

7 July 2025

Our Ref: H-24034

**STATEMENT OF HERITAGE IMPACT TO ACCOMPANY MOD-DA2024/1249
FELONS MANLY, MANLY WHARF, EAST AND WEST ESPLANADE (OPPOSITE THE CORSO,
HARBOUR SIDE), MANLY NSW 2095**

The following Statement of Heritage Impact (SOHI) has been commissioned by Howard Smith Wharves (HSW) Nominees in relation to the proposed installation of solid fuel cooking and exhaust for Felons Barrell Hall at Manly Wharf, which is located at East and West Esplanade, Manly NSW 2095 (subject site).

This SOHI has been prepared to accompany an application for Modification to DA2024/1249. The original DA, which was for *alterations and additions including internal fit-out for use of premises as a pub and artisan food and drink industry*, was approved by Northern Beaches Council on 11 February 2025. The original DA was supported by a SOHI prepared by City Plan Heritage (CPH) dated August 2024 which found that the proposed works were:

deemed acceptable from a heritage perspective and are consistent with the heritage objectives and provisions of the Manly LEP 2013, the Manly DCP 2013 and the SEPP (Biodiversity and Conservation) 2021, as well as the relevant policies contained in the Manly Ferry Wharf Conservation Management Plan, 2016.

The current Mod-DA is seeking approval under section 4.55 of the *Environmental Planning and Assessment (EP&A) Act 1979* for modification of approved consent which involves no or minimal environmental impact. It seeks to incorporate a solid fuel mechanical exhaust on the roof above the subject site.

This SOHI provides an assessment of the proposed addition of the solid fuel mechanical exhaust to the roofscape of the wharf. It concludes that the works will have a minimal visual impact on the significance of the 'Manly Wharf' heritage item.

1. HERITAGE LISTING

The subject site is within the curtilage of 'Manly Wharf', a heritage item listed under Part 1 of Schedule 5 of the *Manly Local Environment Plan (LEP) 2013*. The subject site is also listed on the State Heritage Register (SHR) as 'Manly Wharf', West Esplanade, Manly NSW 2095, SHR # 01434.

The subject site is also located in proximity to the following heritage items:

Environmental Planning and Assessment Act, 1979***Manly LEP 2013, Part 1 Heritage items***

- 'Pier (former Fun Pier), Manly Wharf', East and West Esplanades, item no. I146.
- 'Park/Reserve', East Esplanade, item no I143.
- 'All stone kerbs', Manly municipal area, item no. I2.
- 'Governor Phillip Monument', West Esplanade Reserve, item no. I248.
- 'Park', West Esplanade, item no. I251.

Environmental Planning and Assessment Act, 1979

- 'Commercial and residential building', 53 East Esplanade, item no. I153.
- 'Commercial and residential building', 50 East Esplanade, item no. I152.

Manly LEP 2013, Part 2 Heritage conservation areas

- 'Town Centre Conservation Area', item no. C2.



Figure 1: Map showing the approximate location of the subject works (overlaid in red) and the curtilage of the State heritage item 'Manly Wharf', SHR listing no. 01434 (blue hatching). Source: NSW Planning Portal Spatial Viewer, accessed August 2024.



Figure 2: Map showing the approximate location of the subject works (overlaid in red) and the curtilage of the local heritage item I145 'Manly Wharf'. Source: NSW Planning Portal Spatial Viewer, accessed February 2024.

2. DESCRIPTION OF WORKS

The proposal involves the installation of a new solid fuel mechanical exhaust duct above the existing retail wing of the Manly Wharf. The height is to be at or below the existing height of the duct discharge currently on site.

For this assessment, CPH has relied upon drawings prepared by Little Boat Projects

Date	Drawing Name	Drawing No.	Revision
27.06.2025	Cover Page + Site Plan	D.99.00	D3
27.06.2025	Roof Plans - Existing and Proposed	D.99.01	D3
27.06.2025	Elevations - Existing and Proposed	D.99.02	D3
27.06.2025	Axonometric - Demolition and Proposed	D.99.03	D3
27.06.2025	Section and Diagram	D.99.04	D3

