5th August 2025

Mr & Mrs Wang
231-233 McCarrs Creek Road,
Church Point NSW 2104

RE: Sight Line Analysis at the Proposed Driveway
Residential Dwelling House at 231-233 McCarrs Creek Road, Church Point

ParkTransit (PT) have been engaged by Mr & Mrs Wang to assess the sight line distance available at the driveway associated with the proposed residential dwelling house located at 231-233 McCarrs Creek Road, Church Point.
The proposal involves the construction of a three-storey residential building accommodating five bedrooms. As part of the proposal, an enclosed double garage will be provided on-site, accessible via a new driveway located along the sole frontage of McCarrs Creek Road.

Reviewed Plans

As part of the review process, PT have undertaken the sight line assessment of the proposed driveway with reference to the following drawings prepared by S&E Design Studio.

A copy of the following plans is presented in **Attachment A**:

- Site Pan - Sheet Number 01, Revision Number E;
- Entry Floor Plan - Sheet Number 02, Revision Number E;
- Middle Floor Plan - Sheet Number 03, Revision Number E;
- First Floor Plan - Sheet Number 04, Revision Number E;
- Elevations - Sheet Number 10, Revision Number E; and
- Driveway Gradient - Sheet Number 23, Revision Number E.

The sight line analysis was undertaken in accordance with AS2890.1-2004 (The Standard).

Sight Distance Assessment

Section 3.2.4 of AS2890.1- 2004 (the Standard), specifies the recommended sight distance associated with the driveway. The sight distance requirement is prescribed in accordance with the posted speed limit along the frontage road.

The proposed site is accessible via a new driveway located along the sole frontage on McCarrs Creek Road. The following map shows the hierarchy of the surrounding road network as classified by Transport for New South Wales (formerly known as Road and Maritime Services (RMS).

Figure 1-Surrounding Road Network (Source TfNSW Website)

The section of McCarrs Creek Road located along the site frontage is classified as a Regional Road and the carriageway comprises one traffic lane in each direction. There is no provision of a footpath on either side of the carriageway, and unrestricted on-street parking is permitted within the informalised indented bays along the carriageway. In the immediate vicinity of the subject site, McCarrs Creek Road has a posted speed limit of 50 km/h and includes the provision of two flat-top speed humps. These speed humps are located at a distance of 120 metres apart. The location of the existing speed humps is shown in the figure below:

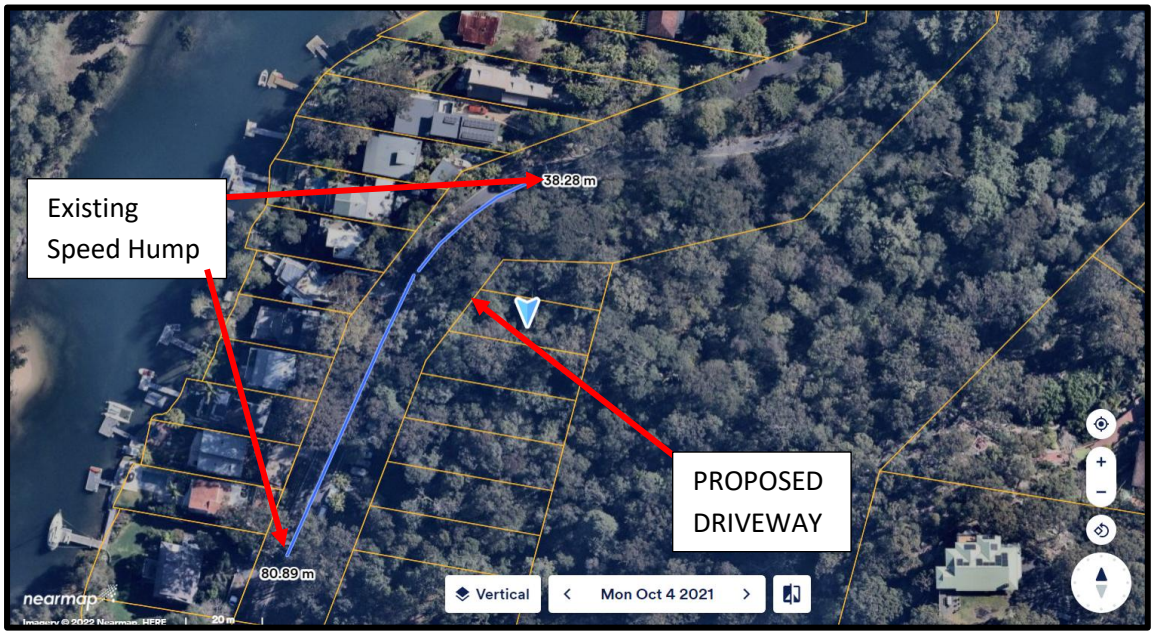


Figure 2-Speed Hump Location (Image Source Nearmap)

As stated earlier, the proposed development will be accessible via a new driveway located along McCarrs Road, which have a posted speed limit of 50kph. A site visit was undertaken during the daytime on 14th March 2022. The weather conditions were fine, and the visibility was clear.

It is expected that the operating speed of this stretch of road is likely to be less than 50kph as the speed humps themselves will require an approach speed of 25kph. Considering the distance between each speed hump is only 120 metres and both humps require an approach speed of 25 kph, we should expect the operating speed to be lower than the posted speed limit.

During our site visit, we observed that a majority of motorists travelling along the section of McCarrs Creek Road located outside the site frontage, were driving at less than 40kph – thus confirming the existing speed hump located along the site frontage was effective in slowing down the motorists.

Figure 3.2 of The Standard outlines the required sight distances for each tabulated speed. In accordance with Figure 3.2 of the Standard, the sight distance requirement should be established with the prevailing speed environment (i.e. 85th percentile speed) and not the posted speed limit. As motorists were travelling at a reduced speed of less than 40kph and thus, it would be appropriate to assess the sight distance requirement based on travel speed instead of the posted speed limit (i.e. 40kph instead of 50kph). For domestic driveways, Section 3.2.4 of the Standard specifies a minimum distance of 30 metres for streets having a posted speed limit of 40kph.

Additionally, a number of mature trees were located at the top of the bank adjacent to the edge of the bitumen (on McCarrs Creek Road). These trees have long trunks and are unlikely to block the visibility of the motorists exiting the subject site. Therefore, the presence of these trees is highly unlikely to have any detrimental impact on the visibility of the motorists exiting the subject site.

