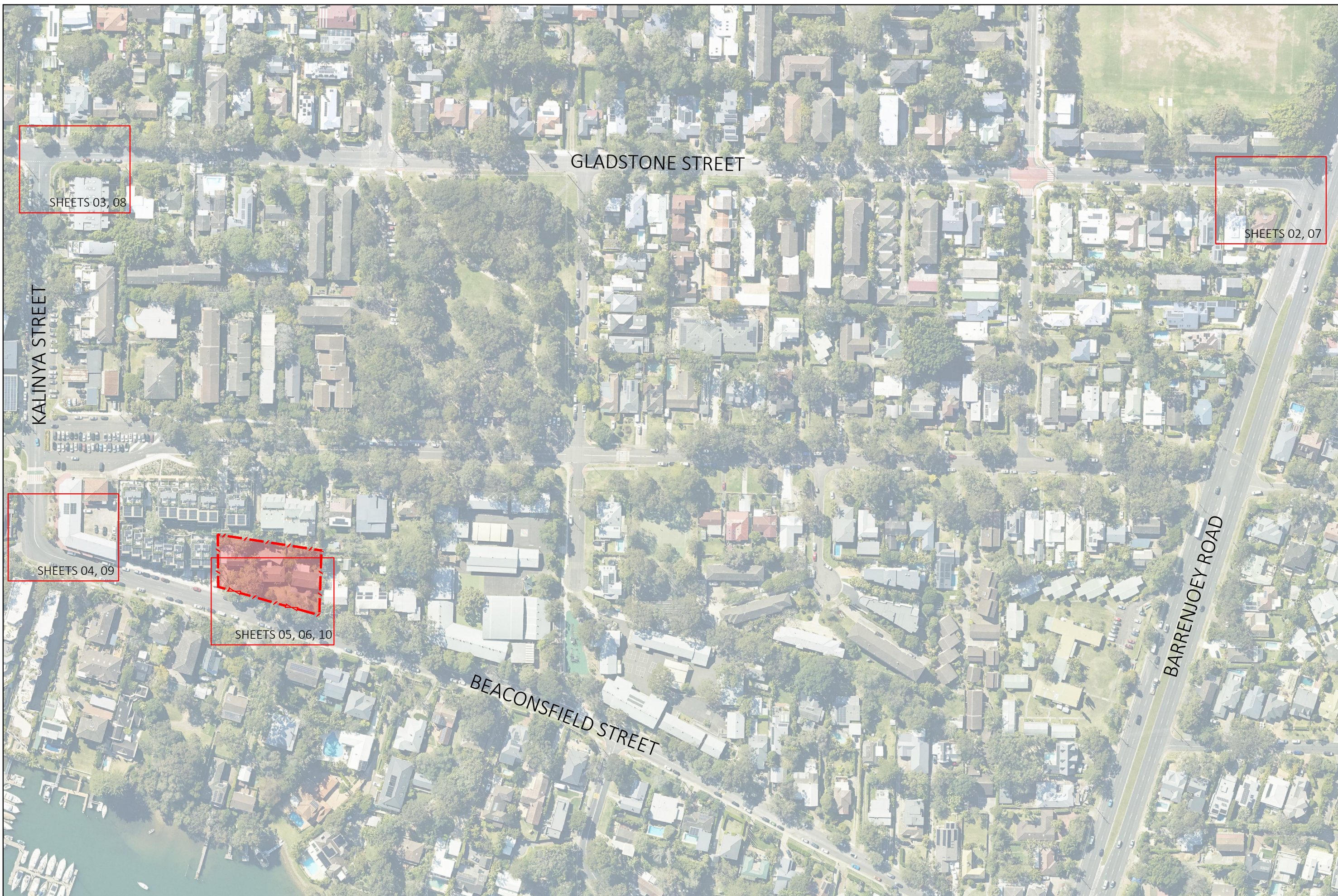


# Attachment 3

## Swept Path Analysis

DRAFT





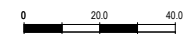
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Plotted by Brian Bui

**54-58 BEACONSFIELD STREET, NEWPORT**  
**AREA OVERVIEW**  
**SWEPT PATH ASSESSMENT**  
DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 01 OF 10