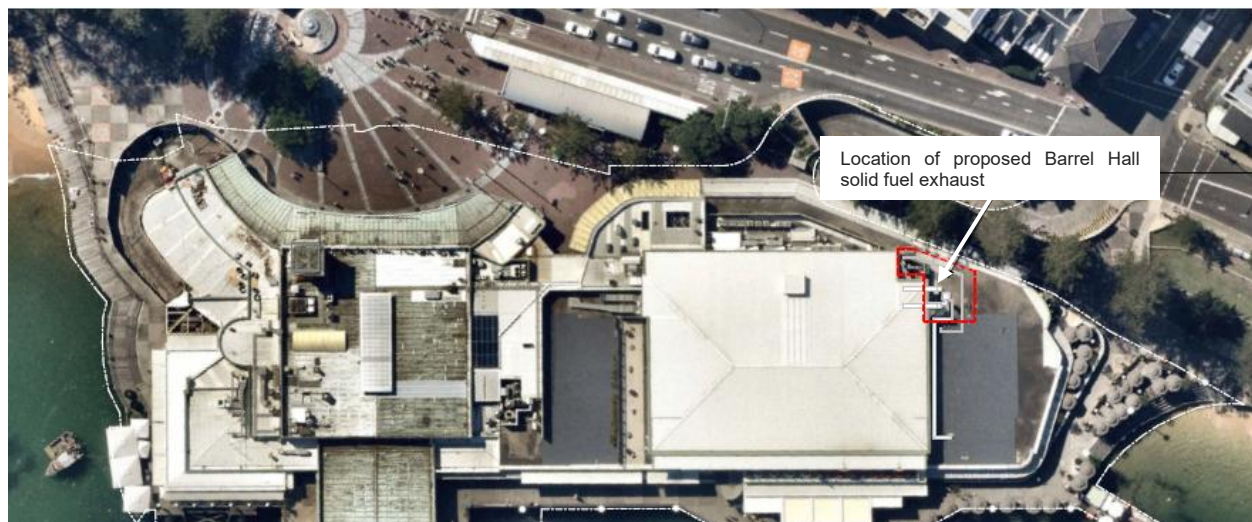


Figure 3: Site Plan showing the location for the proposed solid fuel exhaust duct on the roofscape above the retail wing of the wharf.
 Source: Little Boat Projects, Drawing No. D.99.00, rev D3.

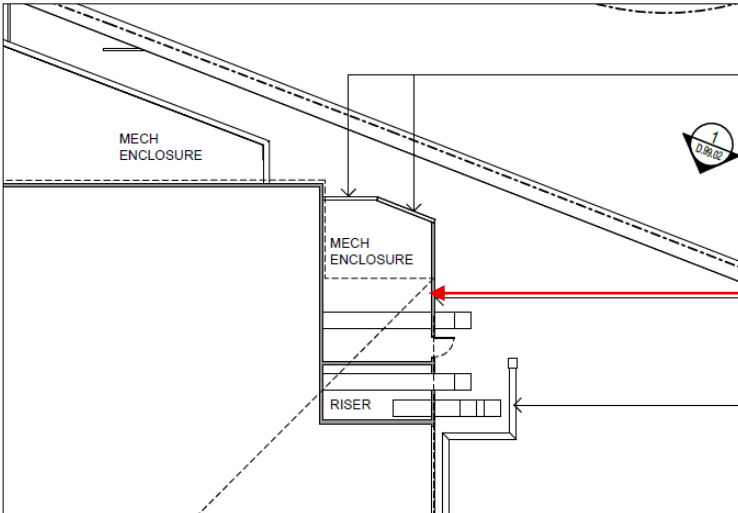


Figure 4: Existing mechanical enclosure. Portion of wall to be removed (red arrow) to accommodate new mechanical duct. Source: Little Boat Projects, D.99.01, rev. D3.