In accordance with the guidelines presented in AS2890.1-2004 Sight distance measurements were undertaken and are presented in the figure below:

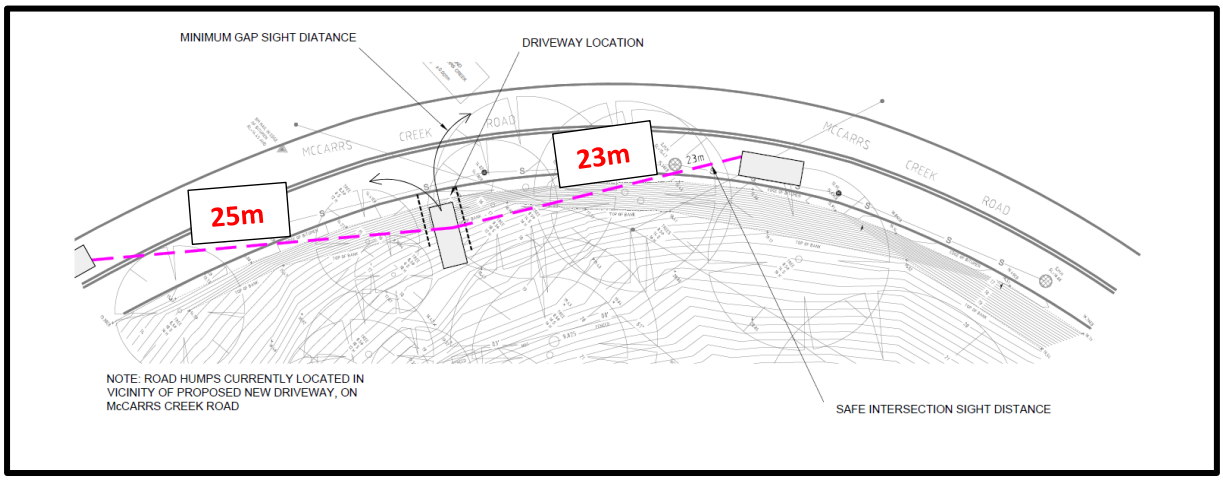


Figure 3- Sight Distance at the Proposed Driveway

It is evident that the sight distance for the motorists exiting the proposed residential dwelling is marginally less than the recommended by the Standard (i.e. 30m). Generally, to improve/enhance the overall visibility for the motorist accessing the development, one (or a combination) of the following measures are considered:

- Explore opportunities to introduce the proposed driveway at an alternate location. In this case, there is a sole frontage servicing the site (i.e. McCarrs Creek Road) and also, limited visibility is available on either side of the driveway - therefore, relocating it to an alternate location will not result in any improvement and thus it is not considered appropriate;
- Introduce an engineered solution to improve the visibility of the residents exiting the residential development and approaching the motorists. Generally, a combination of advanced warning signage and a convex mirror are used to improve this visibility. Therefore, we recommended an advanced warning signage should be implemented on McCarrs Creek Road (in both the Northbound and Southbound directions) informing motorists of the presence of the hidden driveway servicing the site. Additionally, a convex mirror should be introduced (preferably) on the southern end of the driveway to improve the visibility of the motorists travelling in the southbound direction on McCarrs Creek Road. However, we recommend a detailed Road Safety Audit should be undertaken post-construction of the driveway to assess the proposed recommendation and identify any potentially hazardous situation.

Furthermore, as part of this analysis, a review of the crash data along the section of McCarrs Creek Road located along the site frontage was undertaken. The NSW Centre for Road Safety collects crash and casualty data periodically, which is publicly available. A review of the latest crash data from 2019-2023 indicates no crashes were recorded on McCarrs Creek Road, which indicates the local road is operating relatively safely. The Figure below provides the crash location and severity of these crashes recorded in the area.

Figure 4 - Crash data (Source NSW Centre for Road Safety)

Lastly, the proposed driveway will be utilised by the residents who are highly likely to be familiar with the site constraints and its access arrangement and therefore, are anticipated to exercise due care while accessing the site.

The motorists travelling along the section of McCarrs Creek Road located outside the site frontage travel at a lower speed, and the local road network operates safely. We do note, that the sight distance for the motorists exiting the proposed residential dwelling is marginally less than the recommended by the Standard. However, the reduced sight distance can be compensated for by the introduction of an engineered solution involving advanced warning signs and the convex mirror.

In this regard, following the introduction of the proposed driveway, McCarrs Creek Road is likely to continue to operate similarly to the existing condition, and therefore, the proposed driveway is unlikely to have any detrimental impact on the operation of the local road. However, we recommend that a detailed Road Safety Audit be undertaken post-construction of the driveway to assess the proposed recommendation and identify any potentially hazardous situations.

Conclusion and Recommendation

The recommended measure, including advanced warning signs and a convex mirror, is highly likely to improve the overall visibility of both the motorists travelling on McCarrs Creek Road and the residents accessing the site. Following the introduction of the proposed driveway, McCarrs Creek Road is likely to continue to operate similarly to the existing condition, and therefore, the proposed driveway is unlikely to have any detrimental impact on the operation of the local road.

Sincerely,

Abdul Muneeb Khan Mohammad
B.E. (Civil), Masters in Engineering (Transport System Engineering)
Member, Australian Institute of Traffic Planning and Management
Senior Traffic Engineer
ParkTransit Australia
+61 (0) 431 084 571

Attachment A – Architectural Plans

STORMWATER TO HYDRAULIC ENG'S DETAILS

BUSHFIRE ATTACK LEVEL (BAL)
FLAME ZONE CONSTRUCTION REQUIRED
(BAL IN ACCORDANCE WITH AS3959-2009)