ISSUE DATE 2 December 2024

DESIGNED BY  
B.BUI  
  
SCALE  
A3

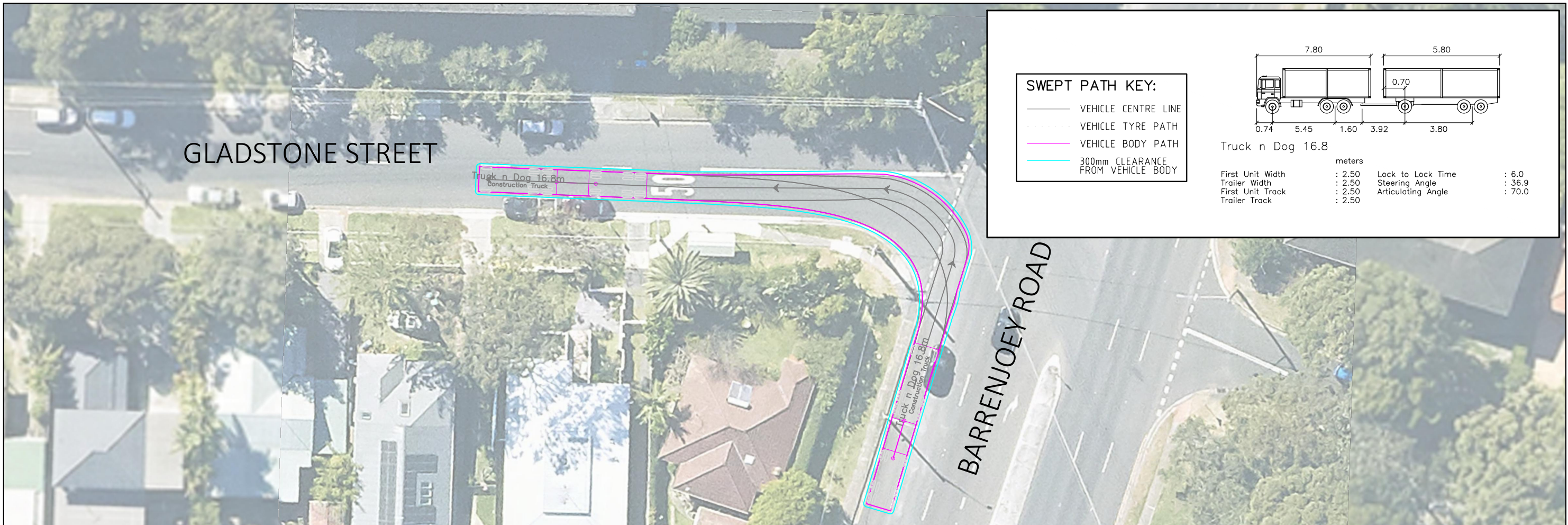


REVIEWED BY  
B.LO  
  
1:2000

DRAWING REFERENCE (SOURCE):  
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NEWPORT\DRAWINGS

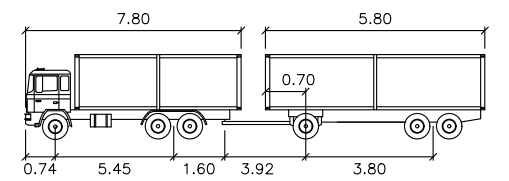






**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



Truck n Dog 16.8

meters	
First Unit Width	: 2.50
Trailer Width	: 2.50
First Unit Track	: 2.50
Trailer Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 36.9
Articulating Angle	: 70.0



G:\2023\23082 - 54-58 Beaconsfield Street, Newport\Drawings\CC\23082-V1.3-SP - CTMP.dwg  
 Plotted by Brian Bui

54-58 BEACONSFIELD STREET, NEWPORT  
 DEMOLITION AND EXCAVATION DURING SCHOOL DAYS  
 SWEEP PATH ASSESSMENT - 16.8m TRUCK & DOG