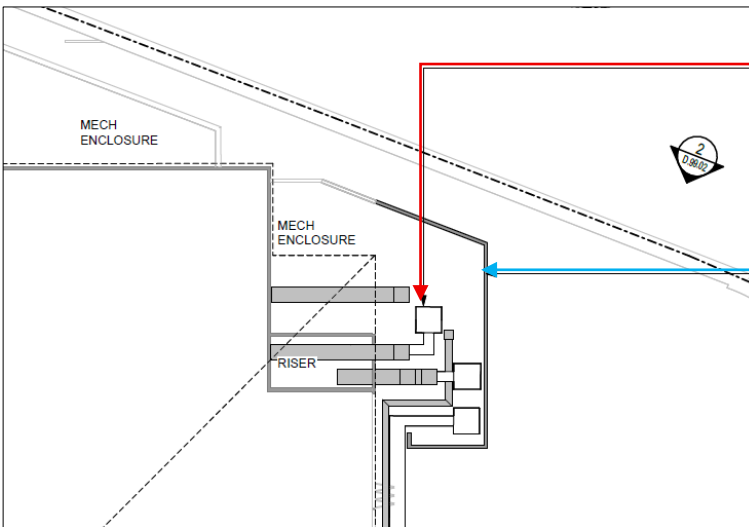


Figure 5: Proposed mechanical exhaust services (red arrow). Note. Blue arrow depicts extent of proposed service enclosure screen associated with Condition 9 of DA2024/1375. This is not proposed under this Mod-DA, but will ultimately aid in screening this proposed mechanical exhaust and has therefore been indicated. Source: Little Boat Projects, D.99.01, rev. D3.

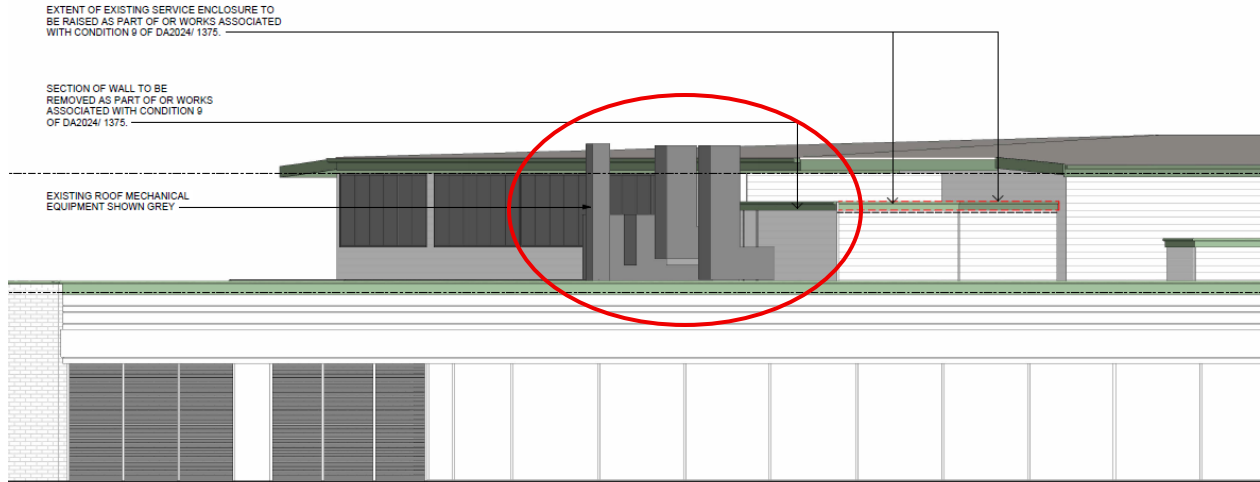


Figure 6: Existing elevation drawing showing existing mechanical services (red circle). Source: Little Boat Projects, Drawing No. D.05.030, rev D3.