TRADES & SUPPLIERS TO COMPLY WITH BAL LEVEL IN ACCORDANCE WITH AS3959-2009

LEGEND

- WATER MAIN
- TELSTRA PILLAR
- TELSTRA MARKER POST
- TELSTRA PIT
- TELEGRAPH POLE
- POWER POLE
- ELECTRICAL PILLAR
- POWER LIGHT POLE
- LIGHT POLE
- HYDRANT
- RECYCLED WATER
- WATER METER
- STOP VALVE
- WATER TAP
- GAS METER
- GAS DIRECTION MARKER
- GAS INSPECTION POINT
- SEWER MANHOLE
- STREET SIGN
- PRAM CROSSING
- VEHICLE CROSSING
- SEWER VENT
- SEWER LAMPHOLE
- SEWER INSPECTION POINT
- SUBSOIL DRAIN
- STORMWATER PIT
- STORMWATER GRATE
- SURFACE INLET PIT
- LINTEL-KERB INLET PIT
- LINTEL-KERB INLET PIT WITH GRATE
- STORMWATER PIPE INCLUDING PIPE SIZE (Ø 0.225)
- HEADWALL
- CLOTHES LINE
- TREE: Ø DIAMETER, S SPREAD, H HEIGHT
- E — OVERHEAD ELECTRICITY LINE
- T — OVERHEAD TELECOM LINE
- S — SEWER LINE

AREA LOT 34
VIDE DP 20097: 417.3 m²
BY CALC : 422.7 m²

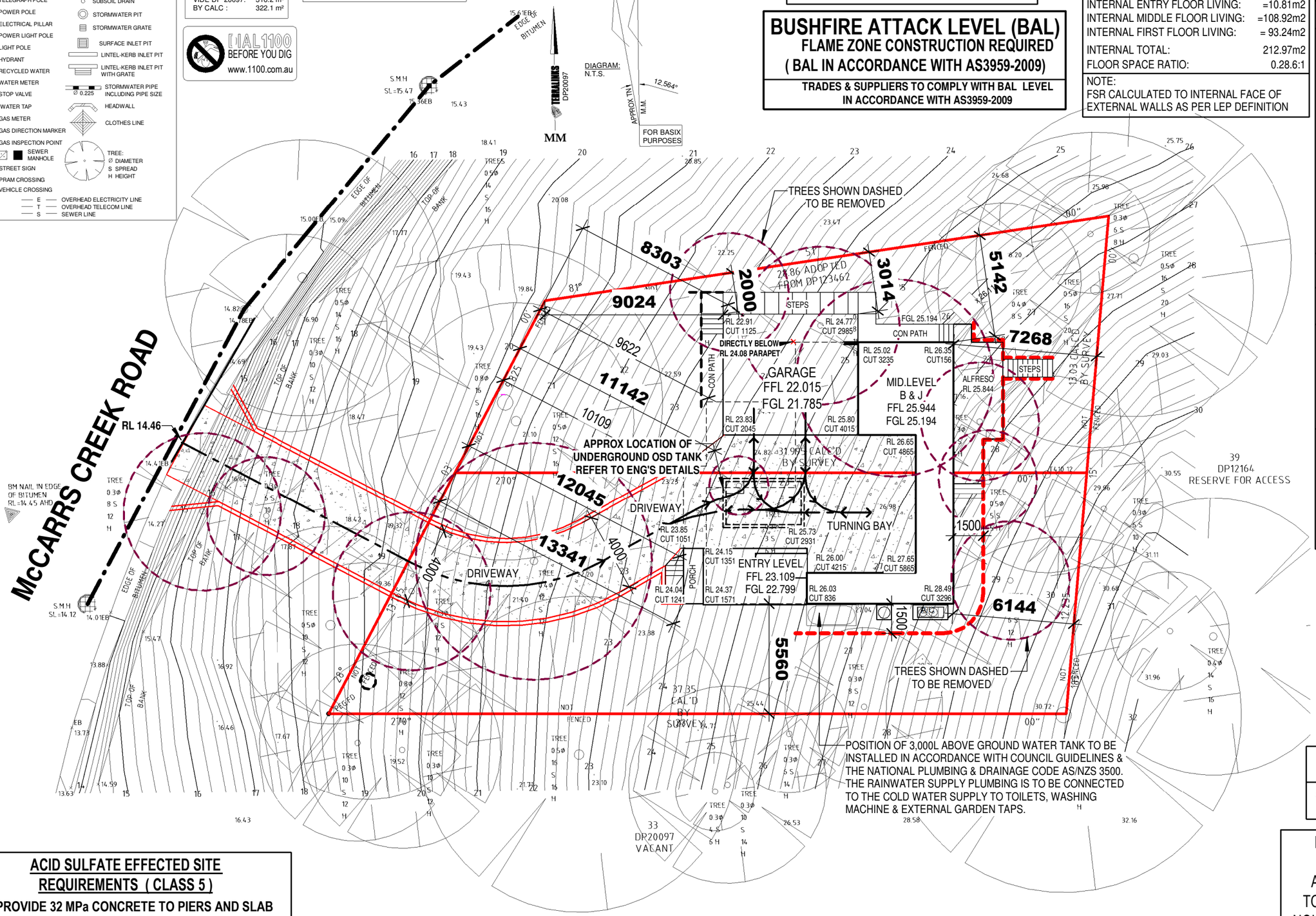
AREA LOT 35
VIDE DP 20097: 316.2 m²
BY CALC : 322.1 m²

DIAL 1100 BEFORE YOU DIG
www.1100.com.au

ORIGIN OF LEVELS :
PM 53844 RL=15.027 (AHD) FOUND ADJACENT TO No.214 MCCARRS CREEK ROAD, CHURCH POINT
ACCURACY OF ORIGIN : ± 0.001m

DIAGRAM: N.T.S.

FOR BASIX PURPOSES



FSR CALCULATIONS

SITE AREA:	744.84m2
HOUSE AREAS:	
INTERNAL ENTRY FLOOR LIVING:	=10.81m2
INTERNAL MIDDLE FLOOR LIVING:	=108.92m2
INTERNAL FIRST FLOOR LIVING:	= 93.24m2
INTERNAL TOTAL:	212.97m2
FLOOR SPACE RATIO:	0.28.6:1

NOTE:
FSR CALCULATED TO INTERNAL FACE OF EXTERNAL WALLS AS PER LEP DEFINITION

SITE DETAILS

LOT NUMBER:	34 & 35
DP NUMBER:	20097
SITE AREA:	**744.84m2**

BUILDING AREAS

MIDDLE LEVEL	133.09 m²
FIRST FLOOR	104.86 m²
GARAGE	41.80 m²
MIDDLE BALCONY	24.33 m²
ALFRESCO	16.46 m²
ENTRY LEVEL	15.63 m²
PORCH	2.85 m²
Grand total	339.00 m²