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 02 OF 10

ISSUE DATE 2 December 2024

DESIGNED BY  
 B.BUI

REVIEWED BY  
 B.LO

SCALE  
 A3



1:400

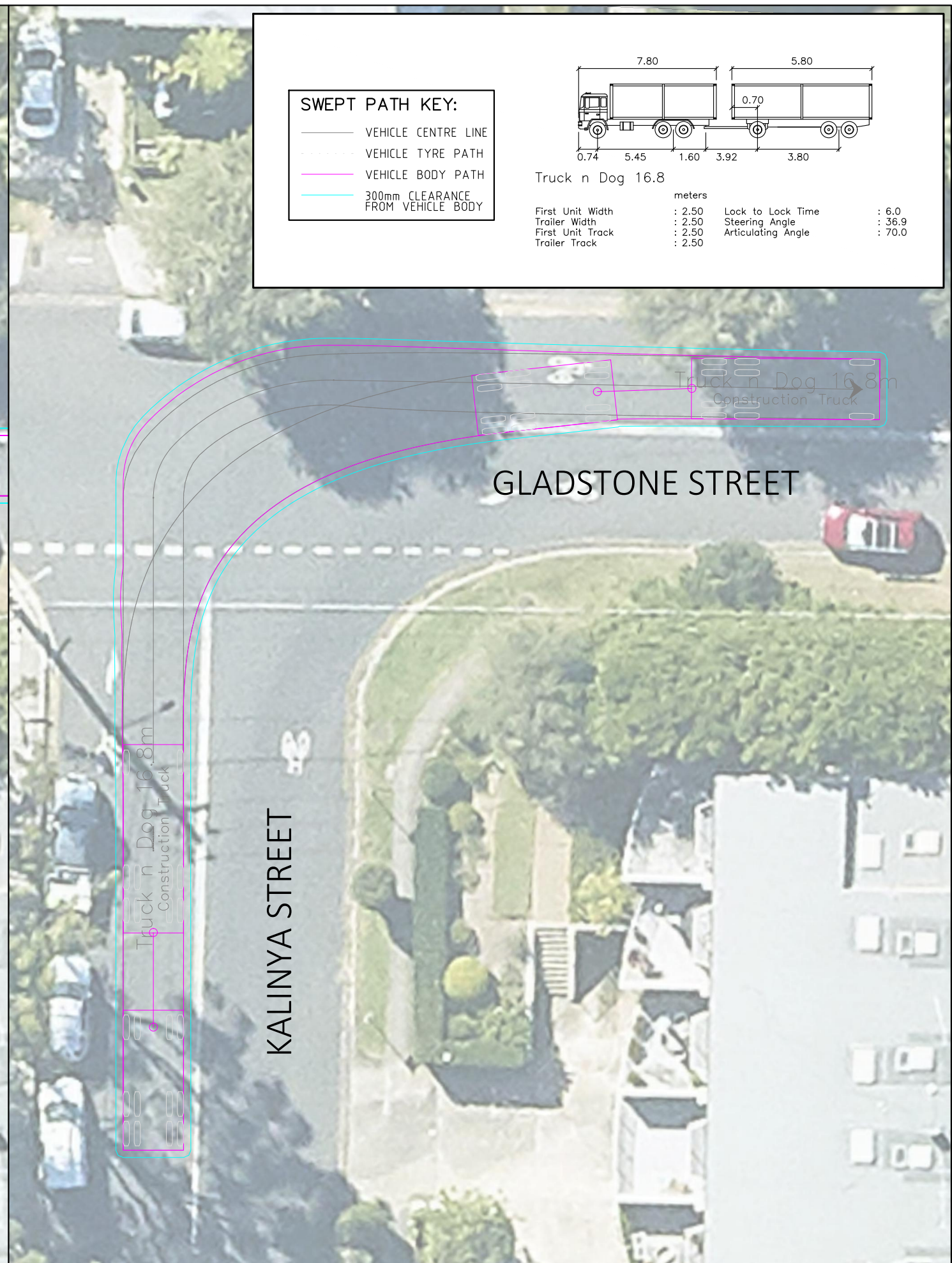
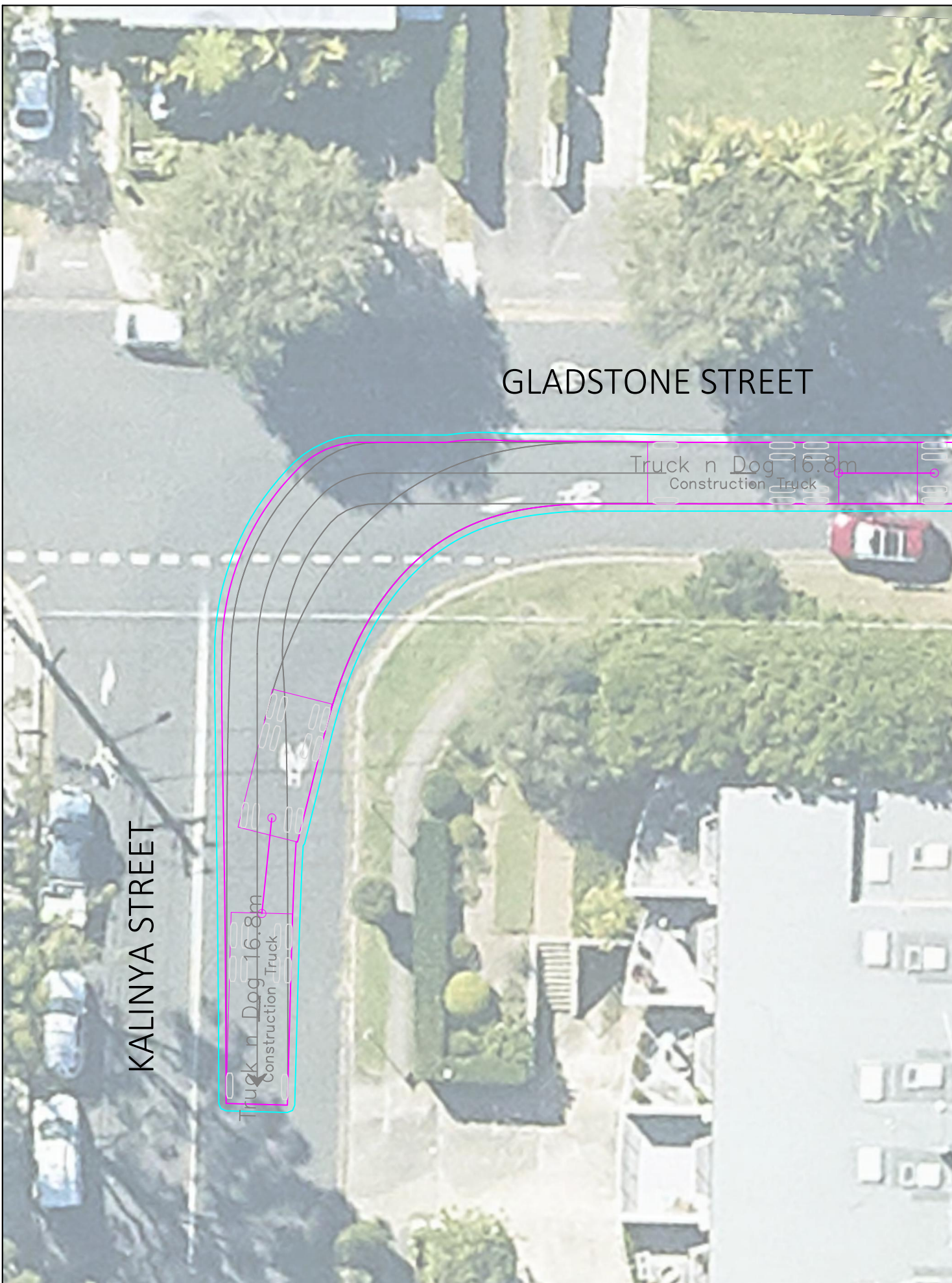
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GenesisTraffic

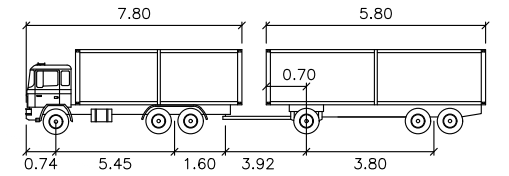


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**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- - - - VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



Truck n Dog 16.8

meters			
First Unit Width	: 2.50	Lock to Lock Time	: 6.0
Trailer Width	: 2.50	Steering Angle	: 36.9
First Unit Track	: 2.50	Articulating Angle	: 70.0
Trailer Track	: 2.50		

54-58 BEACONSFIELD STREET, NEWPORT  
DEMOLITION AND EXCAVATION DURING SCHOOL DAYS  
SWEPT PATH ASSESSMENT - 16.8m TRUCK & DOG