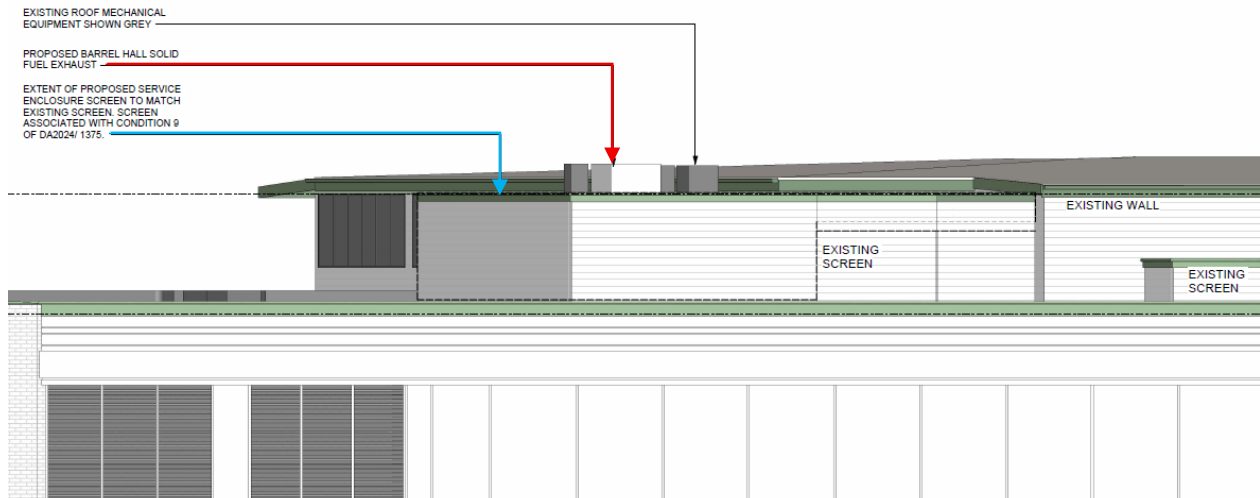


Figure 7: Proposed Elevation drawing showing new mechanical exhaust duct (red arrow). The existing screen is to be modified to screen mechanical services (associated with Condition 9 of DA2024/1375) is indicated by blue arrow. Source: Little Boat Projects, Drawing No. D.05.040, rev D3.

3. SITE DESCRIPTION

The Manly Wharf site comprises buildings from two periods of construction: the 1941 ferry wharf on the west, and the 1990 additions (including the retail arcade) to the east. The following physical description of the original wharf is taken from the SHI form for 'Manly Wharf' (listing no. 01434).

A broad wharf supported on timber piers and with a concrete platform. The superstructure is constructed of steel and timber. The facade and side walls form an important architectural design, similar to the Circular Quay ferry terminals. (Blackmore, Ashton, Higginbotham, Rich, Burton, Maitland, Pike, 1985).

The original part of the wharf was built in a modernistic transport idiom, with typical stylistic features of era including play of circular and rectangular geometric terms, bayed facade to the water (marine connotations), wide arc plan at entrance, clock tower with "fins", flat roofing marked by wide fascia board. The current entrance was originally designed as a tram terminus and turning area. Timber clad framed structure opening and large internal spaces, concrete deck to west enclosed by "ship" railing. Some original shop fittings, signage etc. Subjected to major alterations to the wharf wings involving a T-shaped clerestorey (Stapleton, 1981).

The following physical description of the retail arcade is extracted from the 2016 Manly Ferry Wharf Conservation Management Plan (CMP) prepared by Architectural Projects:

The eastern half of the building dates entirely from 1990. It required partial removal of the original 1941 curved ends. The lower level is below mean tide level, at AHD (Australian Height Datum) 0.550. The upper level is accessible from the western half of the building and, at AHD 3.600, is at approximately the same level as the ferry wharf and the surrounding ground level of East Esplanade. There is a small mezzanine level which contains the plant. It has a floor level at AHD 6.900. There are a number of other mezzanine floors above the ground floor level, which contain mechanical plant.

The eastern half of the building is a concrete slab structure, supported on concrete piles and some timber piles. The walls are faced externally in timber and fibre cement cladding. The building has skylights, and a flat, parapeted metal tray roof. The highest level of the parapet is AHD 10.5.

The subject site itself where the mechanical exhaust duct is proposed is part of the roofscape of the retail addition. This retail wing is a contemporary addition to the wharf, constructed in 1990. The roofscape contains existing mechanical services associated with the retail tenancies of the wharf.



Figure 8: View facing southwest showing main entry to Manly Wharf. Proposed works are not within this area of the roofscape.



Figure 9: View from welcoming arms / public entry to the wharf facing southeast towards the Sake & El Camino tenancies. Proposed mechanical duct works is to be located behind the first floor of the El Camino tenancy (red arrow). This will not be visible from main views of the wharf.



Figure 10: Google Street View (facing south-west) at intersection of East Esplanade and Wentworth Street. Approximate location for mechanical exhaust indicated by red arrow (behind street trees).

4. HISTORY OF THE SITE

The previous SOHI prepared by CPH that accompanied DA2024/1249 provides sufficient historical information for the development of the wharf and the eastern retail wing proposed for modification.

5. SITE SIGNIFICANCE

The following Statement of Significance for Manly Wharf has been extracted from the *Conservation Management Plan (CMP) for Manly Ferry Wharf*, prepared by Architectural Projects in October 2016:

The Manly Ferry Wharf is significant as the gateway to Manly, and for its association with thousands of tourists who hold memories of the ferry trip and first impressions of Manly.