SITE COVERAGE:	295.01m2 = 39.60%
PRIVATE OPEN SPACE:	192.22m2

LANDSCAPE:
TOTAL AREA OF VEGETATION: 449.83m2 = 60.39%

STORMWATER:
RAINWATER TANK SIZE: = 3,000 litre

ROOF AREA CONNECTED TO RAINWATER TANK: (82) % MIN MIN- 148.00m2 **(to eng's details)**
RAINWATER USES: GARDEN/TOILET/LAUNDRY

- **ALL CONSTRUCTION WORK TO COMPLY WITH THE NCC AND ALL RELEVANT AUSTRALIAN STANDARDS**
-HOUSE LEVELS ARE APPROXIMATE ONLY AND MUST BE DETERMINED ON SITE BY BUILDER PRIOR TO CONSTRUCTION.
-DRIVEWAY GRADIENT SHALL COMPLY WITH COUNCILS SPECIFICATIONS - GARAGE LEVEL MUST BE CHECKED ON SITE SO AS TO COMPLY WITH COUNCILS DRIVEWAY SPECIFICATIONS

REFER TO ARBORIST REPORT
BY: BLUES BROS ARBORICULTURE
DATED : 14.04.25

MARINE RATING "R4"
ALL TRADES & SUPPLIERS TO COMPLY WITH THE BCA & ALL RELEVANT AUST STANDARDS

EXTERNAL A/C UNIT, A/C VENTS AND DROPPERS ARE APPROX. ONLY AND MAY BE RE-POSITIONED ON SITE TO SUIT DIFFERENT CONSTRUCTION OR NOISE REQUIREMENTS. FINAL POSITION TO BE DETERMINED BY A/C CONTRACTOR

ACID SULFATE EFFECTED SITE REQUIREMENTS (CLASS 5)
PROVIDE 32 MPa CONCRETE TO PIERS AND SLAB WITH SULFATE RESISTING CEMENT
PROVIDE 0.3mm HIGH IMPACT VAPOUR MEMBRANE

All dimensions shown are to frame, underside of roof trusses and FFL (concrete to the underside of floor joist
FFL denotes - concrete level
floor joist + selected flooring
All construction work to comply with the NCC and all relevant Australian standards

All previous issues are to be discarded only these plans are to be read

rev	amendment	date
A	FD	16.02.25
B	AMENDMENT	07.03.25
C	AMENDMENT	18.03.25
D	AMENDMENT	06.04.25
E	FFD DA SUBMISSION	29.05.25

S&E Design Studio
Email - suljobr@hotmail.com Mob - 0422 461 767

for / client: MR AND MRS WANG

SITE PLAN

location: LOT34/35, 231-233 McCARRS CREEK RD CHURCH POINT NORTHERN BEACHES COUNCIL

DO NOT SCALE DRAWINGS

stage:	project no:	dwg no:
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drawn - SB	date: 16.02.25	scale: 1 : 200
checked - SB		

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LEGENDS

- 720 — LIFT OFF DOOR HINGE
- CJ * — CONTROL BRICK JOINT
- MH — CEILING MAN HOLE
- SA — SMOKE ALARM
- DP — DOWN PIPE
- AC DUCTS

NOTE:
PROVIDE ENG'S DETAILS FOR ALL:
CONCRETE SLABS, FOOTINGS & STEEL BEAM

ALL EXHAUST FANS TO BE DUCTED EXTERNALY

HEBLE POWERWALL SYSTEM TO BE ERECTED IN ACCORDANCE TO MANUFACTURES DETAILS AND INSTRUCTIONS

2340H INTERNAL DOOR THROUGHOUT

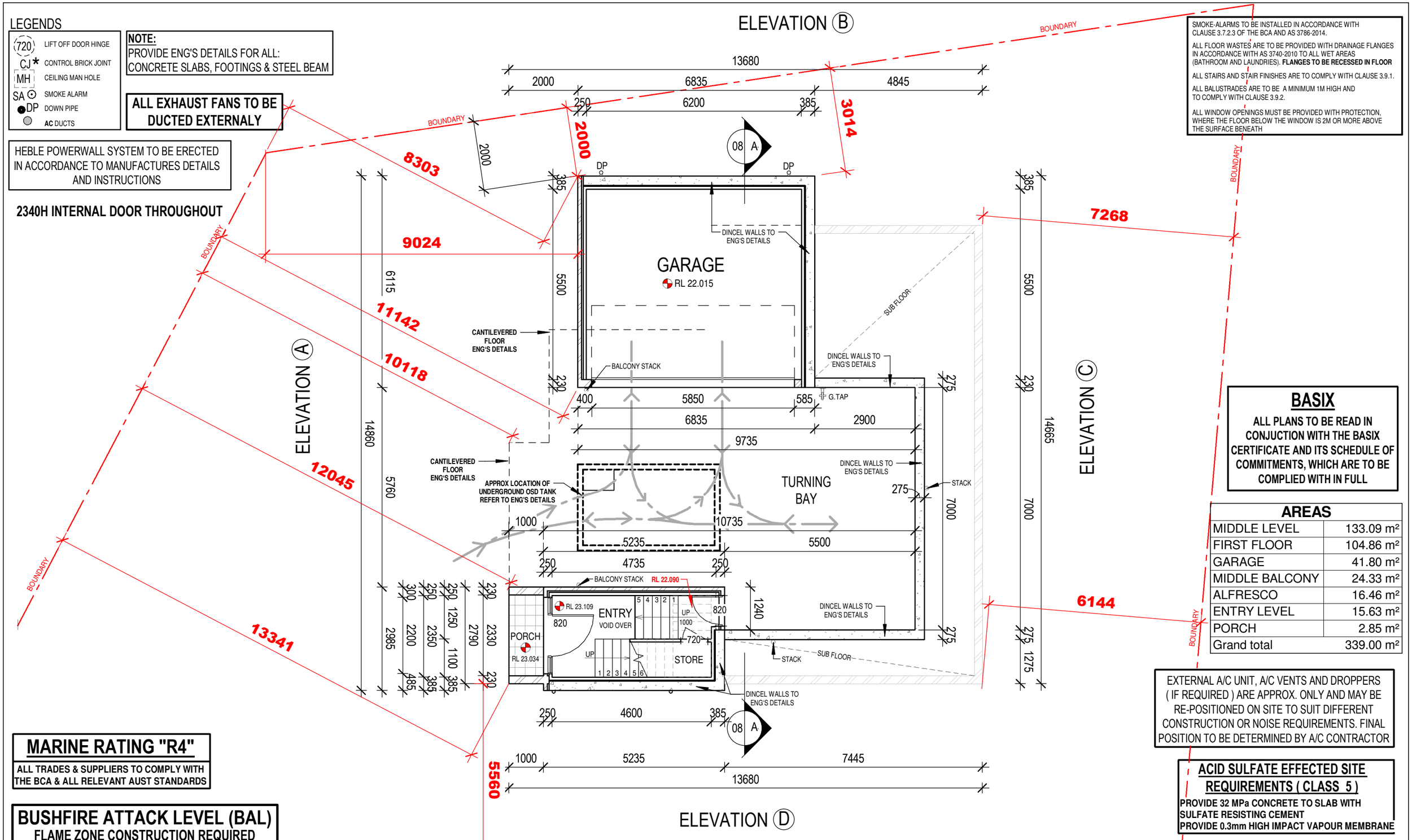
SMOKE-ALARMS TO BE INSTALLED IN ACCORDANCE WITH CLAUSE 3.7.2.3 OF THE BCA AND AS 3786-2014.