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 03 OF 10

ISSUE DATE 2 December 2024

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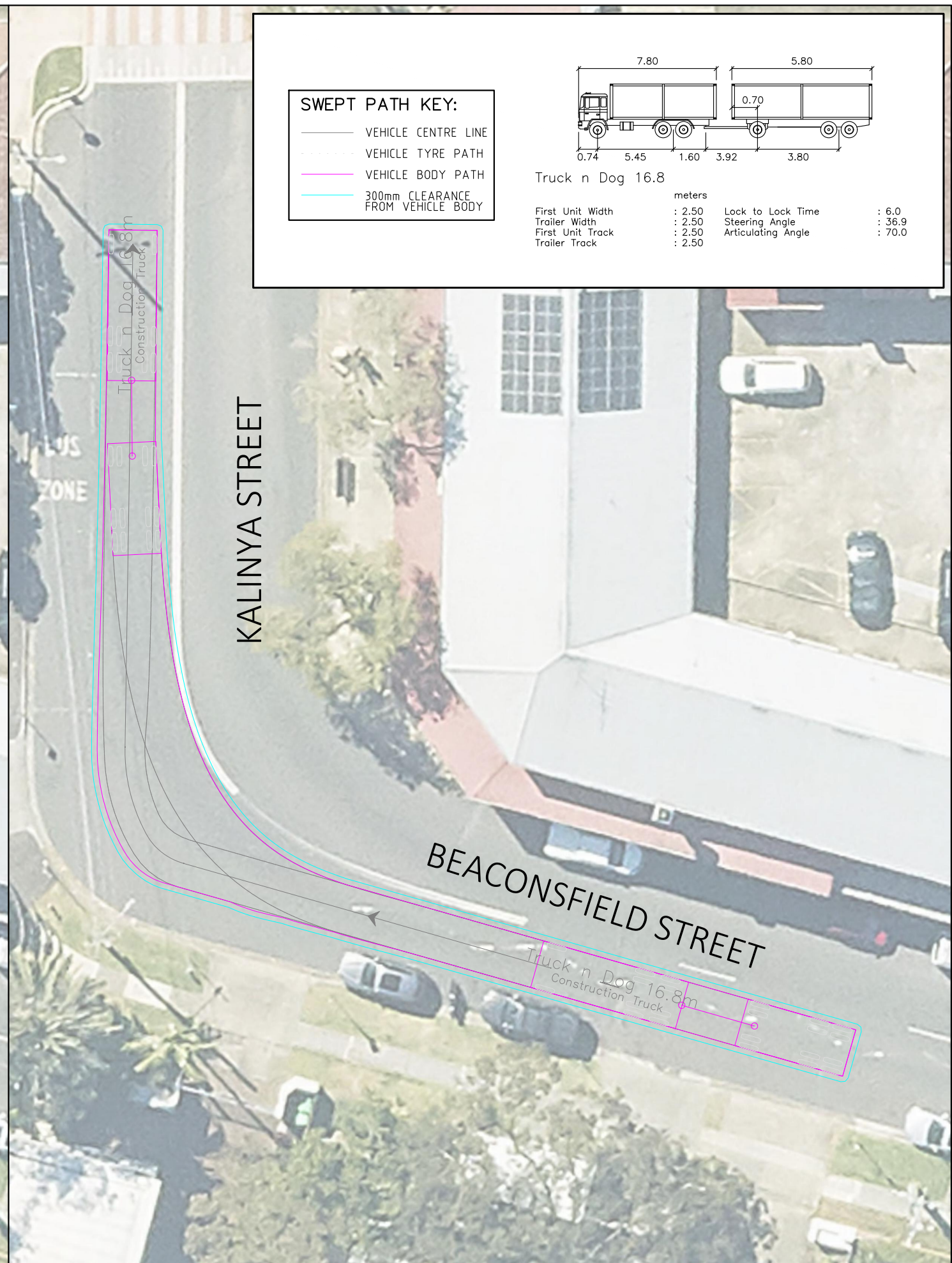
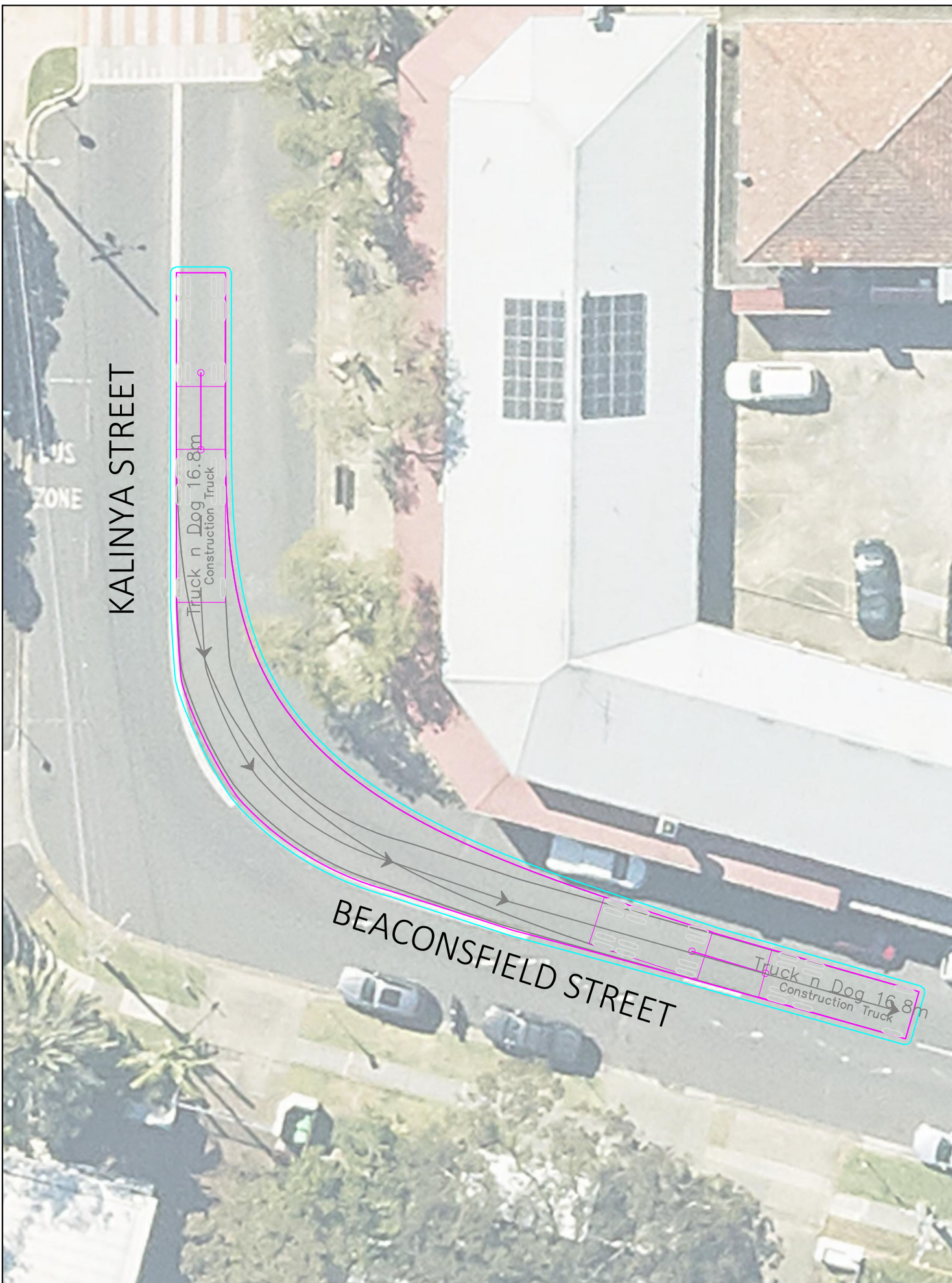
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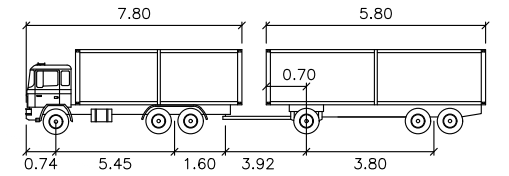


