

introduction

The NSW Government outlines that State Environmental Planning Policy No. 65 – Design Quality of Residential Flat Development (SEPP 65) aims to improve design quality of residential flat buildings of three or more storeys and containing 4 or more self contained dwellings. The Policy recognises that the design quality of residential flat development is of significance for environmental planning for the State due to the economic, environmental, cultural and social benefits of high quality design.

SEPP 65 contains ten Design Quality Principles, which form the basis for achieving good design. The Design Quality Principles provide a guide for evaluating the merits of development proposals.

SEPP 65 and Design Quality Principles are implemented through the following items:

- β Environmental Planning and Assessment Regulation 2000 includes a number of specific provisions to complement SEPP 65 to raise the design quality of residential flat development;
- β Residential Flat Design Code - a tool for improving the design of residential flats;
- β Residential Flat Design Pattern Book - a resource of ideas and precedents to guide better design of residential flat development in NSW.

SEPP 65 requires that a registered architect is to verify the design, construction certification and occupation certification of residential flat development. This SEPP 65 review has been prepared by Dr Stephen Lesiuk of Lesiuk Architects.

Clause 5 in Schedule 1 of the Environmental Planning and Assessment Regulation 2000 also requires an SEE, amongst other things to provide:

‘an explanation of the design in terms of the design quality principles set out in Part 2 of State

Environmental Planning Policy No65—Design Quality of Residential Flat Development’

The following provides an assessment of the proposal with respect to the 10 design quality principles

sept. 09 A da + sepp 65 review

date issue

amendments

palm beach village

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1. context

There is an eclectic mix of built form within the Palm Beach locality. Within close proximity to the site there are 2 and 3 storey dwellings and business premises as well as residential flat buildings (in Palm Beach Road) (see photos in Design Report).

There is also three distinctive bands of development on the western escarpment adjacent to Barrenjoey Road, these being at the street level, middle and upper levels of the slope.

The height of the proposal is 4 storeys. Although this height is greater than those existing buildings immediately adjoining the site, the development would still remain in context within the locality as its height and bulk have been nestled into the slope and setback significantly from the street frontage.

This siting of the building bulk away from the site's frontage also respects its location as a transition from a business to a residential land use zone where the front building line requirement changes dramatically from 3.5m to 10m respectively. By demolishing the existing commercial buildings on the site that currently have a nil setback to Barrenjoey Road and setting the bulk of the new proposal back 10m from the site's frontage, the potential visual impacts upon the adjoining dwelling at 1120 Barrenjoey Road would be reduced.

The desired future character for land (including the site) within the Palm Beach Locality includes:

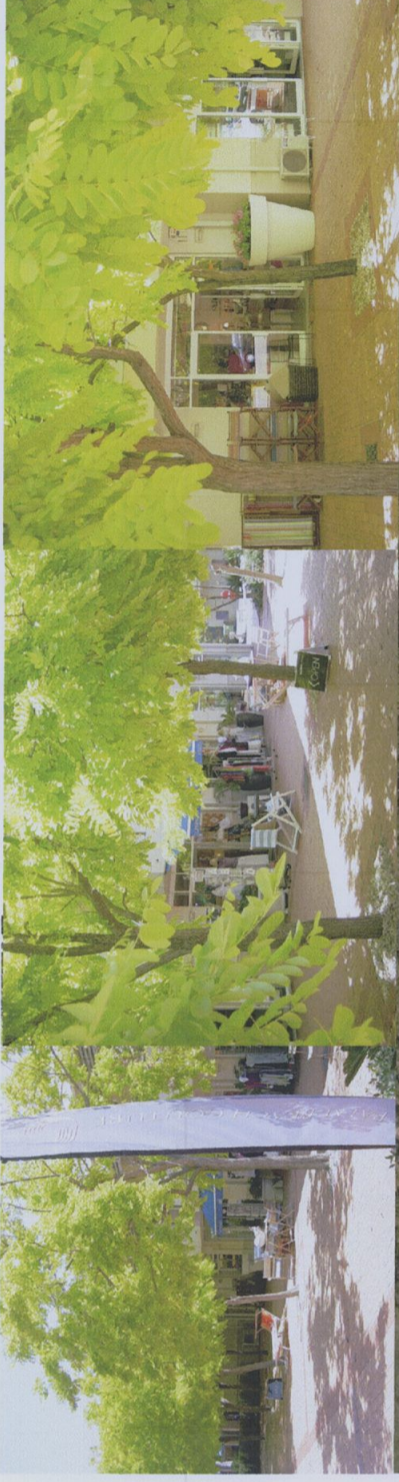
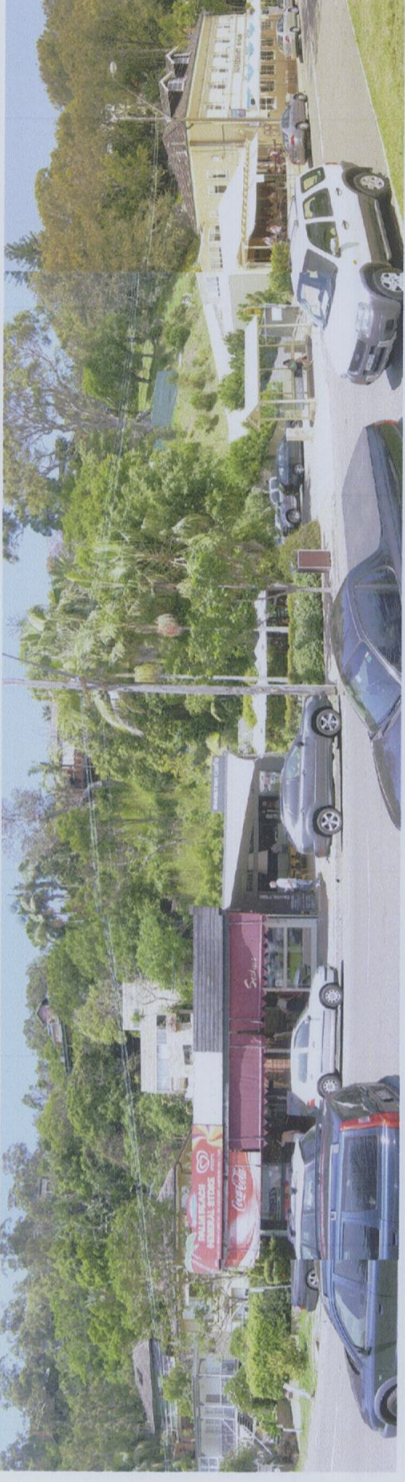
Development on slopes will be stepped down or along the slope to integrate with the landform and landscape, and minimise site disturbance; and

The design, scale and treatment of future development within the commercial centres will reflect a 'seaside-village' character through building design, signage and landscaping, and will reflect principles of good urban design. Landscaping will be incorporated into the building design. Outdoor café seating will be encouraged.

The design of the building nestles into the slope of the site and steps down towards its Barrenjoey Road frontage. In comparison to the previous STH development (N0088/04) refused on the site, the proposal has significantly reduced the site disturbance by proposing only 1 level of basement car parking.

The materials and finishes proposed would consist of lightweight fixtures including stained timber blinds, timber louvres, metal roofing and natural 'earth' tones. The combination of these features provides the proposal with those coastal elements envisaged for development within this locality.

Given all of the above reasons, the proposal would remain in context with the existing development and the desired future character of the locality.



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