ALL FLOOR WASTES ARE TO BE PROVIDED WITH DRAINAGE FLANGES IN ACCORDANCE WITH AS 3740-2010 TO ALL WET AREAS (BATHROOM AND LAUNDRIES). **FLANGES TO BE RECESSED IN FLOOR**

ALL STAIRS AND STAIR FINISHES ARE TO COMPLY WITH CLAUSE 3.9.1.

ALL BALUSTRADES ARE TO BE A MINIMUM 1M HIGH AND TO COMPLY WITH CLAUSE 3.9.2.

ALL WINDOW OPENINGS MUST BE PROVIDED WITH PROTECTION, WHERE THE FLOOR BELOW THE WINDOW IS 2M OR MORE ABOVE THE SURFACE BENEATH



BASIX

ALL PLANS TO BE READ IN CONJUCTION WITH THE BASIX CERTIFICATE AND ITS SCHEDULE OF COMMITMENTS, WHICH ARE TO BE COMPLIED WITH IN FULL

AREAS	
MIDDLE LEVEL	133.09 m²
FIRST FLOOR	104.86 m²
GARAGE	41.80 m²
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PROVIDE 0.3mm HIGH IMPACT VAPOUR MEMBRANE

ENSURE THAT RECESSED PUDDLE FLANGES ARE INSTALLED TO ALL WASTE PIPES AS PER AS.3740

MARINE RATING "R4"

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S&E Design Studio
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for / client: MR AND MRS WANG

ENTRY FLOOR LEVEL

location: LOT34/35, 231-233 McCARRS CREEK RD CHURCH POINT NORTHERN BEACHES COUNCIL

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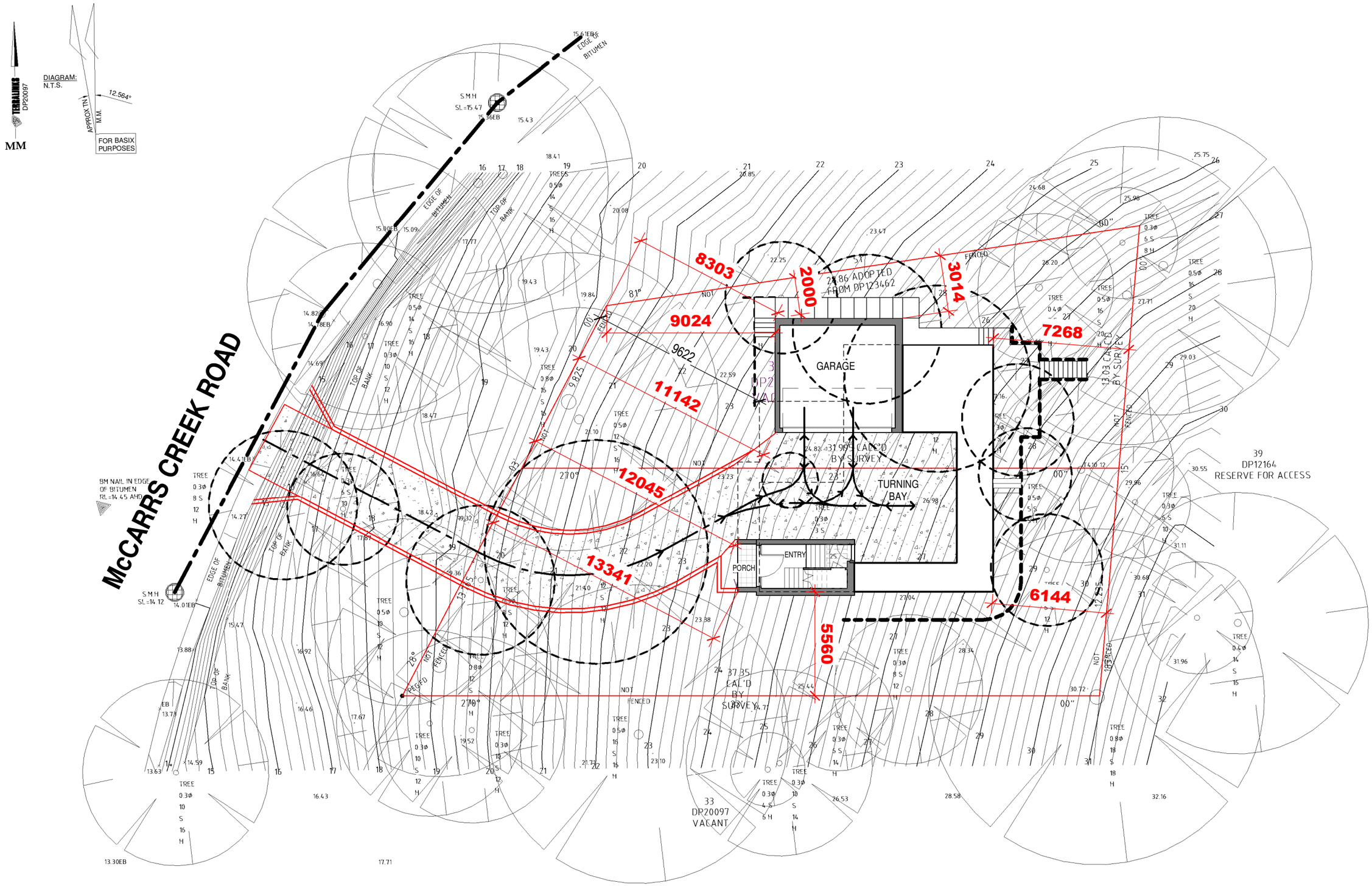