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**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- - - VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



Truck n Dog 16.8

meters			
First Unit Width	: 2.50	Lock to Lock Time	: 6.0
Trailer Width	: 2.50	Steering Angle	: 36.9
First Unit Track	: 2.50	Articulating Angle	: 70.0
Trailer Track	: 2.50		

54-58 BEACONSFIELD STREET, NEWPORT  
DEMOLITION AND EXCAVATION DURING SCHOOL DAYS  
SWEPT PATH ASSESSMENT - 16.8m TRUCK & DOG

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 04 OF 10

ISSUE DATE 2 December 2024

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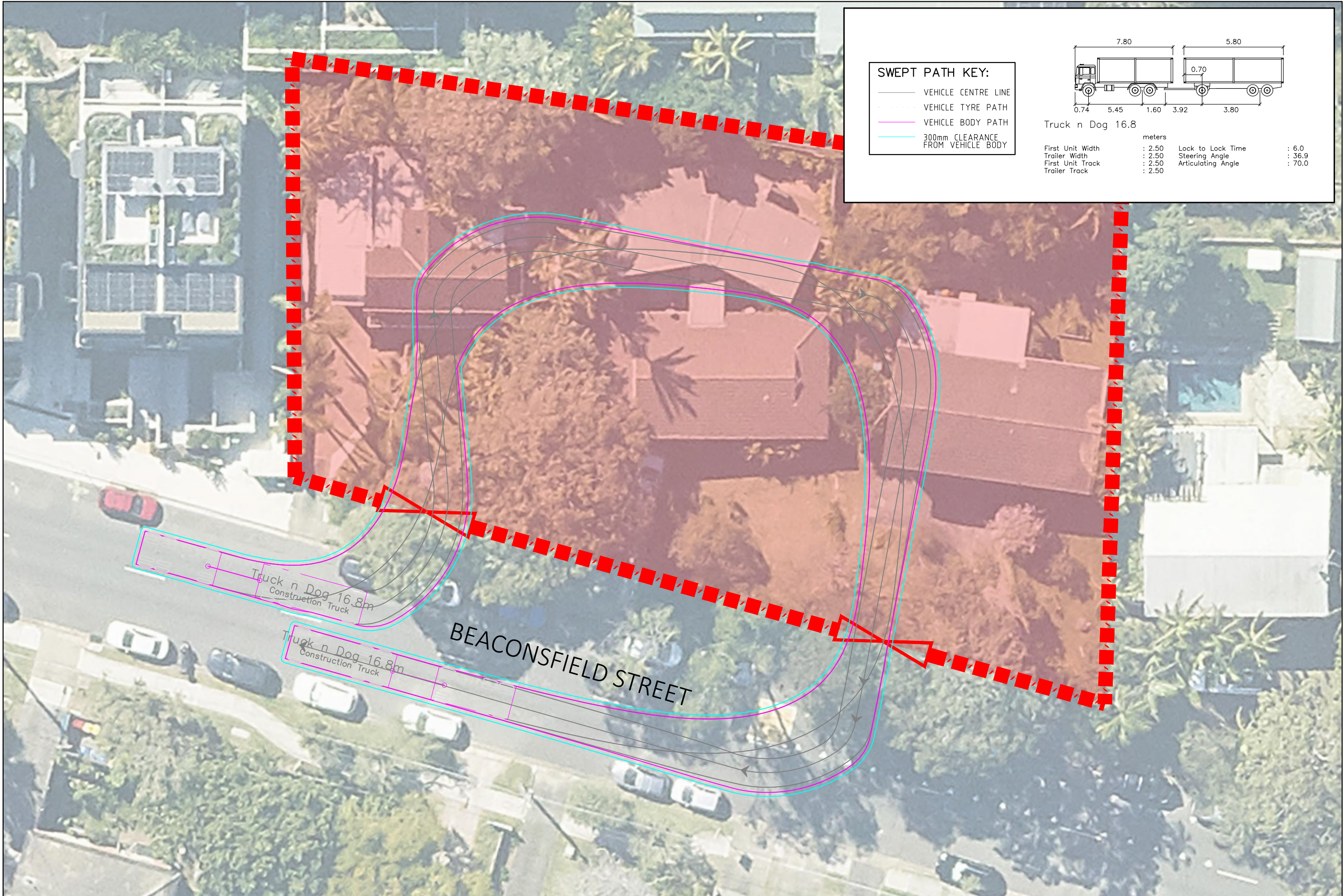
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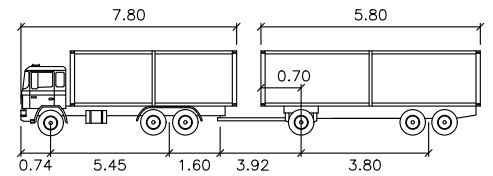
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**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- - - - VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