The Manly Ferry Wharf is significant for its age and association with the development and continuation of Manly as a seaside resort of Sydney from the earliest days of European settlement. The low sweeping form has significance for its contribution to the context of the sweeping beach and line of the trees which is now an integral part of this mainland view of Manly. The Manly Ferry Wharf is significant for the positive contribution of the low building scale to the topographical setting of Manly Cove. The Wharf is enhanced by its visual relationship with the curved lines of the beaches, seawalls, and pavement promenades of East and West Esplanades.

The Manly Ferry Wharf in association with the fun pier is significant, for its ability to reflect the long continuous history of Manly Ferry Wharf as the location of retail, transport and recreation.

The Manly Ferry Wharf has historical significance as a major project by an important Sydney Modernist Architect Arthur Baldwinson. The main (north) facade composition, with its clocktower, flat parapets and curved awning is mostly intact. The grooved weatherboard cladding and timber frame windows are features which give the exterior much of its distinctive period character.

The Manly Ferry Wharf is significant as the site of the redevelopment of the area and wharf facilities in particular during Manly's second boom period as a resort (c 1910-1940), when both Circular Quay and Manly Ferry Wharf were constructed for the Maritime Services Board. The Manly Ferry Wharf is significant as a reflection of developments in urban transport and infrastructure in that period.

Manly Ferry Wharf is significant as a rare surviving working example of a maritime building designed in the Modernist style of the mid Twentieth Century. It is a rare example of maritime architecture.

The following grading of significance for the Manly Wharf heritage item has also been extracted from this CMP. This table provides justification for and informs each grading level.

Table 1: Grading of Significance Table used by Architectural Projects to assess the significance of various elements of Manly Wharf.

	Grading	Justification	Status
A	Exceptional	Rare or outstanding element directly contributing to an item's local and State significance	Fulfills criteria for local or State listing
B	High	High degree of original fabric. Demonstrates a key element of the item's significance. Alterations do not detract from significance.	Fulfills criteria for local or State listing

	Grading	Justification	Status
C	Moderate	Elements of typical representative quality. Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.	Fulfills criteria for local or State listing
D	Little	Alterations detract from significance. Difficult to interpret.	Does not fulfil criteria for local or State listing
E	Intrusive	Damaging to the item's heritage significance.	Does not fulfil criteria for local or State listing

The following Schedule of Significant Fabric table identifies the assessment attributed to each element of the Manly Wharf, as determined by Architectural Projects in 2016. Areas of the wharf that are relevant to this proposal are emphasised. Refer to Figure 11 for visual depiction of these significant fabric.

Table 2: Schedule of Significant Fabric, extracted from the Manly Ferry Wharf CMP.

Schedule of Significant Fabric	
The Exterior	
The Manly Ferry Wharf	
The South Elevation to the Wharf	B (High)
The East Elevation to the Wharf	B (High)
The West Elevation to the Wharf	B (High)
The Clerestory	
The East Elevation to the Clerestory	C (Moderate)
The West Elevation to the Clerestory	C (Moderate)
The Corso Façade	
The North Elevation of the Clerestory	A (Exceptional)
The Welcoming Arms	A (Exceptional)
The Clock Tower	A (Exceptional)
The Interior	
The Manly Ferry Wharf	
South Glazed Waiting Area	A (Exceptional)
Transit Space	B (High)
The Clerestory	
Main Space	A (Exceptional)

Schedule of Significant Fabric	
Remnant Service Areas	C/D (Moderate / Little)
The Welcoming Arms	A (Exceptional)
Service Areas	C (Moderate)
Under Awning Space	A (Exceptional)
KEY PHASES ADDITIONS	
The Exterior	
North Façade 1941	A (Exceptional)
South Façade Wharf 1941	B (High)
East Façade 1990	C (Moderate)
West Façade 1990	C (Moderate)