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for / client
MR AND MRS WANG

SITE PLAN - ENTRY LEVEL FLOOR PLAN

location
LOT34/35, 231-233 McCARRS CREEK RD
CHURCH POINT
NORTHERN BEACHES COUNCIL

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LEGENDS

(720)	LIFT OFF DOOR HINGE
CJ *	CONTROL BRICK JOINT
MH	CEILING MAN HOLE
SA	SMOKE ALARM
DP	DOWN PIPE
	AC DUCTS
1.2	CEILING FAN NUMBER DENOTES DIAMETER

NOTE:
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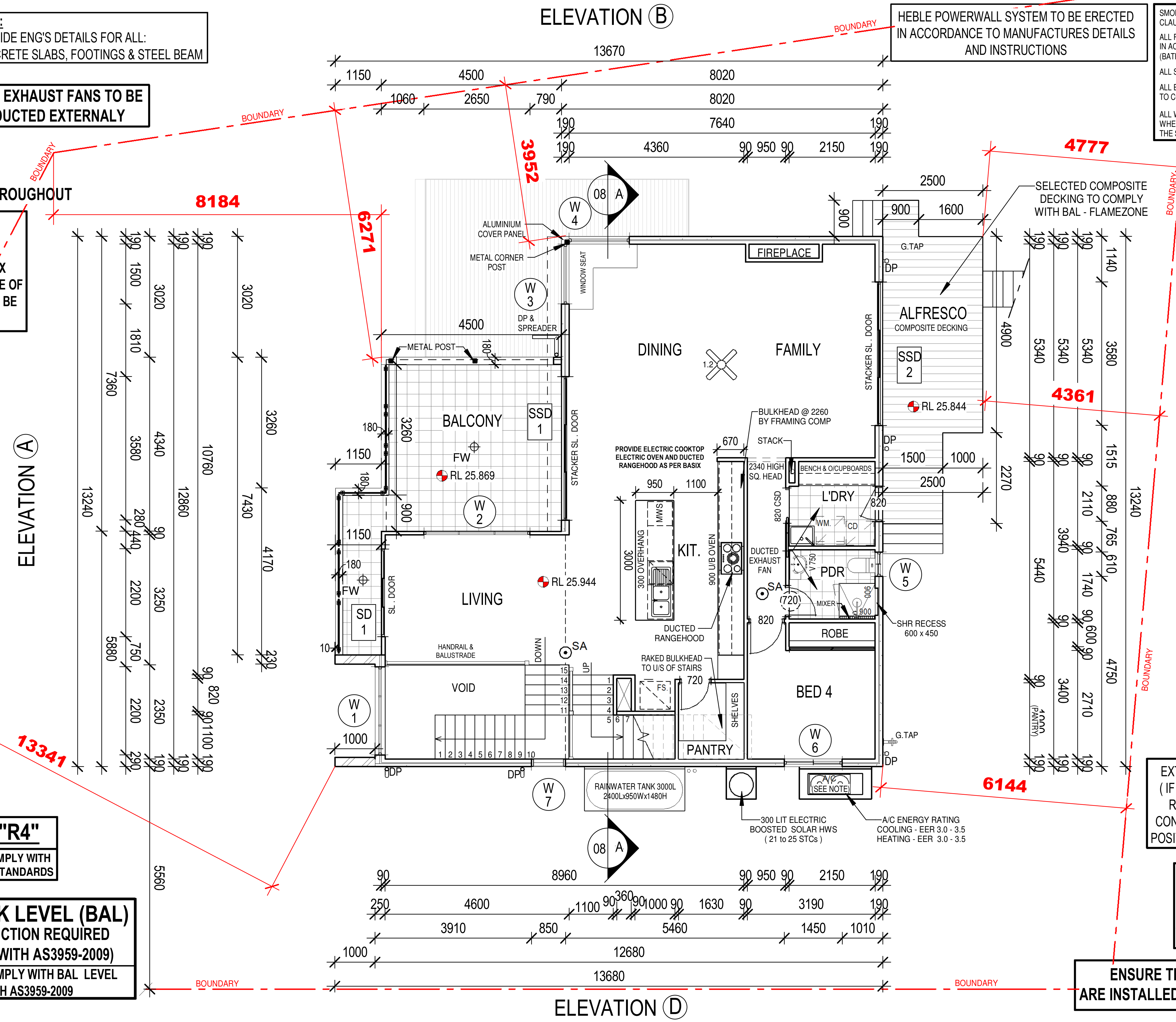

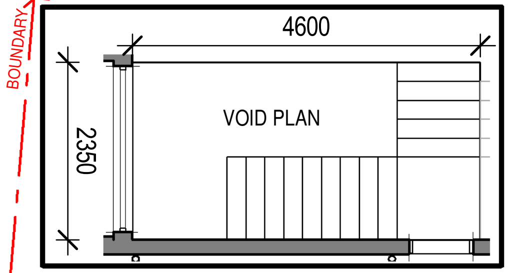

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MARINE RATING "R4"

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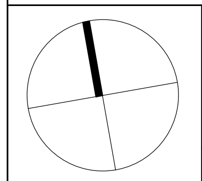

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for / client
MR AND MRS WANG