Truck n Dog 16.8

meters	
First Unit Width	: 2.50
Trailer Width	: 2.50
First Unit Track	: 2.50
Trailer Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 36.9
Articulating Angle	: 70.0

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**54-58 BEACONSFIELD STREET, NEWPORT**  
**DEMOLITION AND EXCAVATION DURING SCHOOL DAYS**  
**SWEPT PATH ASSESSMENT - 16.8m TRUCK & DOG**

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 05 OF 10

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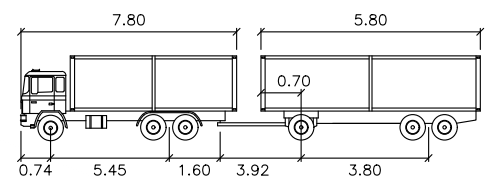






**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- ⋯ VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



Truck n Dog 16.8

meters	
First Unit Width	: 2.50
Trailer Width	: 2.50
First Unit Track	: 2.50
Trailer Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 36.9
Articulating Angle	: 70.0

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54-58 BEACONSFIELD STREET, NEWPORT  
DEMOLITION AND EXCAVATION DURING SCHOOL HOLIDAYS AND SATURDAYS  
SWEPT PATH ASSESSMENT - 16.8m TRUCK & DOG

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 06 OF 10

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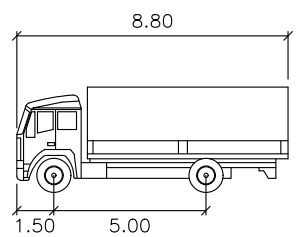


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**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



MRV

Width	: 2.50	meters
Track	: 2.50	
Lock to Lock Time	: 6.0	
Steering Angle	: 34.0	

GLADSTONE STREET



BARRENJOEY ROAD

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54-58 BEACONSFIELD STREET, NEWPORT  
SITE ESTABLISHMENT, CONSTRUCTION AND FITOUT  
SWEPT PATH ASSESSMENT - 8.8m MRV

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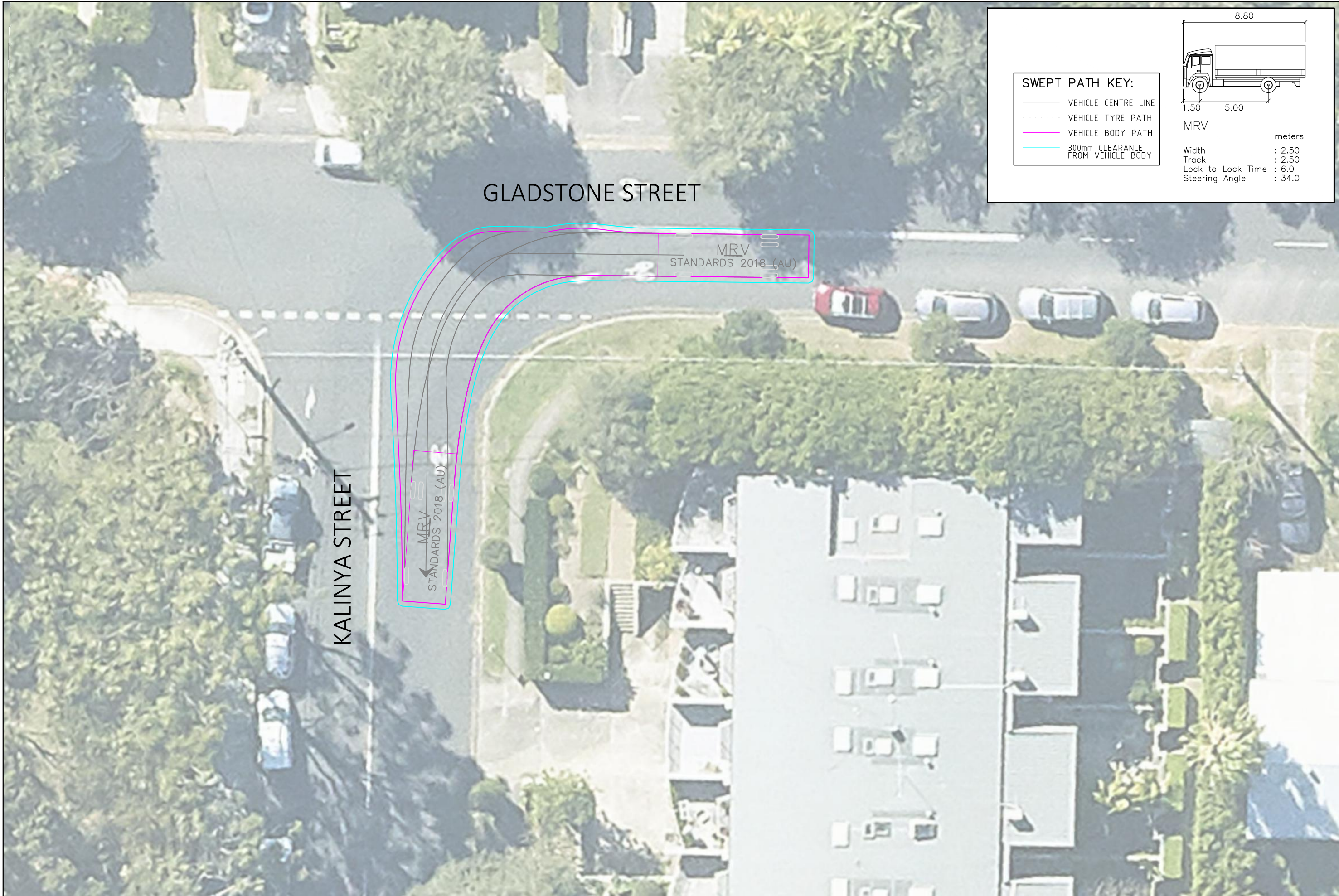