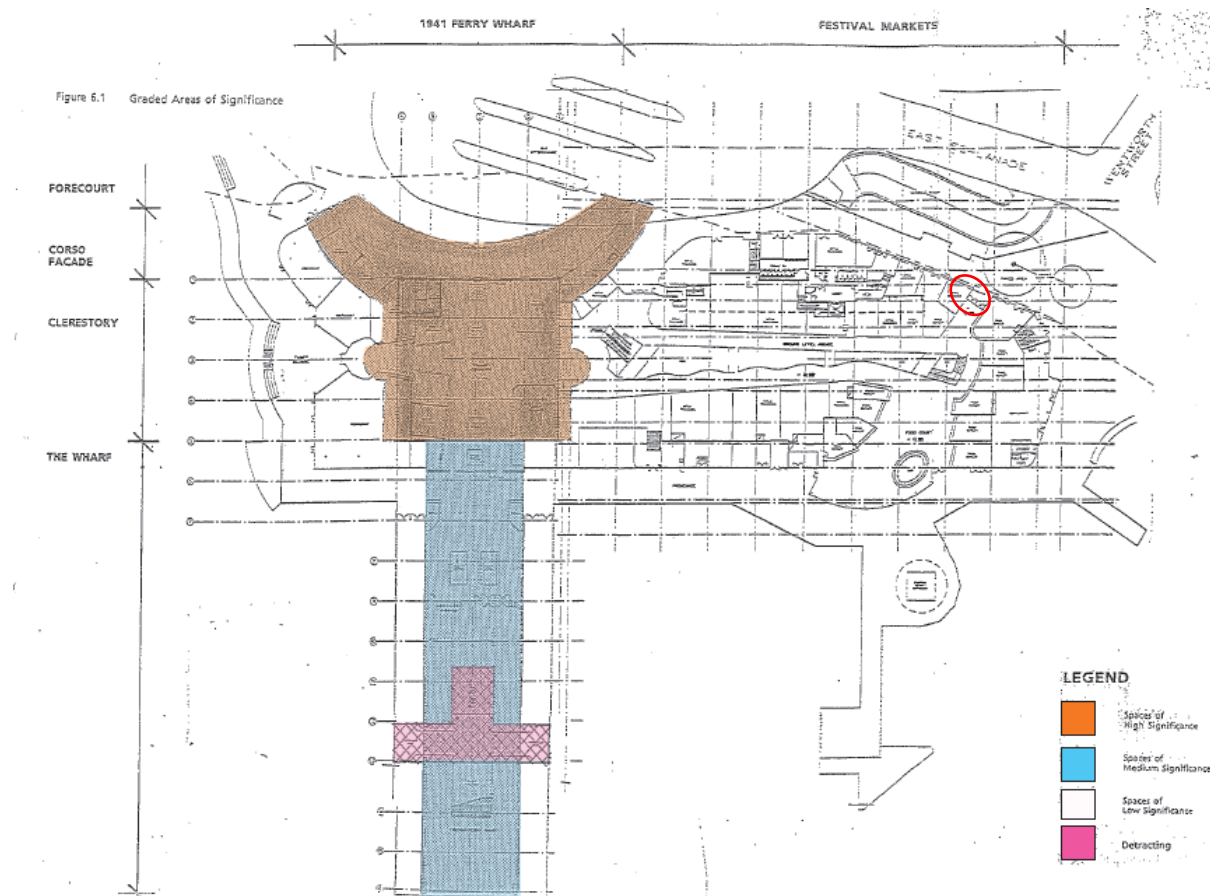


Figure 11: Graded Areas of Significance Map, with red overlay of approximate area where mechanical duct is proposed.

6. HERITAGE IMPACT ASSESSMENT

The proposed works involve the installation of a mechanical exhaust duct on the roof of the retail wing of the Manly Wharf. These works will have no physical impact on the significant fabric of the Manly Wharf heritage item, and a minimal visual impact on the site.

The mechanical exhaust duct is proposed to be installed in the roofscape away from the significant areas of the wharf, as identified in the Grading of Significance map extracted from the 2016 CMP. The significant areas of the wharf are those which were part of the original 1941 construction (clerestory, welcoming arms, Corso entry, wharf entry), with the eastern section of the wharf (where the works are located) part of the 1990 retail wing addition to the site. Being in a non-significant area, this means the work to install this mechanical exhaust will not impact on any significant fabric. Instead, non-significant fabric (existing ductwork) will be altered to accommodate the proposed exhaust duct, which reinforces the minimal impact the works will have on the heritage item.

The visual impact of the proposed works is considered minimal, primarily because they will only alter non-significant façades of the site and not impact any significant views or vistas of the wharf. The 2016 CMP identified the eastern façade of the wharf (where the works are located) as having moderate significance, and therefore the sympathetic alteration of this façade is permissible. The mechanical exhaust will sit at or below the height of the existing duct discharge, meaning it will not be a taller addition to the roofscape. Regardless, there are not many views of this area of the roofscape, with only some views afforded of this roofscape from East Esplanade. It is noted however that the significant tree plantings along East Esplanade obscure most of these non-significant views. It is also noted that there is no visual relationship with the area of the roof proposed for the mechanical exhaust and the main entry to the wharf and its welcoming arms / clerestory, meaning significant views of these elements and features will not be interrupted.