MIDDLE FLOOR PLAN

location
LOT34/35, 231-233 McCARRS CREEK RD
CHURCH POINT
NORTHERN BEACHES COUNCIL

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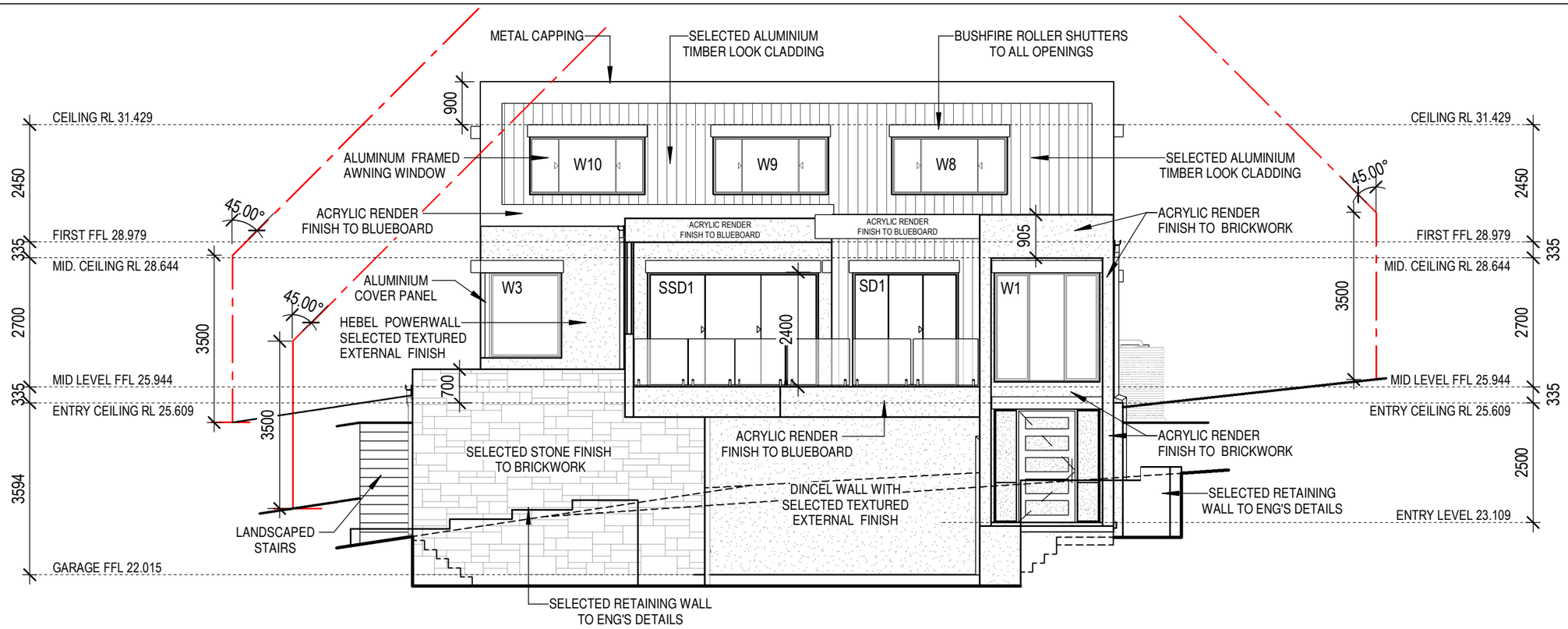
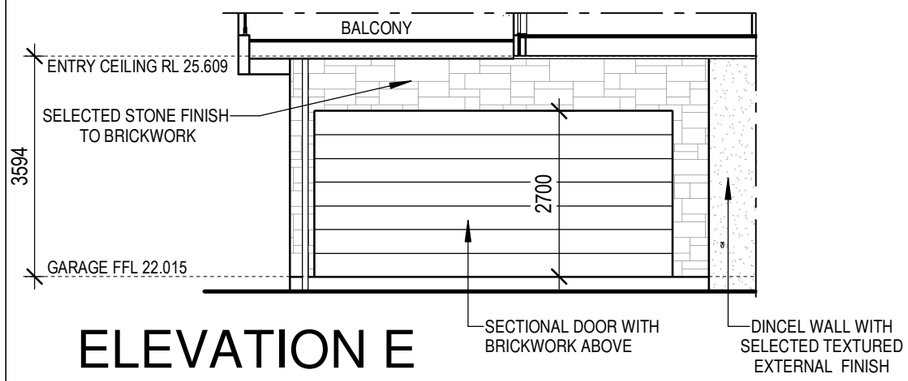