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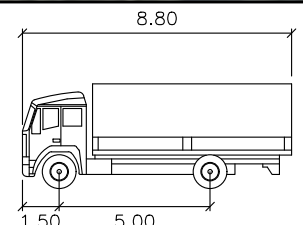






**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- - - VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY



MRV

Width	: 2.50
Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 34.0

meters

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54-58 BEACONSFIELD STREET, NEWPORT  
 SITE ESTABLISHMENT, CONSTRUCTION AND FITOUT  
 SWEPT PATH ASSESSMENT - 8.8m MRV

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SHEET NO. 08 OF 10

ISSUE DATE 2 December 2024

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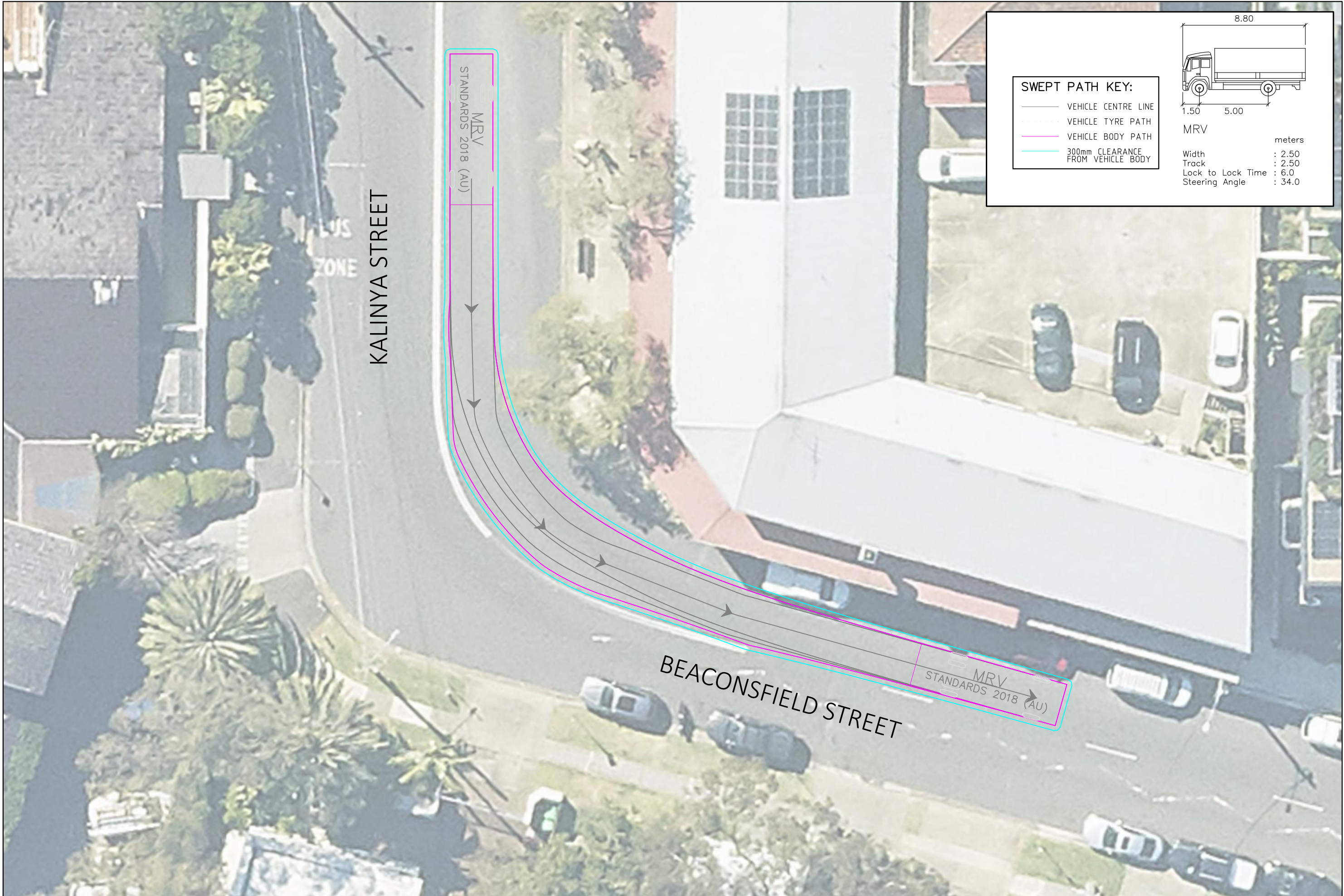