Regardless of the above, the existing screening on the roofscape is being extended across the eastern section of the retail roofscape. This is being undertaken to satisfy condition 9 of DA2024/1249. This screen will not only provide adequate visual screening of the mechanical services associated with that DA, but will also sufficiently screen the proposed mechanical exhaust duct subject to this Mod-DA. This will aid in the sympathetic addition of the fuel exhaust to the roofscape of Manly Wharf. It is noted that the screening is subject to the approval of Northern Beaches Council's heritage officer, and so it may be modified at the Council's request. Regardless of the form of the screening, it will aid in disguising the proposed mechanical exhaust.

The following relevant policies from the Manly Ferry Wharf CMP (Architectural Projects, 2016) have been extracted below for consideration.

7.7 INTEGRATION OF SERVICES

7.7.2 Policy - Installation of Services

The extension or alteration of existing services in the building is acceptable in the context of re-use, but should not have a detrimental impact on the significance of the building as a whole.

7.7.3 Policy - Ventilation

Appropriate ventilation and climate control that enabled retention of long-term tenants.

7.9 EXTERIOR

7.9.1 Policy - Exterior Appearance

The overall 1941 building form should be preserved. All remaining intact fabric on significance façades, as identified in section 5.10 [Gratings of Significance Table, incorporated above at Section 5] should be retained and conserved. The existing form, external surfaces, materials and finishes of the façade should be preserved. It is desirable that 1941 door and window openings should be enlarged to a consistent size and maintain retention of the weatherboard. No new work should comprise the original significance features.

7.9.2 Policy - Façade Modification

It is desirable that where it is necessary to modify the façade, changes to the façade should reinforce the composition of the original façade.

7.9.3 Policy - Façade Additions

Additions of little significance that detract from an appreciation of the original building detail as identified in Section 5.10 [Gratings of Significance Table] should be removed. Scope exists to modify the 1990s alterations particularly if they allow a better appreciation of the original building.

7.9.4 Policy - Façade Changes

The interpretation of the building would benefit by a better understanding of the original construction phase of the 1941 wharf and 1990 additions as modified in 2007.

7.10 INTERIOR

7.10.3 Policy - Impact on Façade

Internal work should not compromise the significance 1941 façades of the buildings.

7.10.4 Policy - Low Integrity Interiors

As the interiors of the building have been extensively modified with numerous [sic] further modification could occur to the interior fitout. The character defined by the original interiors that create the high spatial quality of the clerestory should be preserved.

The proposed fuel exhaust is considered to be in accordance with the above policies. The exhaust will introduce new services to the wharf, however it will not adversely impact the significance of the wharf as a whole. This is because, as described earlier, the services are introduced in non-significant areas that contain contemporary fabric and façades, and will be sympathetically added through appropriate screening (under a separate DA). The fabric of the original 1941 section of the wharf will not be impacted, nor will current significant views afforded to the important façades of the site (the northern façade of the building off East and West Esplanade, and the southern façade of the wharf from the water). Instead, non-significant façades will be visually altered, though this alteration is only considered minor due to the scale of the mechanical exhaust matching that which currently exists in this area of the roofscape.

7. CONCLUSION

In conclusion, it is considered by CPH that the proposed work, incorporating the installation of the mechanical exhaust duct within the Manly Wharf retail wing roofscape, will have no physical impact and a minimal visual impact on the 'Manly Wharf' heritage item. This is because the addition proposes the alteration of non-significant fabric and non-significant façades of the wharf. The exhaust will match the existing mechanical services present in this area, and will be screened through the implementation of an extended screen (under Condition 9 of DA2024/1249). No significant areas or views of the wharf will be impacted by the proposal.

As a result, it is considered by CPH that the proposed s 4.55 modification is appropriate from a heritage perspective and will not cause any additional heritage impact to that outlined in the previous SOHI that accompanied the original DA documentation.

We trust the above statement of heritage impact will satisfy the requirements for the assessment of the proposed S4.55 modifications to roof of Manly Wharf, East and West Esplanade (opposite The Corso), Manly NSW 2095. Should you wish to discuss the matter further please do not hesitate to contact the undersigned on 02 8270 3500 or at kerimed@cityplan.com.au.

Yours Sincerely,



Kerime Danis

Director - Heritage