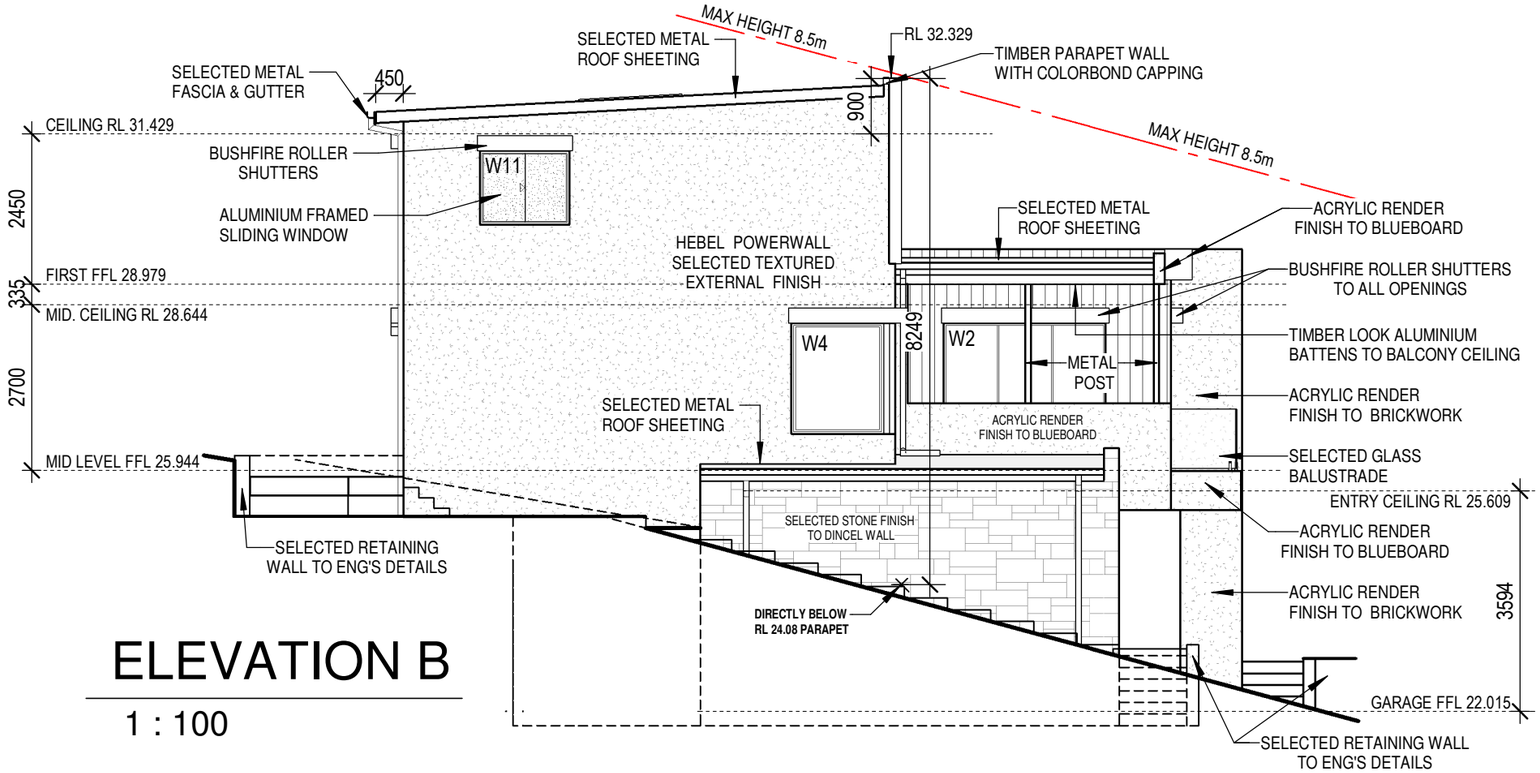
ELEVATION A

1 : 100



ELEVATION E

1 : 100



ELEVATION B

1 : 100

BUSHFIRE ATTACK LEVEL (BAL)
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ELEVATIONS

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CHURCH POINT
NORTHERN BEACHES COUNCIL

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checked - SB	16.02.25	1 : 100

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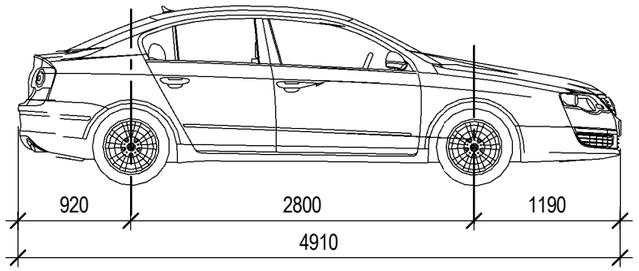
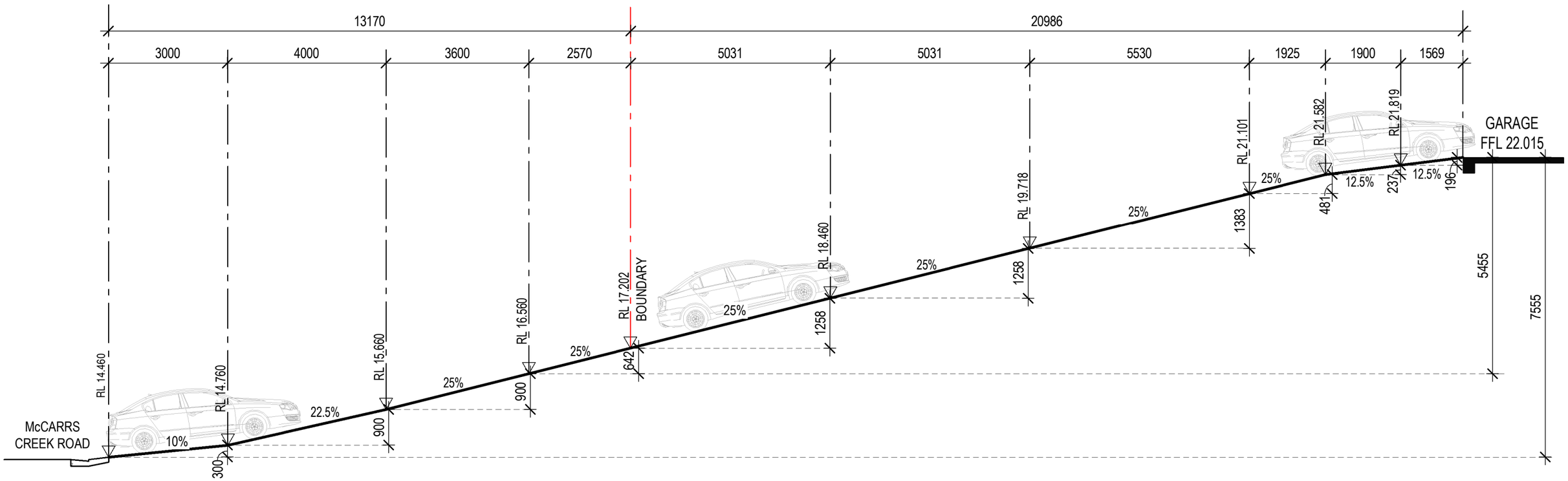


Australian B85 Type Vehicle – AS2890.1:2004



SCALE 1:100

All dimensions shown are to frame, underside of roof trusses and FFL (concrete) to underside of floor joist

FFL denotes - concrete level
floor joist + selected flooring

All construction work to comply with the NCC and all relevant Australian standards

All previous issues are to be discarded only these plans are to be read

rev	amendment	date	rev	amendment	date
A	FD	16.02.25			
B	AMENDMENT	07.03.25			
C	AMENDMENT	18.03.25			
D	AMENDMENT	06.04.25			
E	FFD DA SUBMISSION	29.05.25			

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for / client
MR AND MRS WANG

LONGITUDINAL DRIVEWAY SECTION

location
LOT34/35, 231-233 McCARRS CREEK RD
CHURCH POINT
NORTHERN BEACHES COUNCIL

DO NOT SCALE DRAWINGS

stage:	project no:	dwg no:
DA	SE2501	23 / 23
drawn - SB	date: 16.02.25	scale: As indicated
checked - SB		

LOCATION: D:\work\WINCREST\17491_Wang&Ma\Drawings\WORKING DRAWINGS\WORKING 06.04.25\WANG&MA - working - 06.04.25.rvt