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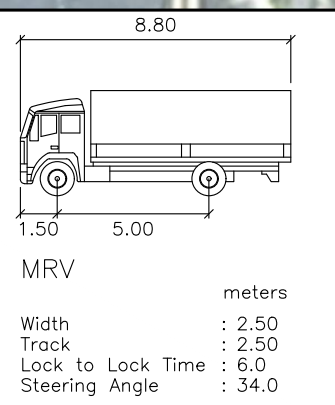






**SWEPT PATH KEY:**

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- VEHICLE BODY PATH
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54-58 BEACONSFIELD STREET, NEWPORT  
 SITE ESTABLISHMENT, CONSTRUCTION AND FITOUT  
 SWEPT PATH ASSESSMENT - 8.8m MRV

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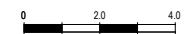
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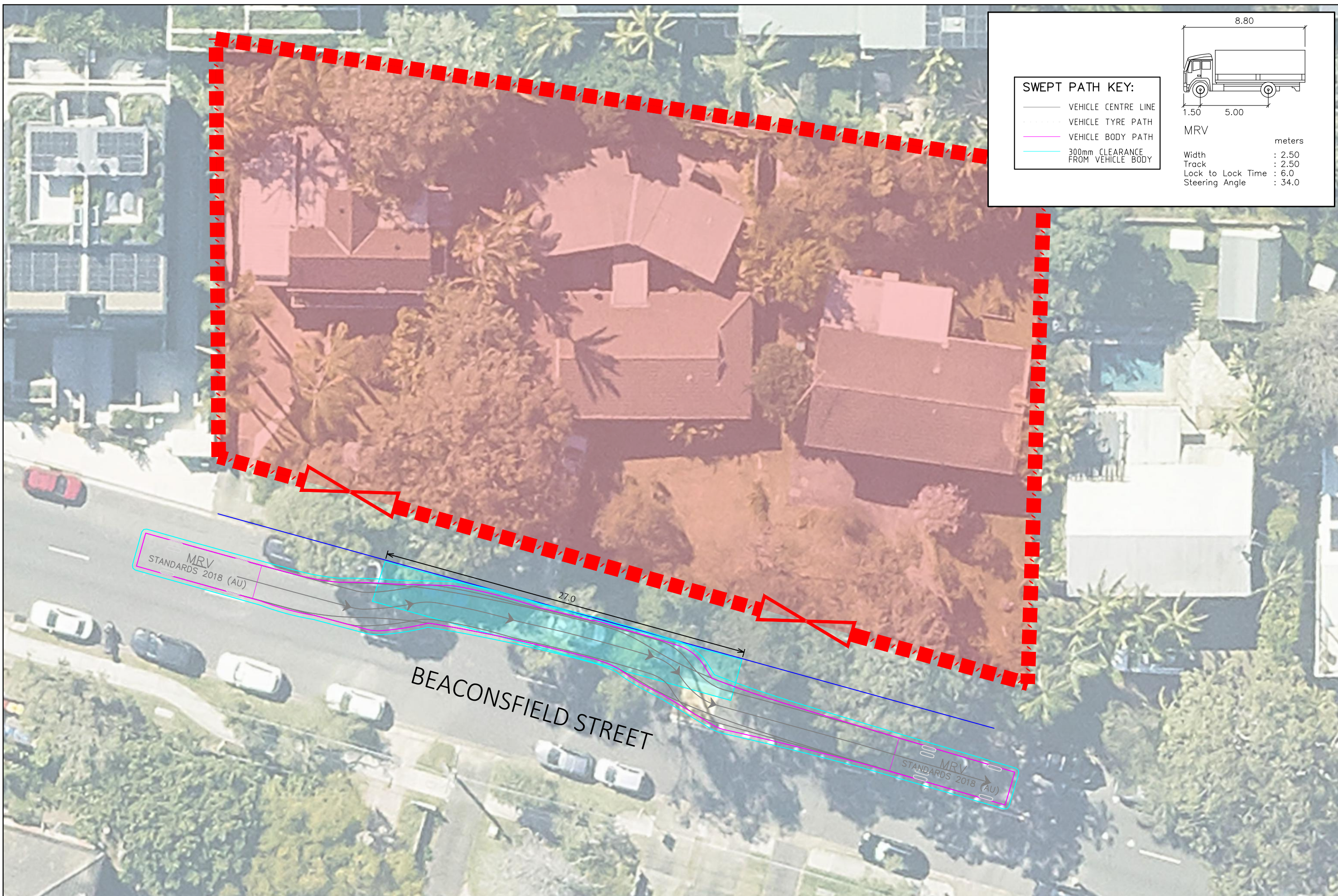


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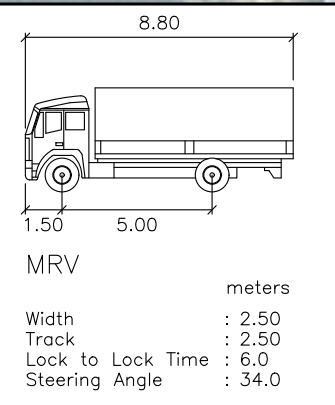
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- SWEPT PATH KEY:**
- VEHICLE CENTRE LINE
  - VEHICLE TYRE PATH
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54-58 BEACONSFIELD STREET, NEWPORT  
SITE ESTABLISHMENT, CONSTRUCTION AND FITOUT  
SWEPT PATH ASSESSMENT - 8.8m MRV

DRAWING REF NO. 23082-V1.3-SP - CTMP

SHEET NO. 10 OF 10

ISSUE DATE 2 December 2024

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